



COMPLIANCE CONDITIONS 2 & 31

for PROPOSED RESIDENTIAL DEVELOPMENT
CLONBURRIS STRATEGIC DEVELOPMENT ZONE
PHASE T3

November 2023

CONTENTS

- 1. Introduction
- 2. Response to Conditon 2
- 3. Response to Condition 31



IMAGE 1 – SDZ LANDS STRATEGIC CONTEXT MAP

- Clonburris SDZ Lands
- Local Authority Boundaries
- Train Line
- M50

DESIGN TEAM

Applicant	Cairn Homes Properties Ltd.
Planning Consultant	John Spain Associates.
Architect	McCossan O'Rourke Manning Architects
Civil and structural Engineer's	DBFL Consulting Engineers
Landscape Architects	Cunnane Stratton Reynolds

1. INTRODUCTION

DEVELOPMENT:

The development will consist of the construction of 157 no. dwellings on a site of c.3.45 hectares in the Clonburris South-West Development Area of the Clonburris Strategic Development Zone (SDZ) Planning Scheme 2019 as follows:

- A) 81 no. houses comprising 3 no. 2-bedroom houses, 64 no. 3-bedroom houses and 14 no. 4-bedroom houses (all 2-no. storey with associated private open space and car parking);
- B) 76 no. apartment units consisting of 26 no. 1-bedroom and 50 no. 2-bedroom units within Block 1 (4 no. storeys);
- C) Vehicular access will be provided from the permitted street under SDZ21A/0022 and the permitted Clonburris Southern Link Street (SDZ20A/0021) and R113 (Fonthill Road) to the east;
- D) All ancillary site development works including footpaths, landscaping boundary treatments, public and private open space areas, car parking (170 no. spaces) and bicycle parking (170 no. spaces), single-storey ESB sub-stations, bin and bicycle stores and all ancillary site development/construction works.

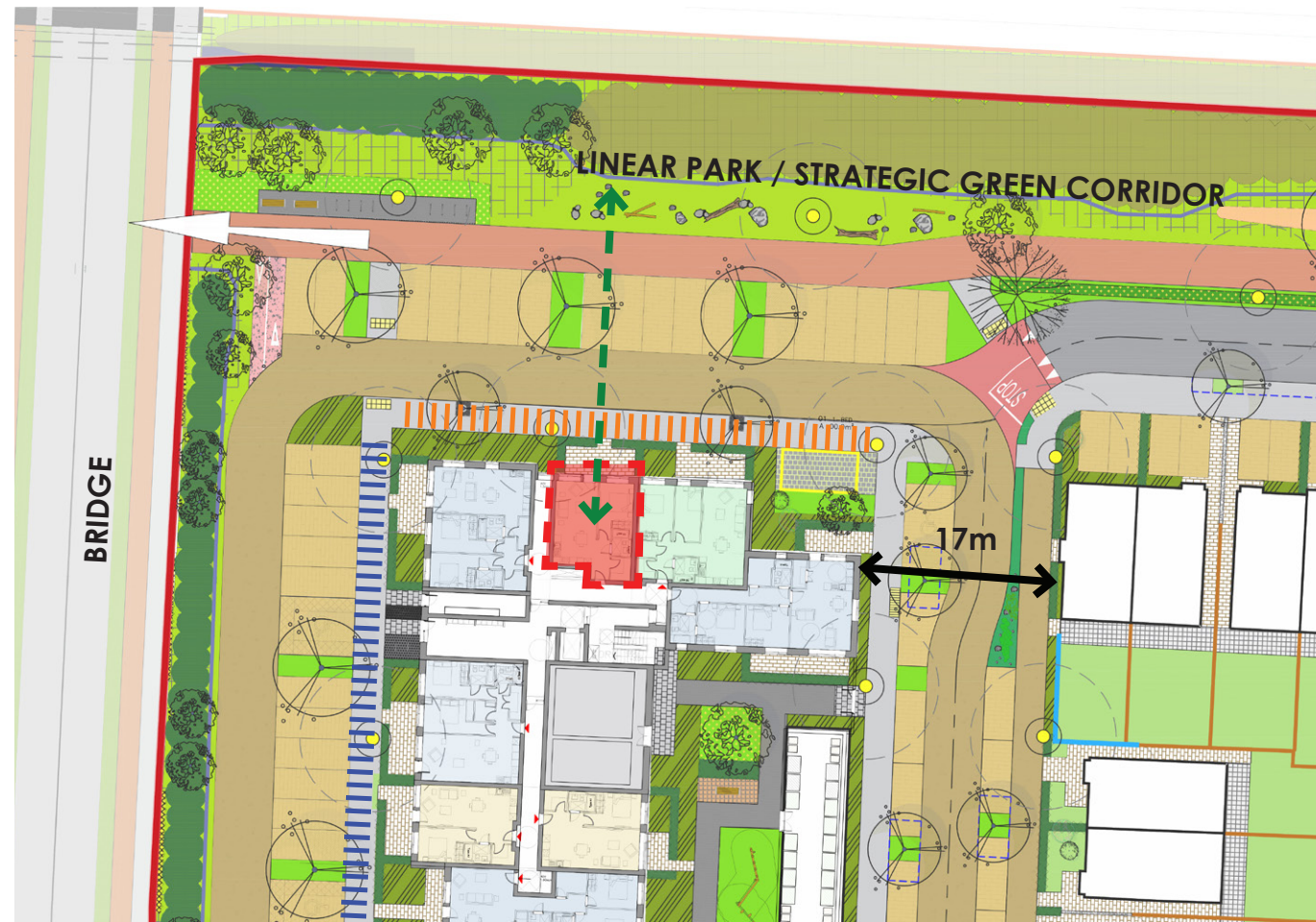


IMAGE 2 – NORTH FACING APARTMENT - GROUND FLOOR PLAN PREVIOUS SUBMISSION

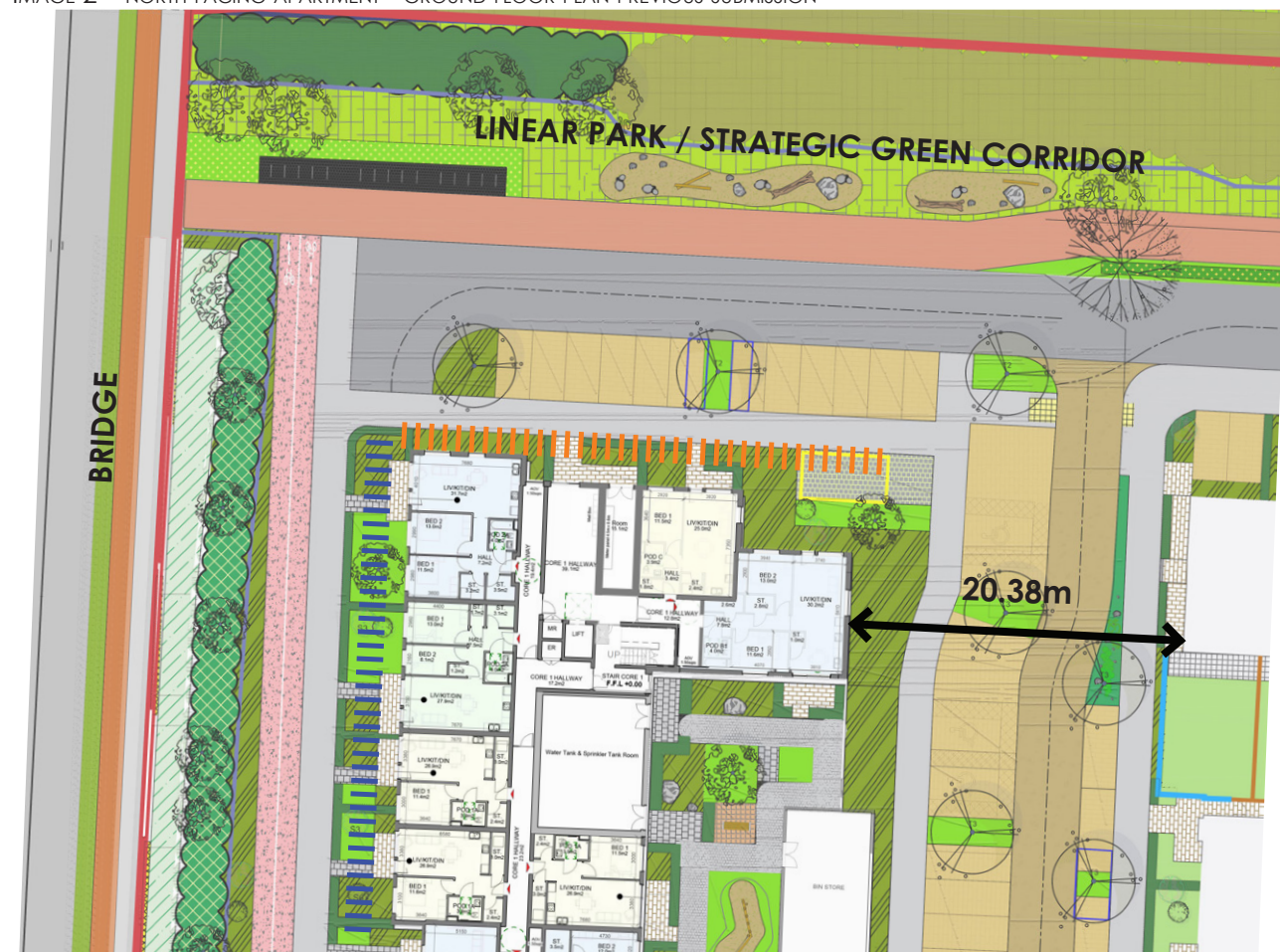


IMAGE 3 – REVISED NORTHERN CORE - GROUND FLOOR PLAN PROPOSAL

CONDITION 2 - AMENDMENTS

Prior to the commencement of development the applicant, owner or developer shall submit the following for the written agreement of the Planning Authority:

Unless otherwise agreed revised plans that incorporate all of the following amendments-
 (a) The ground floor single aspect north facing apartment shall be amalgamated with the adjacent apartment. All upper floor north facing single aspect apartments shall be redesigned to provide additional private amenity space. An updated Schedule of Accommodation, HQA and Schedule of Areas shall be provided.

REASON: To protect the amenities of the area and in the interests of the proper planning and sustainable development of the area.

In response to this item, and in tandem with condition 31 which requests the removal of the parking spaces on the western side of the apartment block, we propose an alternative arrangement to that proposed in condition 2. Our proposal amalgamates the 1 bed unit at ground floor with the neighbouring 2 bed unit as requested but also relocates the core entrance to the northern facade of the building, effectively swapping its location with the 1 bed unit on the western edge. This proposal retains the 76 units previously proposed. Furthermore, the removal of the parking spaces along the western side of the building as requested in condition 31 means that the revised entrance location is now more conveniently located in relation to the amended parking layout and the retained apartment avails of a more favourable aspect/interface to the west.

On the northern facade, from the 1st to the 3rd floors, we are proposing to provide larger balconies for the 1-bedroom units to provide additional private amenity space. For a more detailed breakdown of these modifications, please refer to the itemized HQA, which is included with the compliance documents.

A reduction in size of the ground floor apt. no. 120 is required to comply with statutory requirements, however it is proposed to allocate apt. no. 82 as a universal design unit in lieu of apt. no. 120

CONDITION 31 - VEHICULAR BRIDGE

A. Prior to the commencement of the development on the apartment building located in the west of the application site, the applicant shall demonstrate the following, unless otherwise agreed in writing;

i. The feasibility of the bridge and the requirement for and extent of an associated embankment;

The team has undertaken a feasibility study of an embankment scenario for the vehicular bridge to assess its potential impact on the site plan and apartment design, as well as a cost analysis. As part of this exercise, 3 scenarios for the bridge were explored.

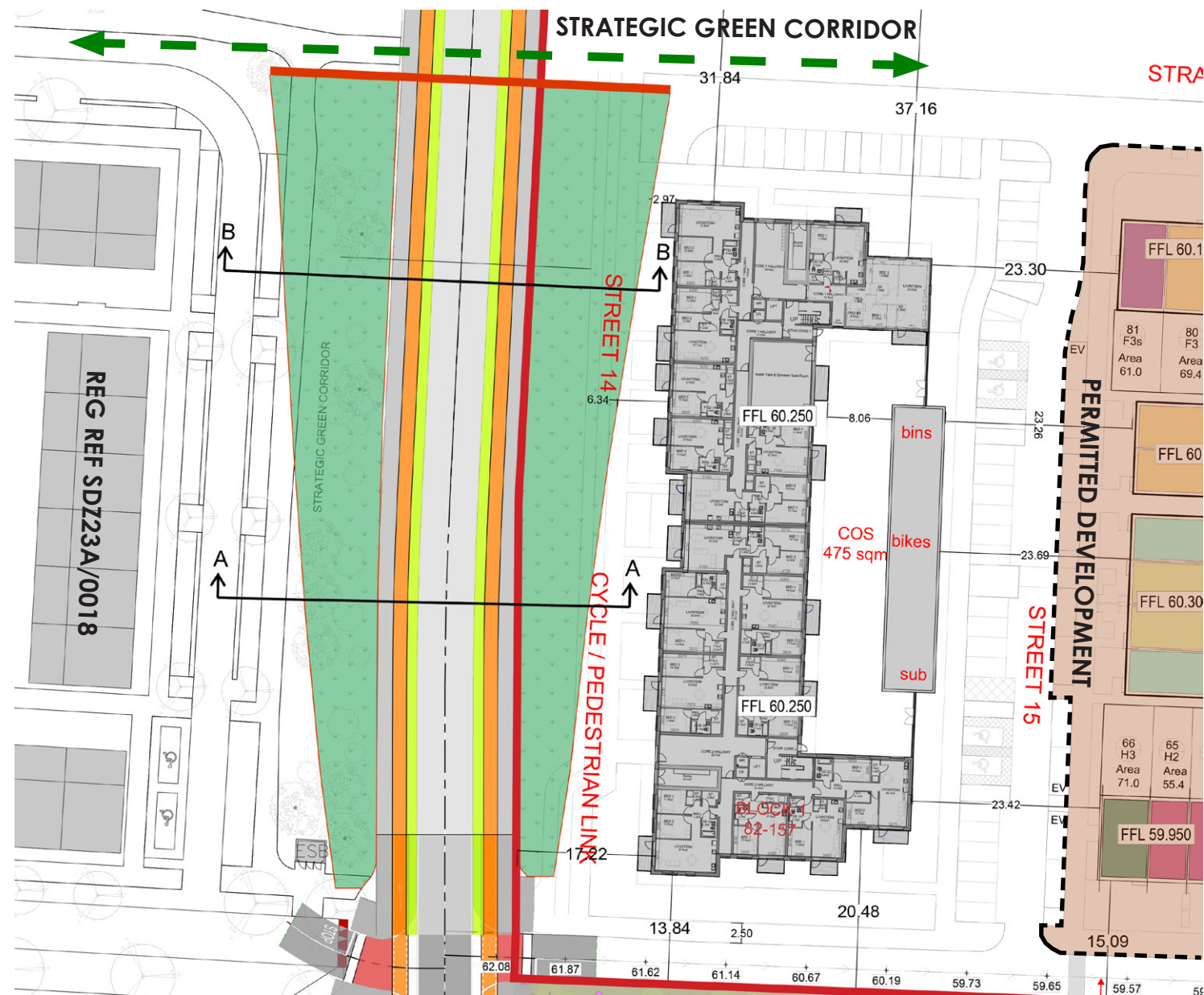


IMAGE 4 – SITE PLAN - BRIDGE EMBANKMENT FEASIBILITY

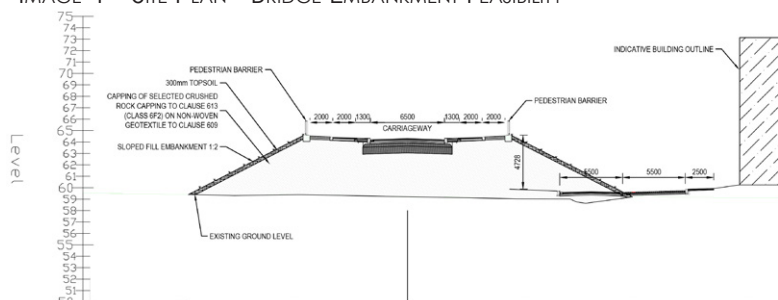


IMAGE 5 – SECTION AA - EXTENT OF EMBANKMENT

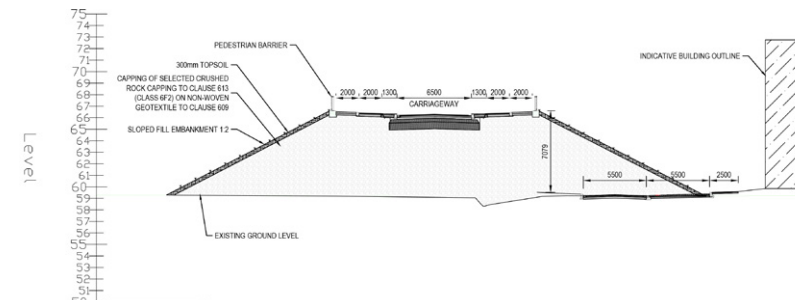


IMAGE 6 – SECTION BB - EXTENT OF EMBANKMENT

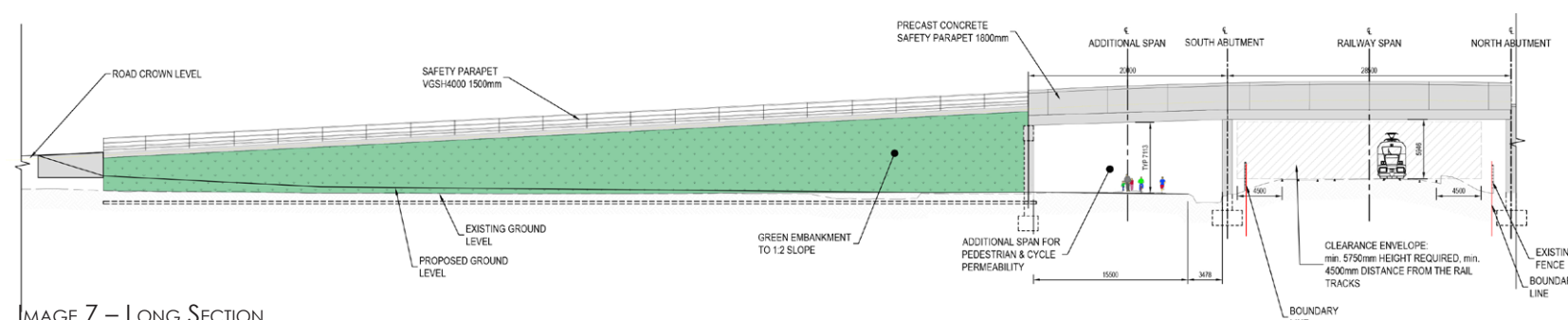


IMAGE 7 – LONG SECTION

As well as being the most expensive alternative, we respectfully submit that the embankment scenario appears at odds with the objectives set out in the planning scheme. Most notably, the linear park located parallel to the rail line which is to run uninterrupted from east to west, connecting the Clonburris and Kishogue Urban centres. This vital site connection will necessitate a pedestrian and cycle route which passes underneath the bridge. With the embankment scenario, the area under the bridge measures 50 metres in length. By comparison, in the landscaped retaining wall scenario (refer to page 8), as submitted at planning and further information stages, the span under the bridge is 17m. Given its length the 50m span has potential to facilitate anti-social behaviour as well as introducing a general less inviting environment for the proposed east-west link.

The height and land take for the embankment will result in a greater negative impact on the quality of the amenity space for the adjacent residential blocks. In order to mitigate this impact the developable land available for the apartment block would be reduced. This could, in turn, impact the target dwelling numbers required for compliance with planning scheme objectives. It should also be noted that the quantum of developable land in this location has already been eroded by the 20m gas wayleave which runs along the R113 to the east of the subject site.

i. Should an embankment be required, demonstration that the revisions would not impact on the daylight and sunlight of the consented apartments;

We respectfully submit that a landscaped retaining wall approach would be alternative and viable option for the bridge for consideration by the Local Authority and consequently it is our position that the embankment is not required to meet the objectives of the SDZ.

For the option proposed we have made minor amendments to the apartment building footprint, maximising the distance from the bridge, to ensure maximum possible light penetration for the apartments. Additional daylight /sunlight analysis of these revised floor plans has been conducted and all units pass current BRE guidelines.

CONDITION 31 - VEHICULAR BRIDGE

ii. Retention of the existing hedgerow;

Following CSR informal meeting with SDCC Parks & Public Realm to discuss the removal of 'Hedge 8' on the western boundary of Tile 3, it was demonstrated that it would not be possible to retain this hedge due to the location of the existing hedgerow relative to the proposed adjacent road bridge (not within the T3 redline). Whilst we are unable to retain this hedge, it is possible to compensate for its removal with the alternative landscaped retaining wall option for the bridge. The proposal is to replace this hedge with a new mixed species native hedgerow and green wall along the length of this boundary/ bridge embankment (please refer to Landscape General Arrangement drawing 22234-3-101). Topsoil from the removal of Hedge 8 will be retained, stored separately, and reused between the footpath and the green wall to ensure that the existing seed bank is protected and re-used for the new hedgerow and any other native planting areas (e.g. Miyawaki planting areas). In addition, it is proposed that a biodiverse brown roof is to be placed on the apartment building to replace the green roof that is currently proposed. (Please refer to Landscape General Arrangement drawing 22234-3-101).



IMAGE 8 – PROPOSED PLAN

iii. Removal/ Relocation of the car parking within the area to the west of the apartment block, ensuring a parking ratio of approx. 0.4 is maintained for the apartments and the overall on / off curtilage parking requirement is in accordance with the scheme. Revised layout / landscaping plans should be provided if necessary;

The site layout has been revised, as requested by this condition, to remove parking spaces on the western side of the apartment block (as illustrated on image 8 to the left). The removal of all spaces on the western side of the building presents an opportunity to create a landscaped link, dedicated to pedestrian and cyclist movement between the strategic local corridor and the local park to the south of Block 1.

This condition means that the street formally street 14 is no longer required as a local street. A local street allows for a 23m width and parking on both sides of the street. We feel that it is appropriate to suggest a similar redesignation of Street 15 as a 'local Street' rather than a homezone, in order to relocate some of the parking lost as a result of the condition which removes spaces on street 14. This still represents an overall reduction in the quantum of parking dedicated to the apartments from a ratio of 0.67 to 0.51. Parking is still in compliance with the overall on / off curtilage parking requirements in accordance with the planning scheme.

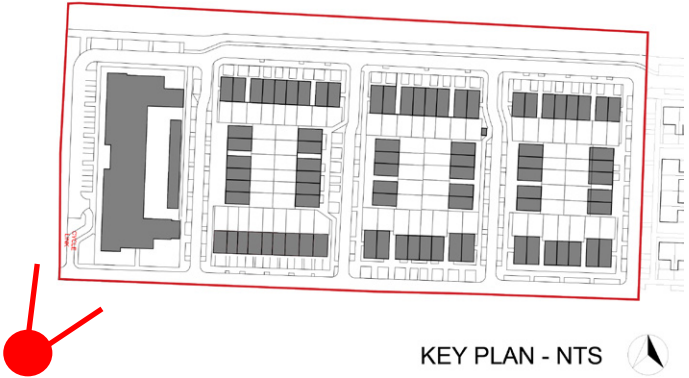
Further to the above A reduction in the apartment block footprint will allow us to maximize the distance between the bridge and the residential units, safeguarding their amenity, as well as preserving adequate daylight and sunlight levels. These refinements have resulted in small reductions to overall dimensions as well as minor revisions to the units within the apartment block. Compliance with space standards for all residential units is maintained. We also have removed the step in the building between the northern and southern cores. We respectfully submit that the combination of these proposals will address the requirements of this condition.

B. Should the applicant fail to demonstrate full compliance with Part A above, the apartment block and associated infrastructure and open space shall be omitted.

REASON: To ensure that the proposed development would not prejudice the delivery of key infrastructure with the Clonburris Strategic Development Zone area and ensures the protection of the existing Ecological Infrastructure.

We respectfully submit that our proposals demonstrate the compliance with Part A can be achieved in accordance with proper planning and therefore section B is no longer relevant. We note that If the apartment block is removed from the scheme, the number of units proposed for CSW-S3 would reduce from 445 units to 369 units - well below the 422 dwellings allocation for the sector. 162 units have been granted planning under SDZ21A/0022 and a further 126 as part of SDZ22A/0018. 81 houses have been granted planning with this application. This means that a minimum of 53 units have to be proposed in this block in order to comply with the planning scheme targets. Our client is focused on the delivery of these targets and following the successful launch of Parkleigh in early September looks forward to quickly implementing the planning granted for this tile.

3. UPDATED CGI



Clonburris T3 Embankment



Future Bridge - Steep Embankments	
* Steep Embankments only	
Steep Embankments (approach span)	
STEEP REINFORCE EMBANKMEN €160/m2	
	€115,680.00
NON-WOVEN GEOTEXTILE TO CLAUSE 609 TO REINFORCE EMBANKMENT €4.5/m2	€3,253.50
Site Clearance & Excavations	
	€42,258.00
TOPSOIL	
	€2,066.00
FILLING CAPPING OF SELECTED CRUSHED ROCK CAPPING TO CLAUSE 613 (CLASS 6F2) TO SREEP EMBANKMENT €60/m3 (AECOM Rate)	€189,661.00
Total Cost for Option 3	€352,918.50