



Please note that this report address RFI points
1B, 1C, 1D, 1E, 1F, 1G, 2.1, 2.2, 3.2 & 3.3

RFI Response

Clonburris SDZ
November 2023

Relevant SDZ areas diagram:

Requested AI:

1B. 385 units are proposed under the current application. No indication is give as to how many are located within subsectors S1 and S2 and the applicant is requested to clarify this. It is also noted that there is a 220KV power lines traversing the site. The applicant is requested to set out how undergrounding of these cables may impact phasing / delivery of the houses.

Response:

Please see explanation of the proposed development unit numbers broken down into the respective S1 & S2 areas. You will note that the calculation is shown for 2 scenarios:

- Scenario No. 1 - showing the entirety of the development area as set-out within the SDZ
- Scenario No. 2 - showing the areas proposed within the subject application only.

We note that the minimum and maximum achievable number of residential units in Clonburris together with target dwelling numbers are set out in Table 2.1.5 of the SDZ. Residential unit numbers are based on an allowable margin within a prescribed density range for each Development Area (see Section 2.1.5 – Residential Density).

Transfer of Residential Floorspace:

Subject to no net loss of units within a Development Area and the achievement of the built form objectives, the Planning Authority may allow up to 10% of the maximum residential units allocated in any Sub Sector to be transferred to an immediately adjacent Sub Sector.

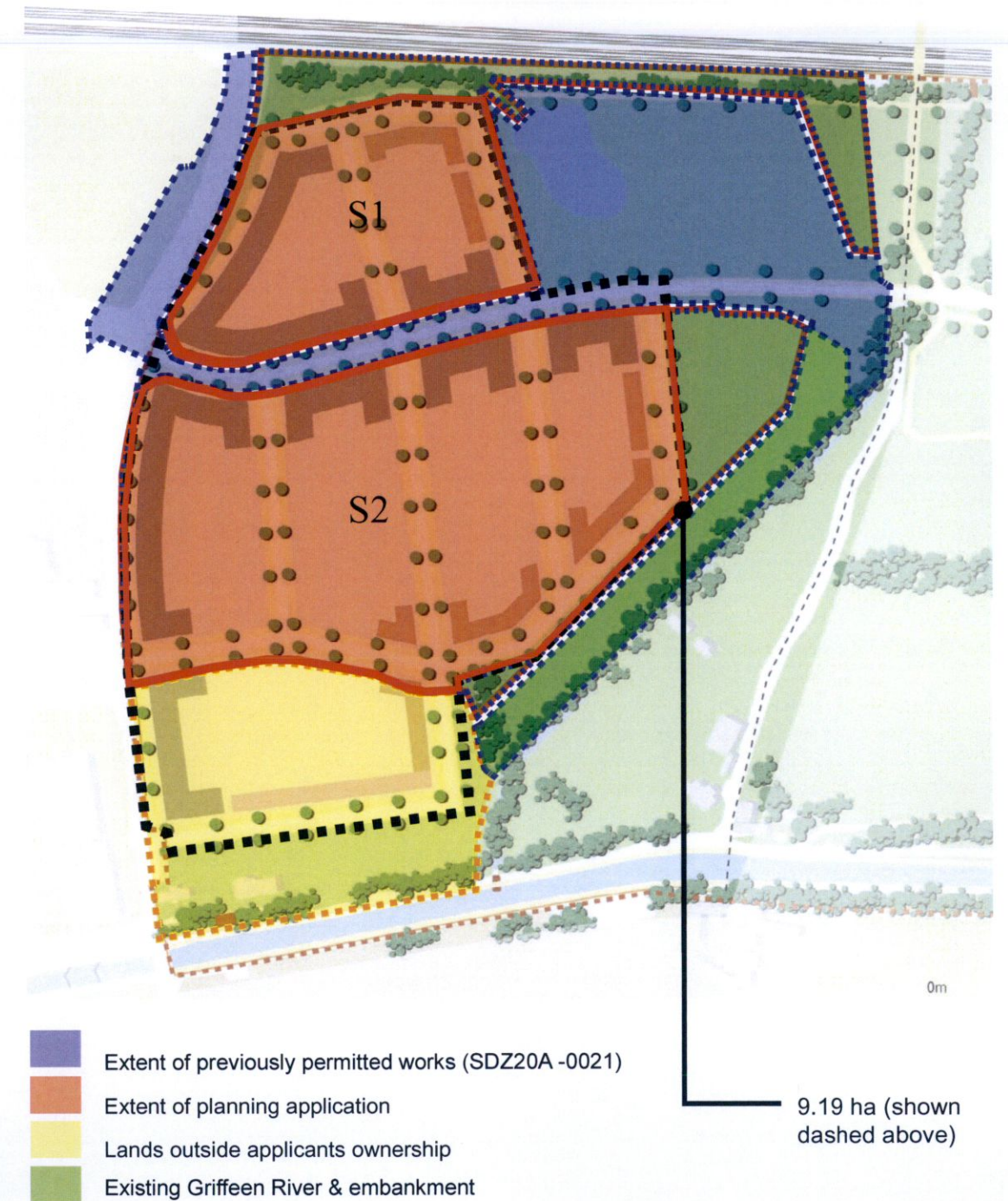


Table 2.1.5 | Extent of Residential Development According to Development Area

Development Area	Total Dwelling Units (number)		
	Min	Target	Max
Adamstown Extension	395	442	487

Area character type	Low to medium density development that will take the form of an extension of Adamstown.	
Net development area	9.19ha	
No of units (Target)	442	
Net Density	Sub Sector	Density Range
	AE-S1	45-53
	AE-S2	45-53
* See also Table 2.1.5 for full range of density		

Proposed Site Plan - revised to account for S1 & S2 densities

Revised S1 layout

now includes 137 units over 2.36 Ha
this equates to a density of 58.0 units/ha
We note that this figure is approx 9% higher
than the 53.0 units/ha max density figure
noted in the SDZ. However we note that the
SDZ refers to an allowance for an additional
10% of the maximum residential units
allocated in any Sub Sector to be transferred
to an immediately adjacent Sub Sector.

Revised S2 layout

now includes 258 units over 5.15 Ha
this equates to a density of 50.1 units/ha
We note that this is c. 5% below SDZ
maximum density figure of 53.0 units/ha

Revised S1 sub-total:

137 units over 2.36Ha = 58.0 units/ha

Revised S2 sub-total:

258 units over 5.15Ha = 50.1 units/ha

Total S1 + S2 figures:

395 units over 7.51Ha = 52.6 units/ha



Proposed Masterplan Plan (incl. a notional layout pitch and putt lands) - revised to account for S1 & S2 densities

Revised S1 layout

now includes 137 units over 2.36 Ha
this equates to a density of 58.0 units/ha
We note that this figure is approx 9% higher
that the 53.0 units/ha max density figure
noted in the SDZ. However we note that the
sdz refers to an allowance for an additional
10% of the maximum residential units
allocated in any Sub Sector to be transferred
to an immediately adjacent Sub Sector.

**Revised S2 layout
Incl. Pitch and Putt Lands**

now includes 339 units over 6.83 Ha
this equates to a density of 49.6 units/ha
We note that this is c. 6% below SDZ
maximum density figure of 53.0 units/ha

Revised S1 sub-total:
137 units over 2.36Ha = 58.0 units/ha

Revised S2 sub-total:
339 units over 6.83Ha = 49.6 units/ha

Total S1 + S2 figures:
476 units over 9.19Ha = 51.8 units/ha



Proposed Overall Schedule of accommodation - Summary

PROPOSED EXTENT OF APPLICATION "CLEAR REAL ESTATE HOLDINGS LIMITED" LANDS ONLY

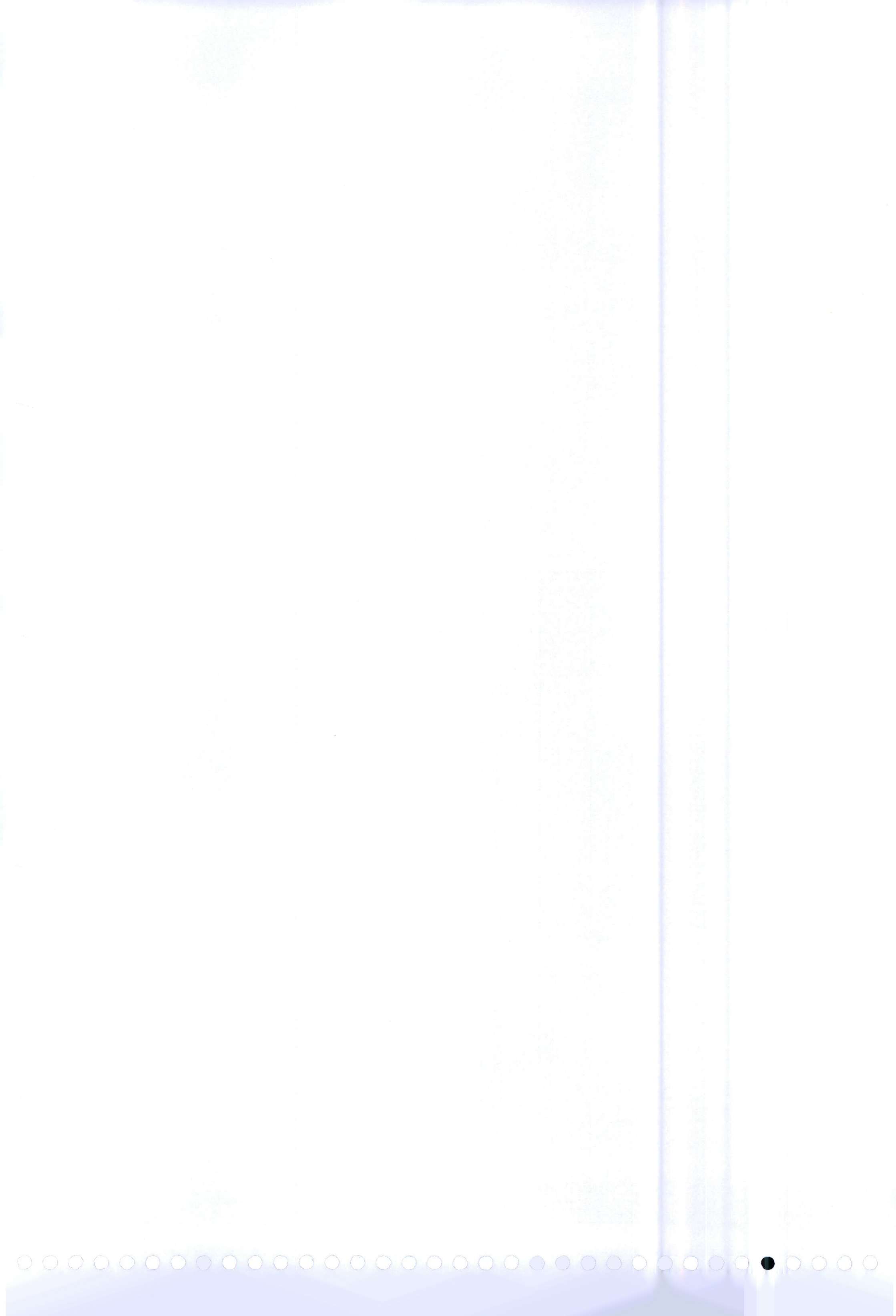
AREA	Ha
OVERALL SITE AREA WITHIN CURRENT APPLICATION RED-LINE BOUNDARY	8.94
NETT DEVELOPMENT AREA *Excl. Avenue / Link St permitted under SDZ20A-0021	7.51
TOTAL NUMBER OF RESIDENTIAL UNITS [PROPOSED]	395
RESIDENTIAL UNITS PER HECTARE [PROPOSED] (OF DEVELOPABLE AREA)	52.6
PUBLIC OPEN SPACE HECTARES [PROPOSED]	1.45

ADAMSTOWN EXTENSION - MASTERPLAN (INCL. PITCH AND PUTT LANDS)

AREA	Ha
OVERALL SITE AREA WITHIN CURRENT APPLICATION RED-LINE BOUNDARY	10.52
NETT DEVELOPMENT AREA *Excl. Avenue / Link St permitted under SDZ20A-0021	9.19
TOTAL NUMBER OF RESIDENTIAL UNITS [PROPOSED]	476
RESIDENTIAL UNITS PER HECTARE [PROPOSED] (OF DEVELOPABLE AREA)	51.8
PUBLIC OPEN SPACE HECTARES [PROPOSED]	2.10

Area character type	Low to medium density development that will take the form of an extension of Adamstown.	
Net development area	9.19ha	
No of units (Target)	442	
Net Density	Sub Sector	Density Range
	AE-S1	45-53
	AE-S2	45-53
<i>* See also Table 2.1.5 for full range of density</i>		

DEVELOPMENT TABLE AS TAKEN FROM SDZ



Requested AI:

1C. The applicant is requested to clarify whether there are any single aspect north facing apartments.

Response:

No north facing single aspect apartments are proposed. All north facing apartment units within apartments buildings are dual aspect. We note that all duplex units within the submitted planning application are proposed as dual aspect. The diagrams below highlights the quantum of units which are proposed across the development with dual aspect or better provision. As a result of the site changes to account for Ai point 1B - the apartment building layout has been adjusted accordingly to suit its new proposed location. This has resulted in a minor reduction in the size of the apartment building and minor adjustments to the internal apartment layouts. Below is an illustration of the quantum of oversized apartment / duplex units which have dual or triple aspects.

c.100% of all duplex units are proposed as dual aspect or better



Diagram showing proposed dual aspect provision for all duplex units

c.46% of all apartment units within Block 1 are proposed as dual aspect or better



Diagram showing proposed dual aspect provision for Apartment Building 01

c.47% of all apartment units within Block 2 are proposed as dual aspect or better



Diagram showing proposed dual aspect provision for Apartment Building 02

195 number units out of the proposed 256 duplex / apartment units are proposed as dual aspect or better. This equates to 76% of the proposed total duplex / apartment units within the development.

Enlarged (oversized) units provision

As a result of the site changes to account for Ai point 1B - the apartment building layout has been adjusted accordingly to suit its new proposed location. This has resulted in a minor reduction in the size of the apartment building and minor adjustments to the internal apartment layouts. Below is an illustration of the quantum of oversized apartment / duplex units (ie. +10% above the minimum apartment recommended floor area), which are proposed within the planning application AI response.

c.96% of all duplex units are proposed as oversized (ie. 10% larger than minimum unit GFA)



Diagram showing proposed enlarged (+10%) provision for all duplex units

c.16% of all apartment units within Block 1 are proposed as oversized (ie. 10% larger than minimum unit GFA)



Diagram showing proposed enlarged (+10%) provision for Apartment Building 01

c.16% of all apartment units within Block 2 are proposed as oversized (ie. 10% larger than minimum unit GFA)



Diagram showing proposed enlarged (+10%) provision for Apartment Building 02

154 number units out of the proposed 256 duplex / apartment units are proposed as oversized (ie. 10% larger than the minimum GFA). This equates to 60% of the proposed total duplex / apartment units within the development.





Proposed Cycling Routes

Requested AI:

1D. The scheme indicates a strategic cycle route to the north of the local street at the north of the site. It is not apparent that this has been included in the proposed layout and the applicant is requested to provide a revised layout indicating the cycle route.



Area of Works permitted under 'SDZ 20A-0021'

-  Extent of dedicated cycle/pedestrian routes (as per SDZ)
-  Proposed cycle route connections through proposed development
-  Spur can be provided to allow potential for future ped/cycle connection
-  Location of proposed dedicated cycle facilities

Response:

As illustrated within the planning application dedicated cycle-route have been planned across the site in accordance with the SDZ. To the north of the proposed application area - the pedestrian and cycle route runs parallel to the train-line - linking from the pedestrian and cycle bridge over the railway (north-east of site) to the Adamstown Rd (N52). We note that the SDZ illustrates a connection to the N52 to the north-east of the site which is not feasible to provide due to the c. 6 metre change of level from the existing green field level up to the N52 bridge level.

In lieu of a connect at this point - a more gently sloping and cycle friendly route has been proposed - running along the eastern side of the apartment buildings - linking the cycle route to the proposed cycle facilities for the apartments, amenity pavilion and providing a safe connection to the Avenue / Link St.

A spur connection can be provided in the north-west corner of the site to facilitate any potential future connection by others. We note that the landscaping works proposed to the northern portion of the site do not preclude the potential for any future connections in this area.

Proposed changes to streets to address RFI queries

Requested AI:

1E. Intimate Local Street 2:
South end of street widens to 22.6m between frontages with perpendicular parking. The applicant is requested to redesign this portion of the street to ensure in keeping with scheme road layout and requirements for intimate local streets.

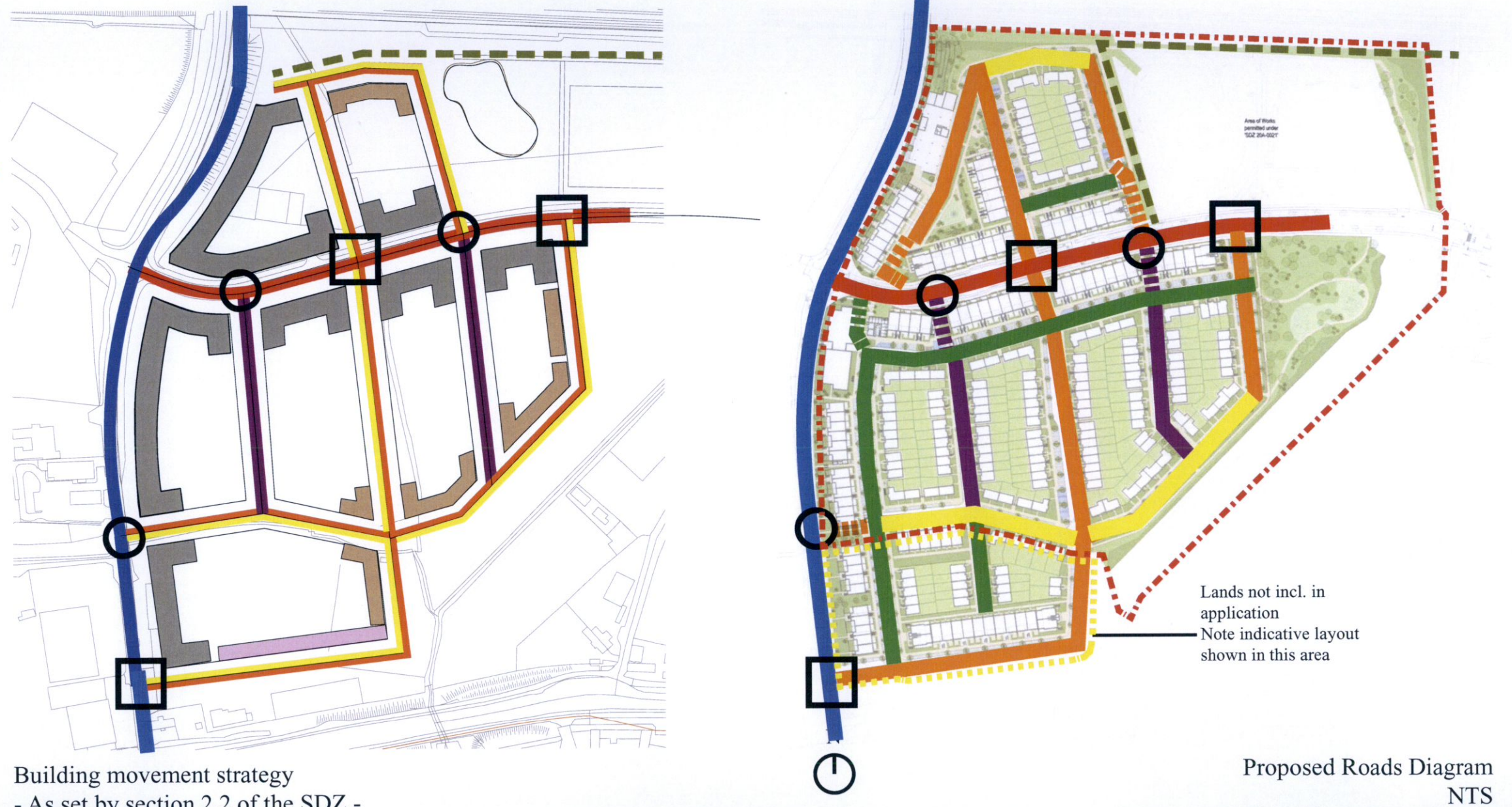
2.2 The applicant is requested to revise plans to include additional street trees on local street 2,3,9,7. Currently only one side of street is planted.










Response:
In order to address points raised under RFI sections 1E and 2.2 - a number of changes have been proposed to street sections and street types across the plan. As requested additional street streets have been added to local streets 2, 3, 7 & 9. To achieve this the street profile has been adjusted to further match the SDZ. Local street 2 has been redesigned to use a Local Street Type B profile - which allows further street trees to be provided within the plan.

Intimate street 2 has been redesigned in line with the requirements of the SDZ street diagrams - along its full length.

Each of these streets are described in further detail on pages 7 - 11

Please note that there is no change to the total number of parking spaces as part of the revision to street types and addition of street trees. Please see parking schedules which show the total numbers on page 24.



- | | | | | | |
|---|--------------------------------------|---|-------------------------|---|--------------------------|
|  | Vehicular Priority Junction |  | Existing Arterial Route |  | Intimate Street |
|  | Pedestrian / Cycle Priority Junction |  | Avenue / Link Street |  | New Neighbourhood Street |
| | |  | Local Street - Type A |  | Northern Bicycle Route |
| | |  | Local Street - Type B | | |

Zoomed-in plan showing street as submitted for planning permission



Zoomed-in plan showing revised street as part of RFI response



Requested AI:

1E. Intimate Local Street 2:

South end of street widens to 22.6m between frontages with perpendicular parking. The applicant is requested to redesign this portion of the street to ensure in keeping with scheme road layout and requirements for intimate local streets.

Response:

The south end of Intimate Street 2 has been amended to remove perpendicular parking - and now shows parallel parking to one side of the street only. The street profile in this location now matches the intimate street requirements set out in the SDZ. The positions of houses along this section of intimate street have been adjusted to ensure the maximum separation distances are not exceeded.

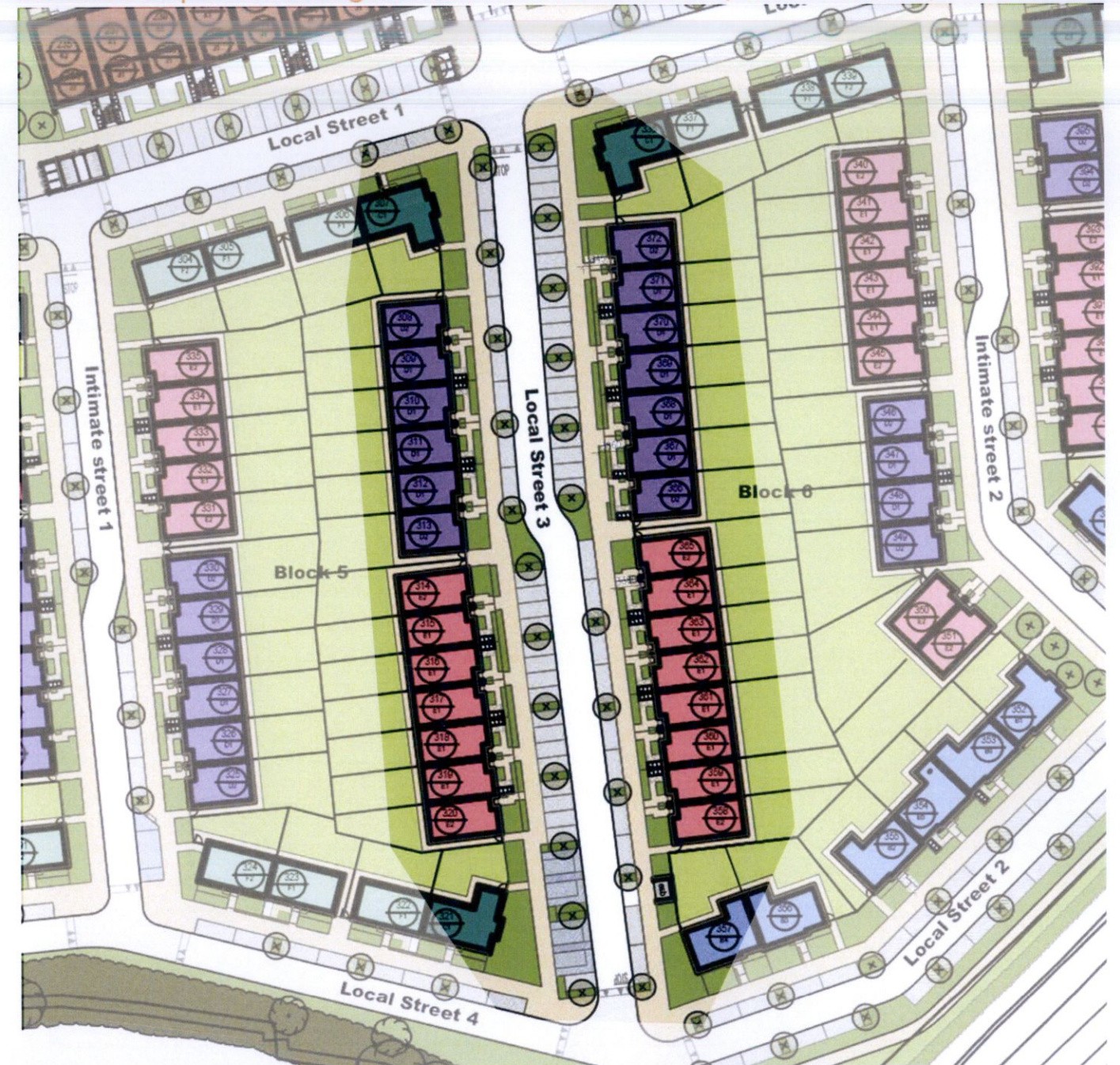


Proposed Changes to local street 3

Zoomed-in plan showing Local Streets as submitted for planning permission



Zoomed-in plan showing revised Local Streets as part of RFI response



Requested AI:

2.2. The applicant is requested to revise plans to include additional street trees on local street 2,3,9,7. Currently only one side of street is planted.

Response:

The street profile of Local street 3 has been redesigned to align with SDZ street diagrams. The footpath location has been amended to remove parking previously shown as in-curtilage now shown as within zone to be taken in charge. Additional street trees have now been added to taken in charge areas in accordance with SDZ diagram. Additionally, parking along street and footpath width adjusted to match position of street trees



Zoomed-in plan showing Local Streets as submitted for planning permission



Zoomed-in plan showing revised Local Streets as part of RFI response



Requested AI:

2.2. The applicant is requested to revise plans to include additional street trees on local street 2,3,9,7. Currently only one side of street is planted.

Response:

The street profile of Local street 2 has been redesigned to align with SDZ street diagrams. The footpath location has been amended to remove parking previously shown as in-curtilage now shown as within zone to be taken in charge. Perpendicular parking has now been replaced with parallel parking - in line with the SDZ defined Local Street type B. Additional street trees have now been added to taken in charge areas in accordance with SDZ diagram.

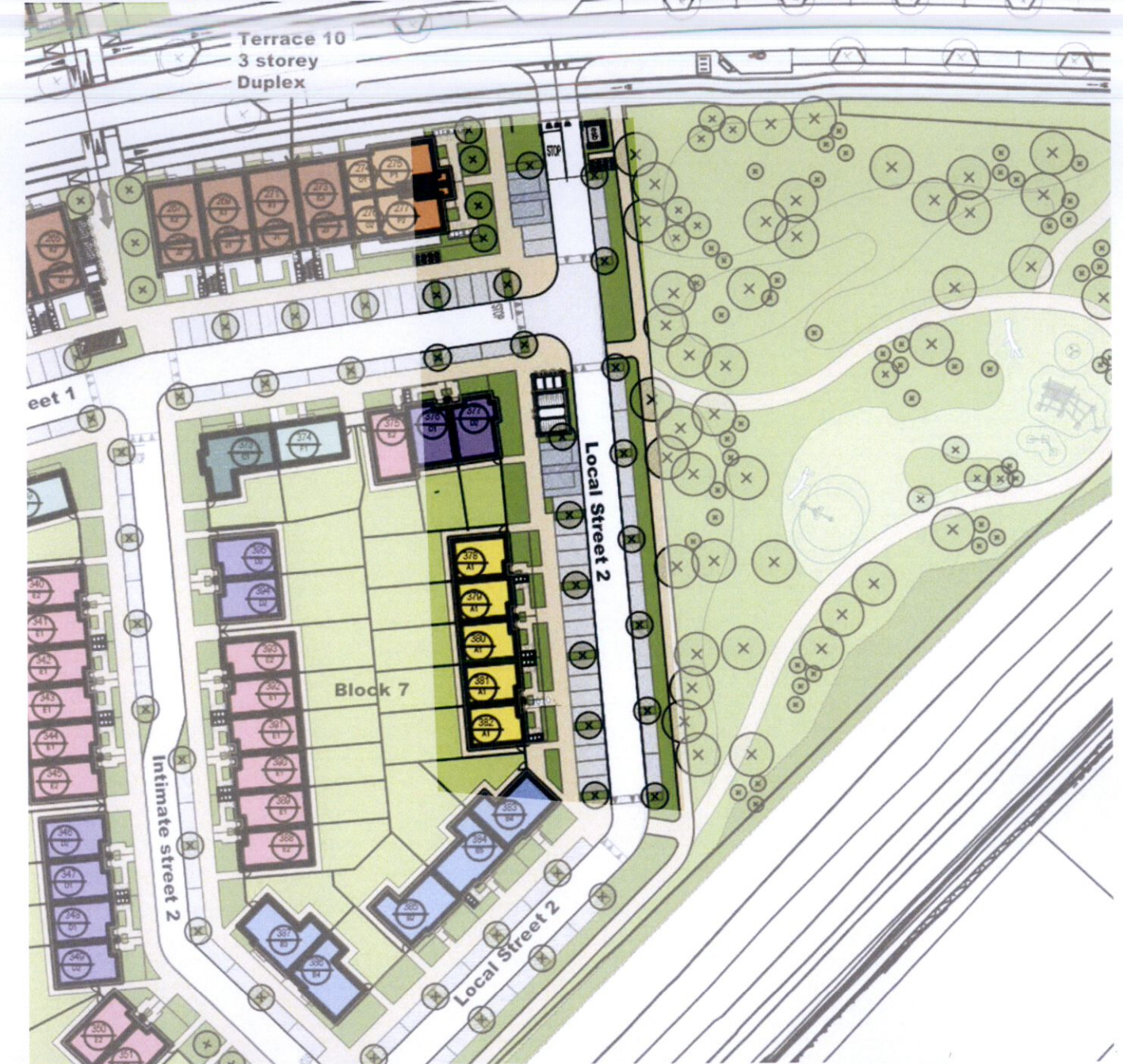


Proposed Changes to northern end of local street 2

Zoomed-in plan showing Local Streets as submitted for planning permission



Zoomed-in plan showing revised Local Streets as part of RFI response



Requested AI:

2.2. The applicant is requested to revise plans to include additional street trees on local street 2,3,9,7. Currently only one side of street is planted.

Response:

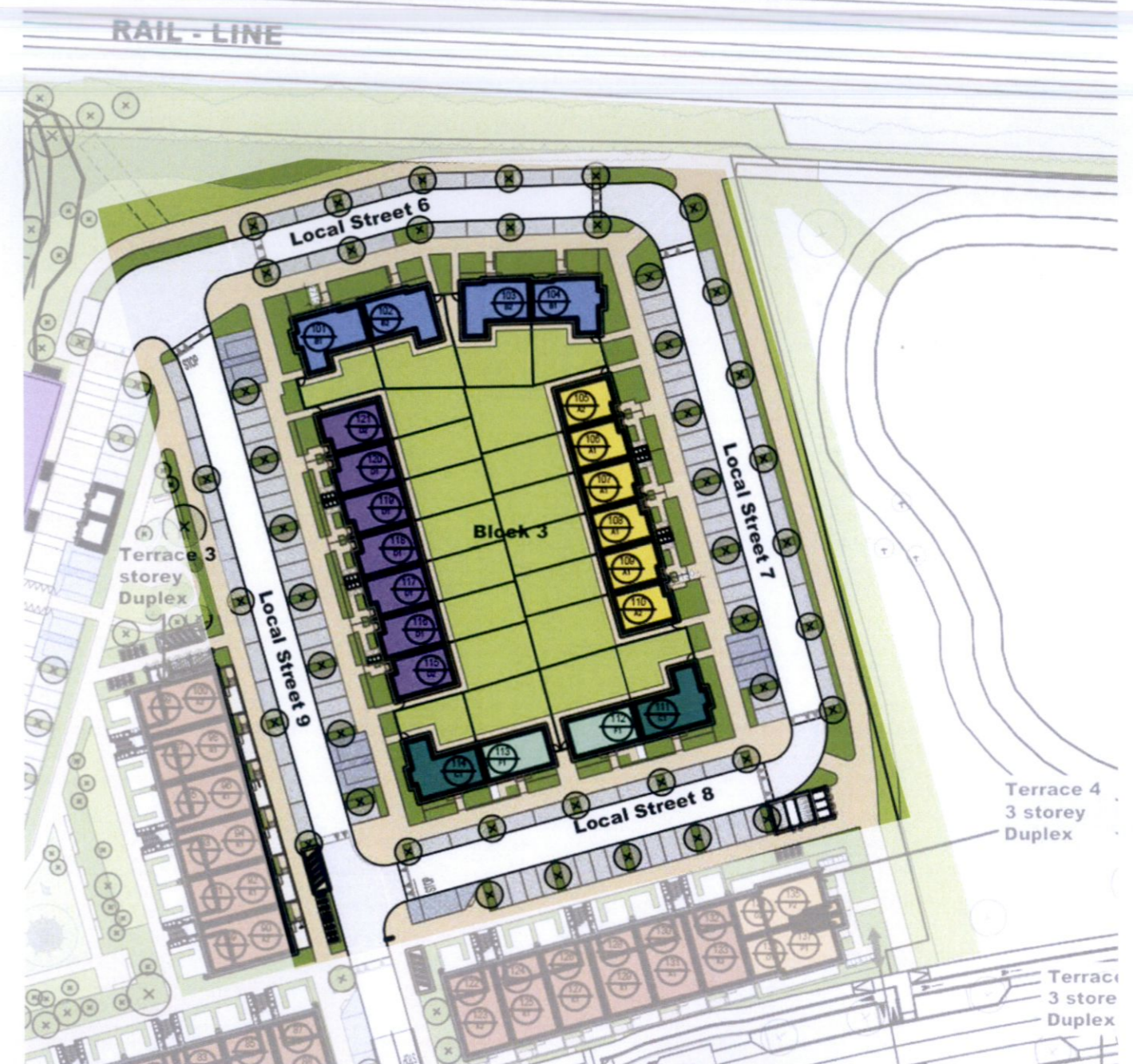
This portion of local street 2 uses a local street type A as defined within the SDZ street diagrams. The footpath location has been amended to remove parking previously shown as in-curtilage now shown as within zone to be taken in charge. Additional street trees have now been added to taken in charge areas in accordance with SDZ diagram.



Zoomed-in plan showing Local Streets as submitted for planning permission



Zoomed-in plan showing revised Local Streets as part of RFI response



Requested AI:

2.2. The applicant is requested to revise plans to include additional street trees on local street 2,3,9,7. Currently only one side of street is planted.

Response:

The street profile of Local street 6,7 & 9 has been redesigned to align with SDZ street diagrams. The footpath location has been amended to remove parking previously shown as in-curtilage now shown as within zone to be taken in charge. Additional street trees have now been added to taken in charge areas in accordance with SDZ diagram. Additionally, parking along street and footpath width adjusted to match position of street trees.

See Street section diagrams for further detail.

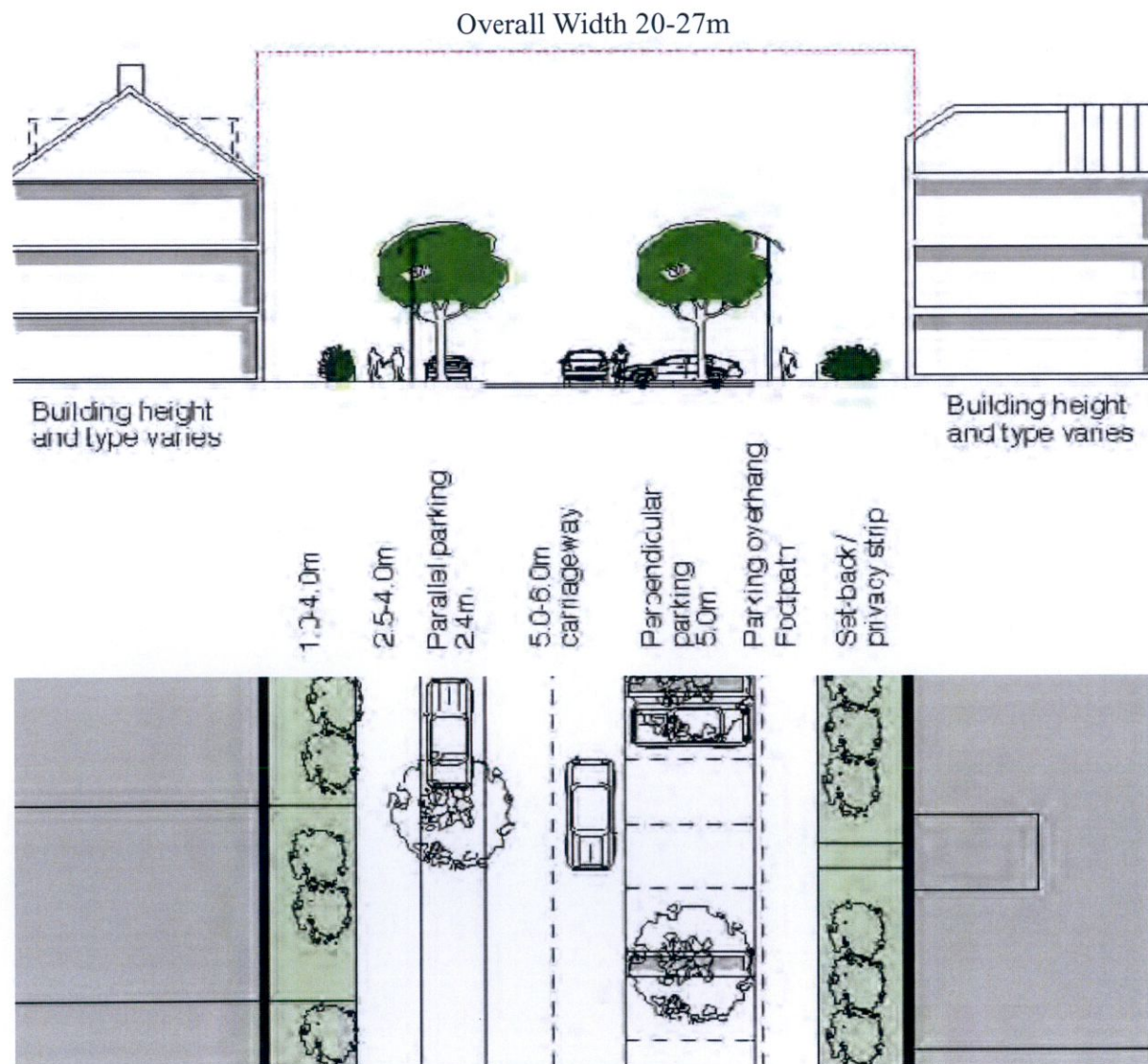


Local street - type A - comparison with SDZ

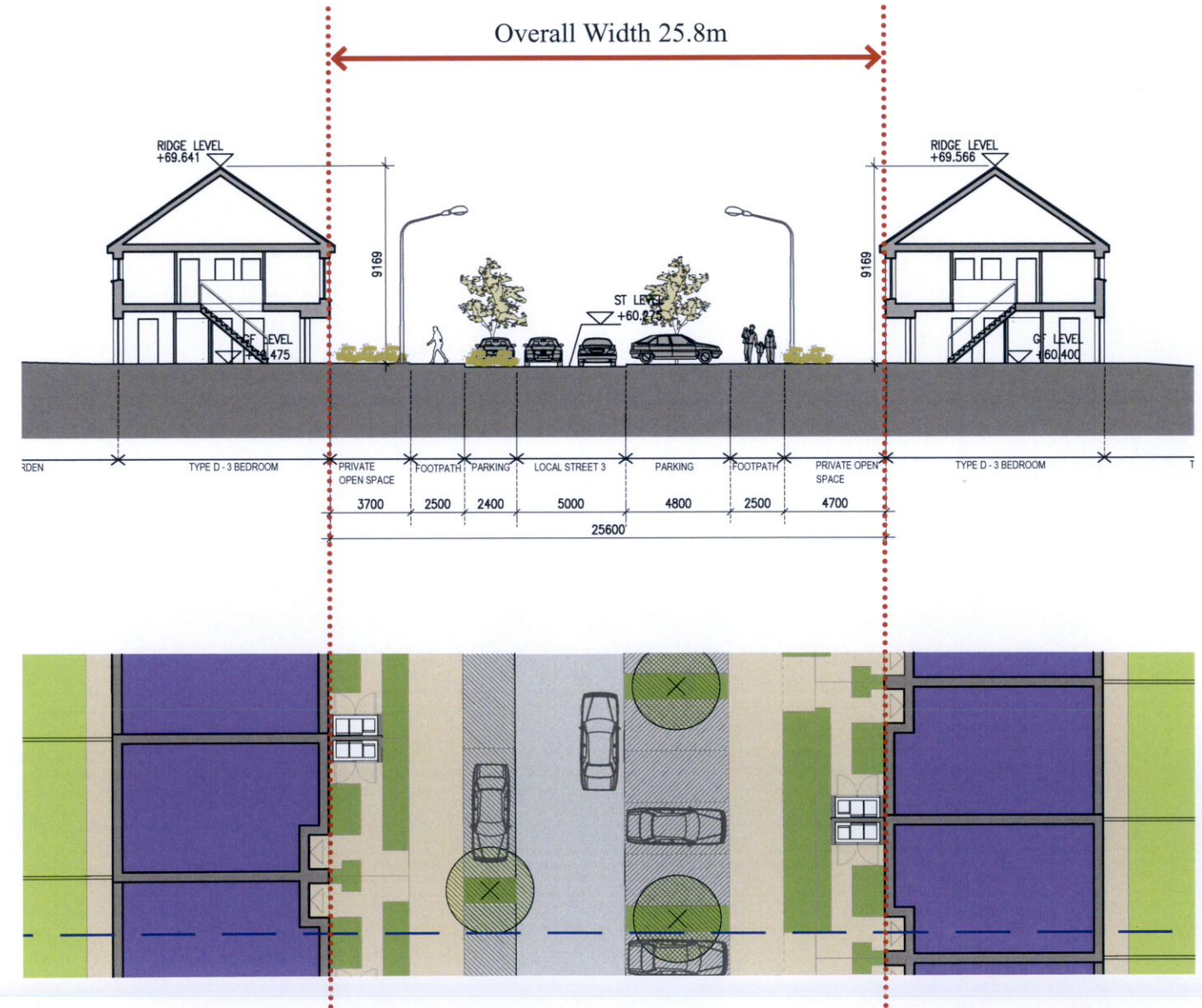
The proposed Local Street A has been set out in accordance with the requirements of the SDZ. Building separation distances have been set to ensure the SDZ maximum figures are not exceeded. Defensible zones, front gardens, sloped access routes to front doors and bin locations have been designed to be fully accommodated within the zone between building and footpath. To address queries raised in the RFI - parking previously shown as in-curtilage has now been showed in a managed arrangement - which has allowed additional street trees to be added to the layouts.



Indicative Local Street A [as shown in SDZ]



Proposed Local Street A

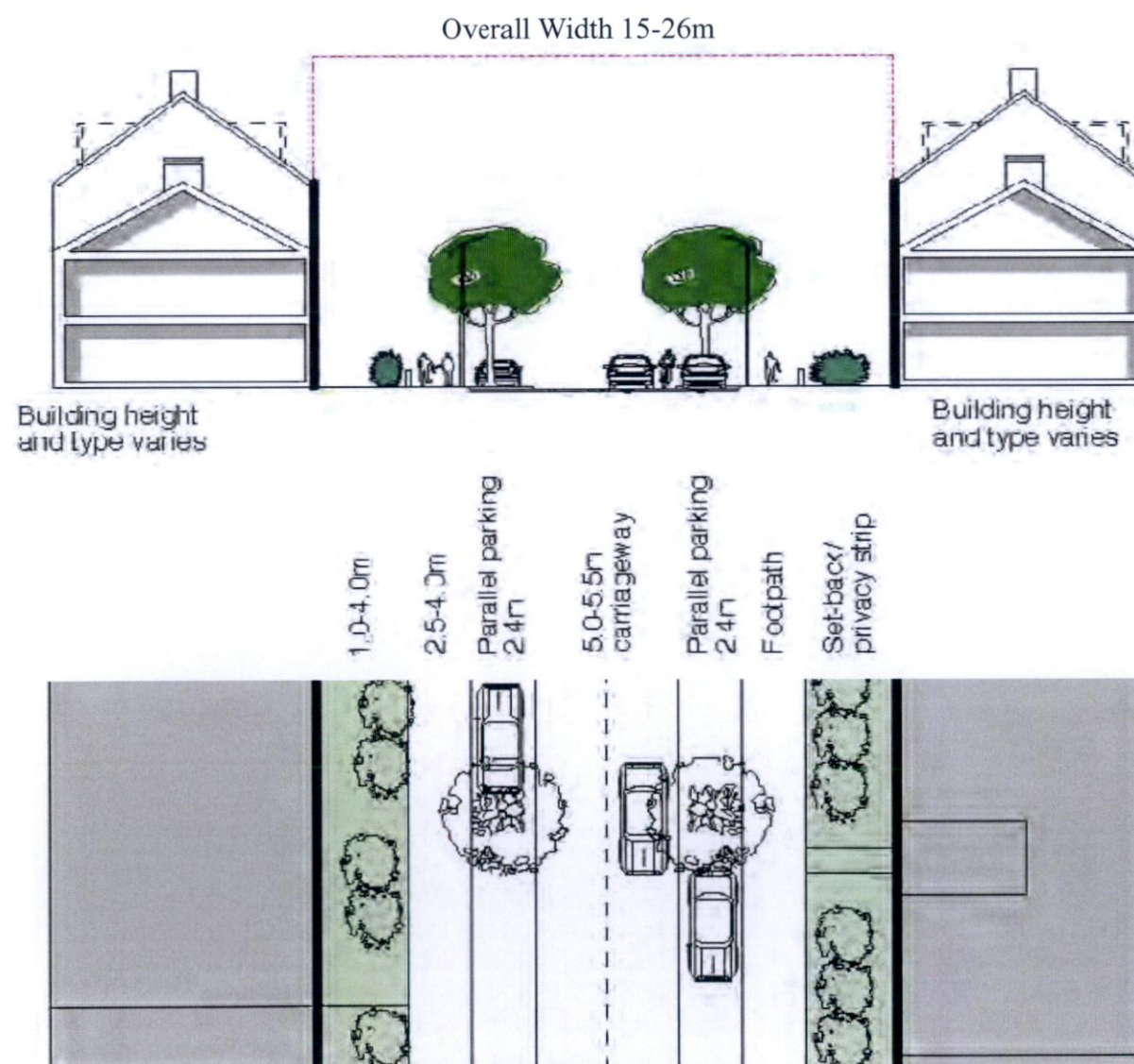


Local street - type B - comparison with SDZ

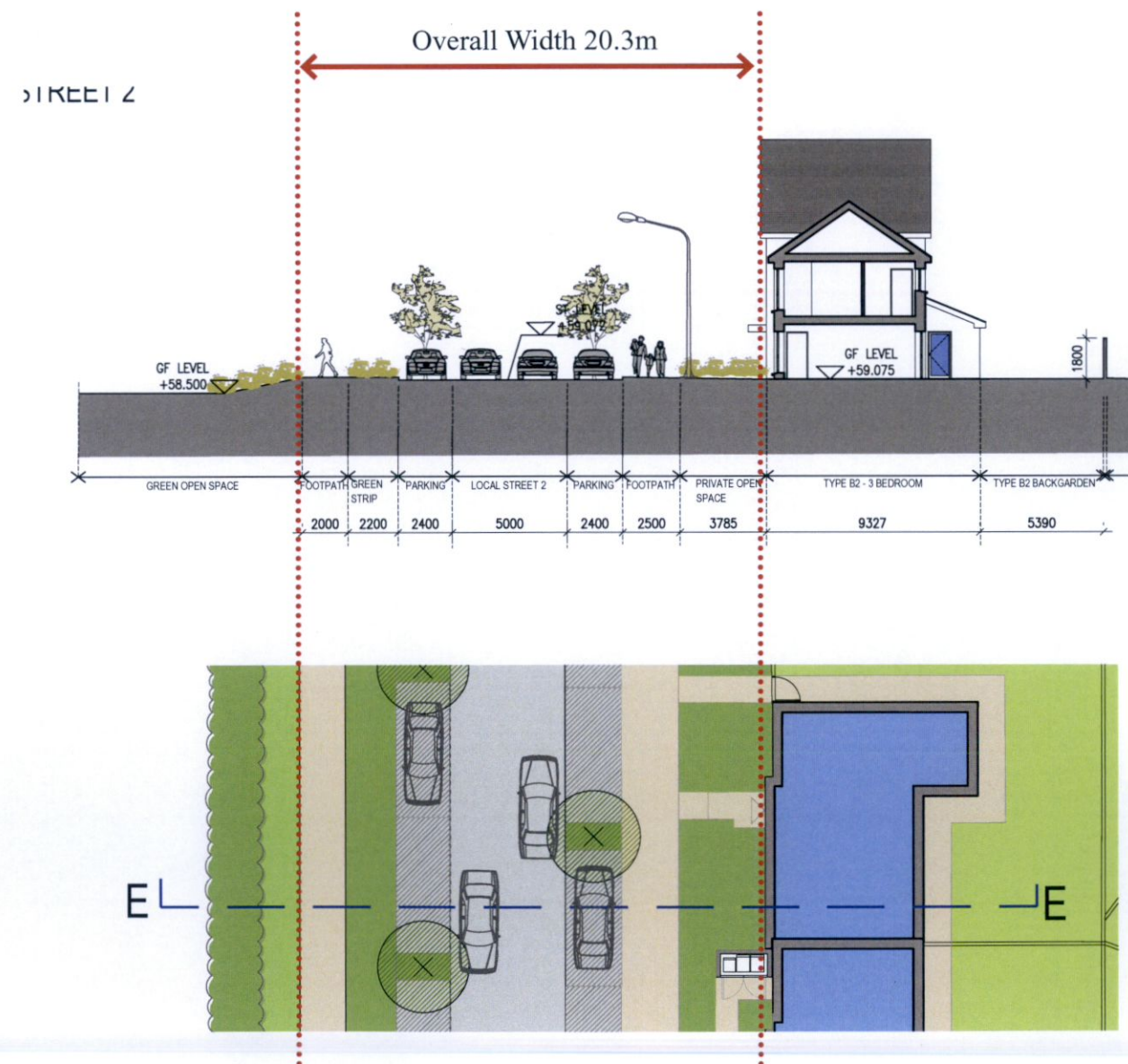
A small proportion of roads within the development which were previously shown as a local street type A condition have now been revised to use a local street type B condition. To address queries raised in the RFI - parking previously shown as in-curtilage has now been shown in a managed arrangement - which has allowed additional street trees to be added to the layouts.



Indicative Local Street B [as shown in SDZ]



Proposed Local Street B



Intimate street - comparison with SDZ



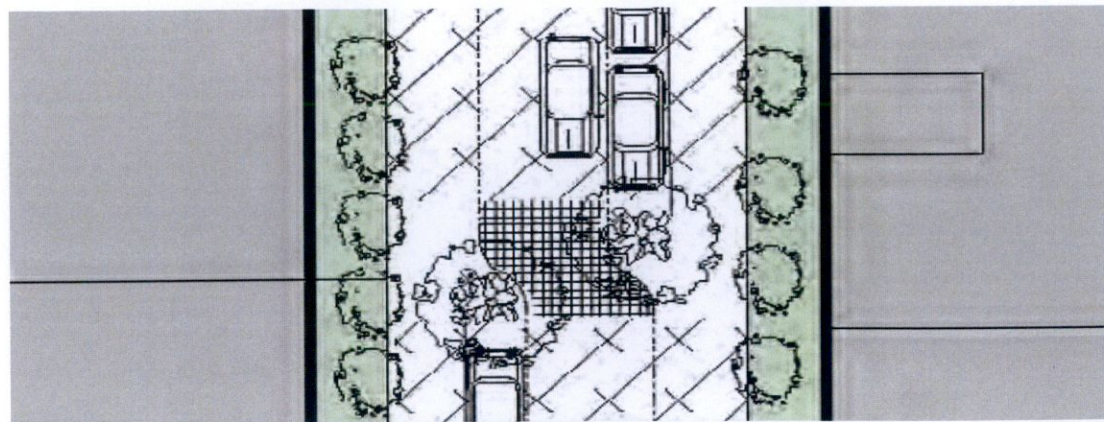
Indicative Intimate Street
[As shown in SDZ]

Overall Width 15-20m



Building height
and type varies

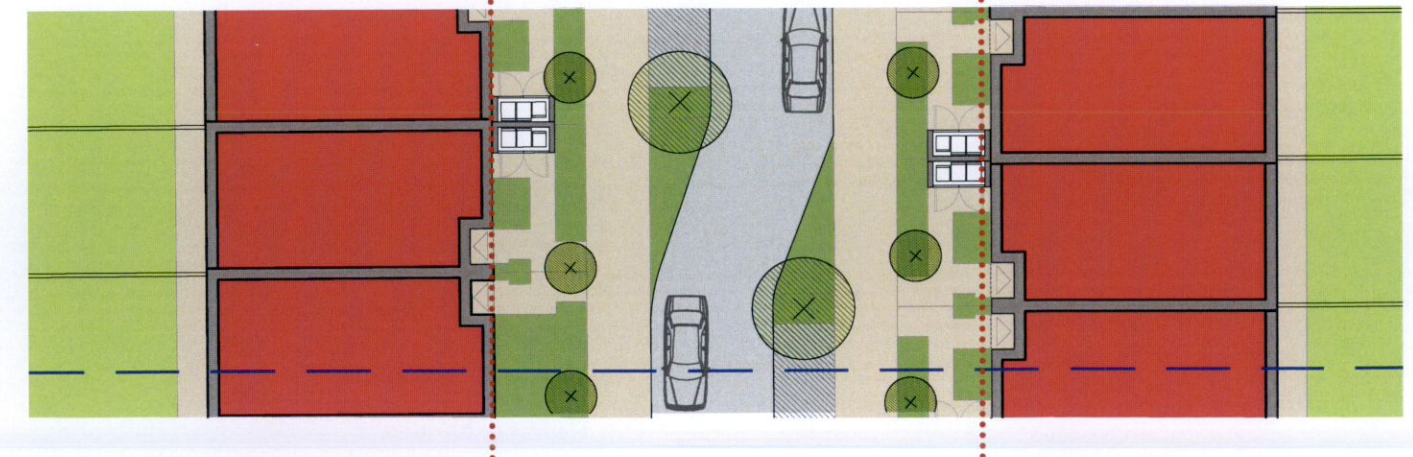
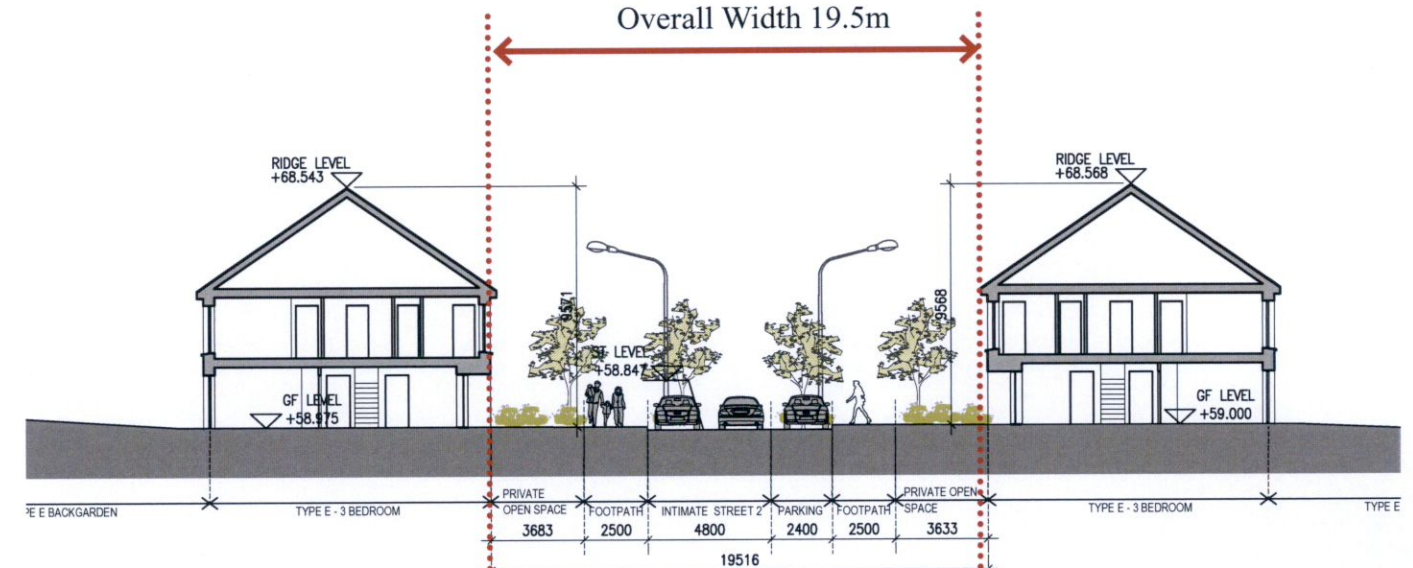
- Set-back/
privacy strip
- Shared surface
carriageway
<4.8m
- Parallel parking
2.4m
- Footpath
- Set-back/
privacy strip



No change has been proposed to the intimate street layout - as this street section was fully in compliance with the SDZ street diagrams at the time of planning application submission. We note that Intimate street 2 now uses this same street profile from start to finish.

Proposed Intimate Street

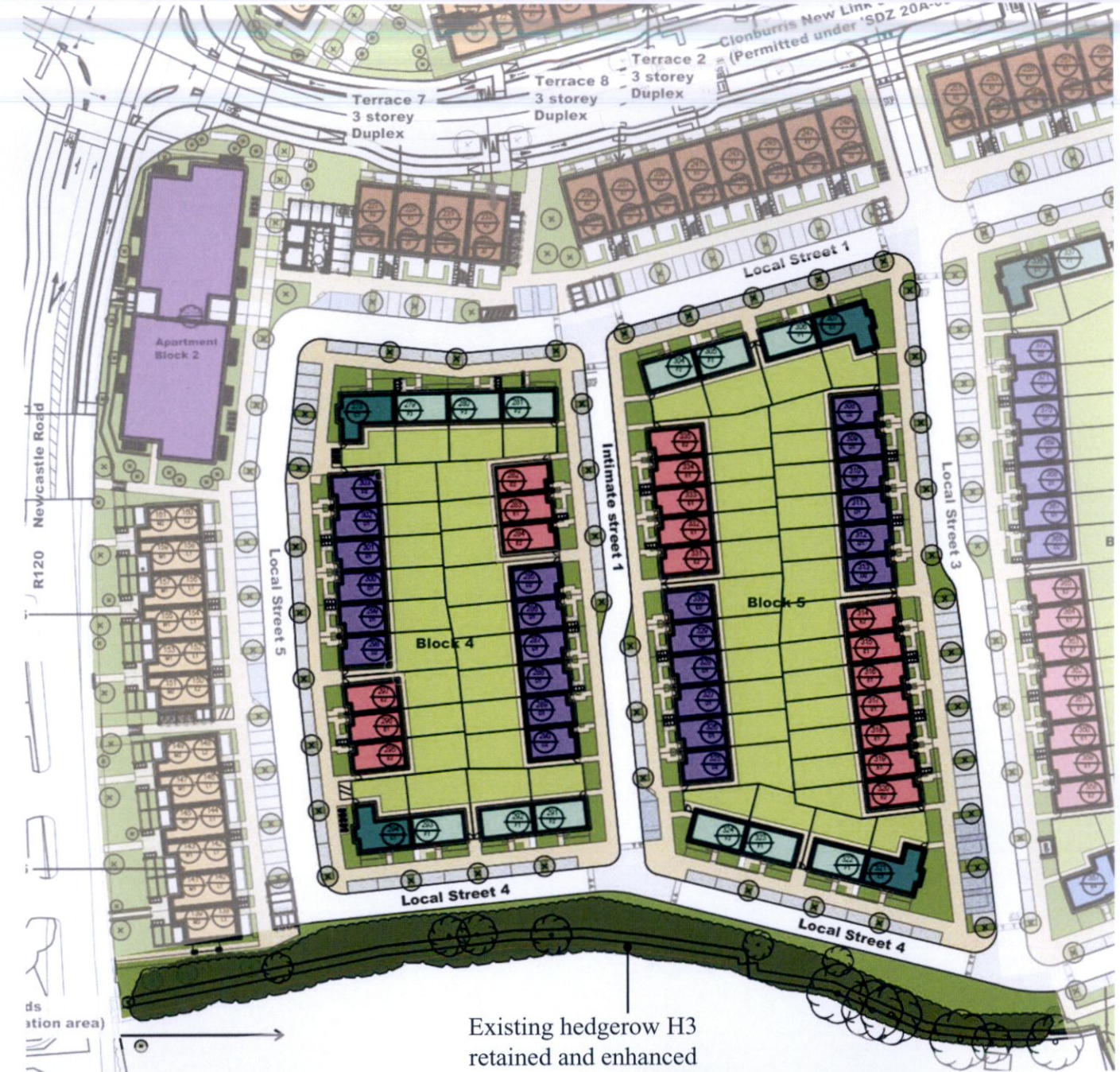
Overall Width 19.5m



Site Plan - as per original application submission



Revised Site Plan - to address RFI request



Requested AI:

2.1. Much of the hedgerow removal is considered unavoidable in order to deliver this development however, removal of Hedgerow H3 along with trees along the southern boundary is considered excessive and damaging to the local green infrastructure. The applicant is requested to provide revised plans to include the retention /enhancement of this hedgerow.

Response:

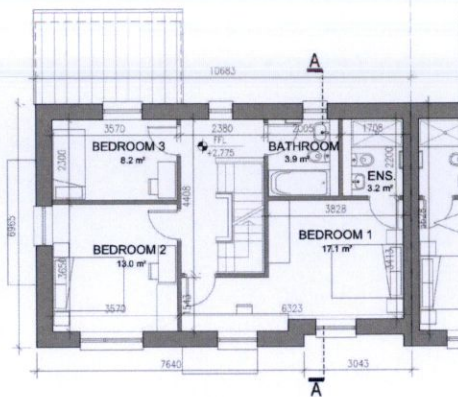
The local street 4 which runs adjacent to hedgerow H3 has now been relocated further to the north to allow the full extent of this hedgerow to be retained. A number of houses have been relocated within blocks 4 & 5, additionally houses 271 & 294 have been changed from a C Type house to an F type house - in order to accommodate additional green space south of Local Street 4 - needed to retain this section of hedgerow. Previously proposed fences & ball nets are now removed - the full extent of the hedgerow retained with enhancement measures proposed by the landscape architect.



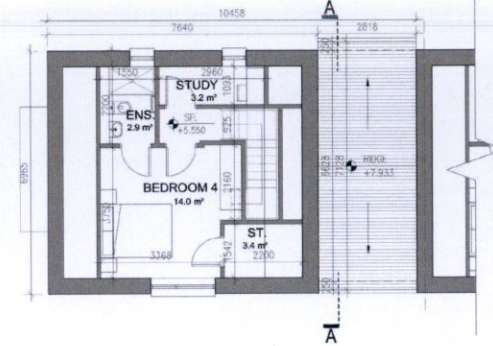
Increased dual frontage - Unit Type B1



01 Ground Floor Plan



02 First Floor Plan



03 Second Floor Plan

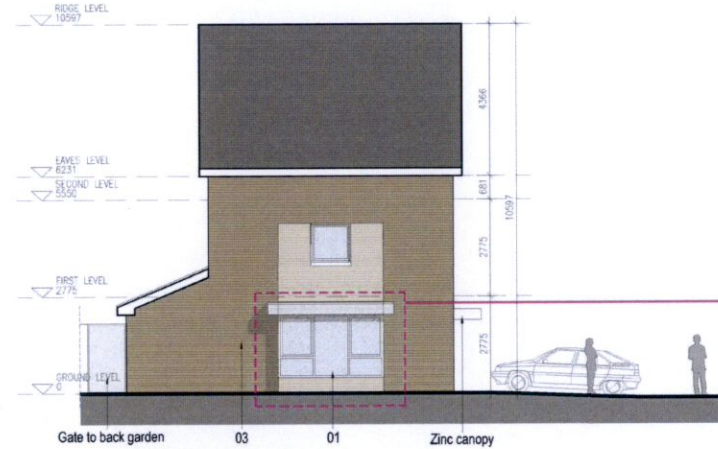
HOUSE TYPE B1	
Ground floor area	69.6m ²
First floor area	59.6m ²
Second floor area	28.1m ²
Total Floor Area (4-bed unit):	157.3m²



Indicates unit locations
KEY PLAN [NTS]



04 Front Elevation



05 Side Elevation

PROPOSED CHANGE



06 Contiguous Elevation

Requested AI:

1F. The applicant is requested to provide revised elevations, increasing dual frontage recommended for the following units: - B1 (handed), B4 (handed), B4 (non handed), C1 (handed), C1 (non handed), F2 (handed), F2 (non handed)

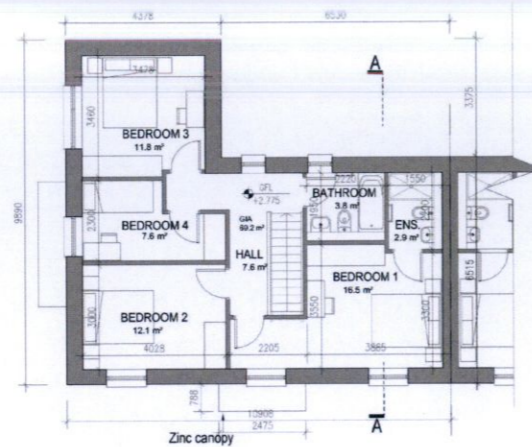
Response:

All end units with large gable elevations have been reviewed. At all key prominent locations where these gables are visible, the end elevations have been revised to include additional windows to further increase the dual frontage provision from what was shown in the original application.

Please see plans and elevations shown in context for house type B1. Please note that the handed and non-handed units shown the same quantum of windows to the these elevations.



01 Ground Floor Plan



02 First Floor Plan

HOUSE TYPE C1	
Ground floor area	71.6m²
First floor area	69.2m²
Total Floor Area (3-bed unit):	140.7m²



Indicates unit locations
KEY PLAN [NTS]

Requested AI:

1F. The applicant is requested to provide revised elevations, increasing dual frontage recommended for the following units: - B1 (handed), B4 (handed), B4 (non handed), C1 (handed), C1 (non handed), F2 (handed), F2 (non handed)

Response:

Please see plans and elevations shown in context for house type C1. We note that the layout as submitted includes multiple windows on the front & gable elevations to ensure dual frontages are provided - this has been further added to with the inclusion of an additional bay window on the ended elevation. Please note that the handed and non-handed units shown the same quantum of windows to these elevations.

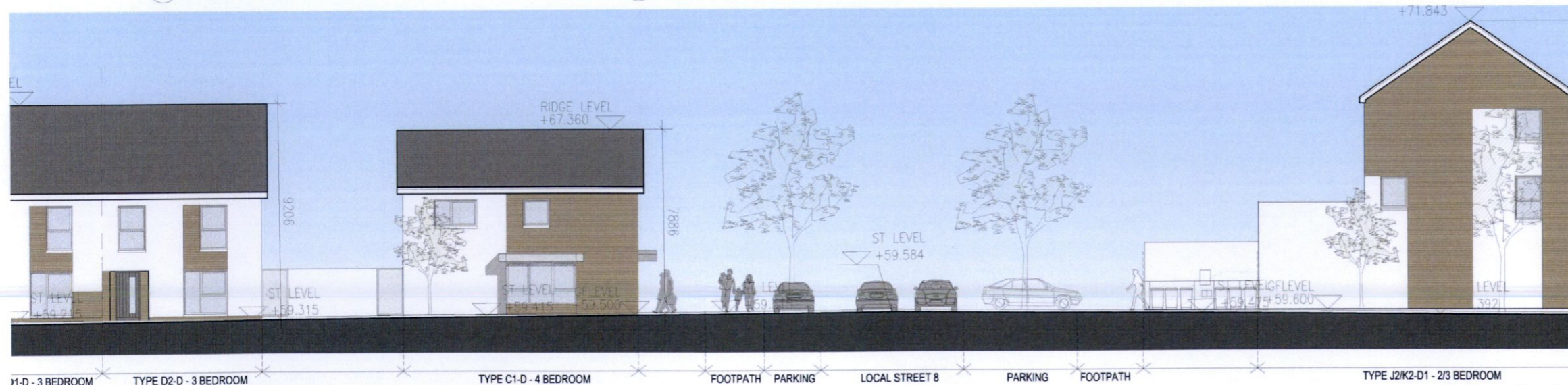


03 Front Elevation



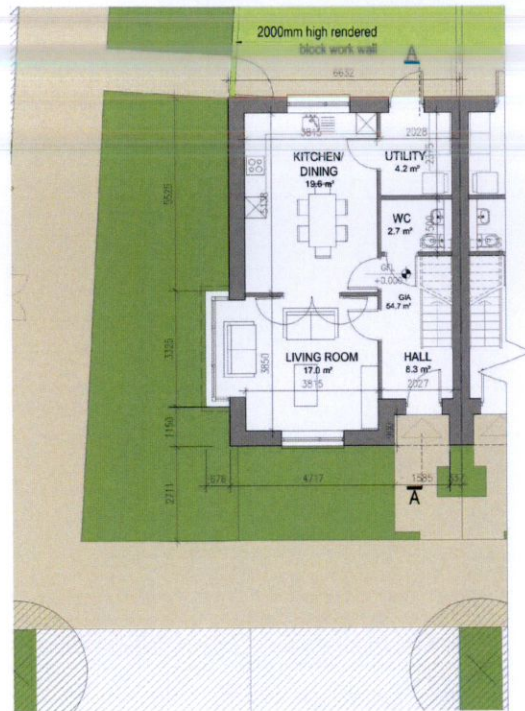
04 Side Elevation

PROPOSED CHANGE

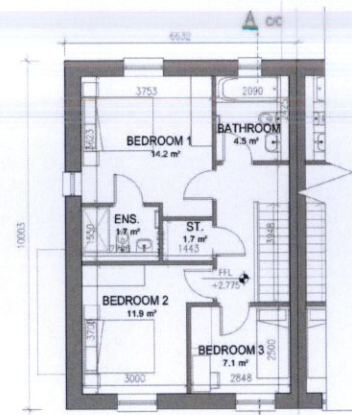


05 Contiguous Elevation

Increased dual frontage - Unit Type D2A

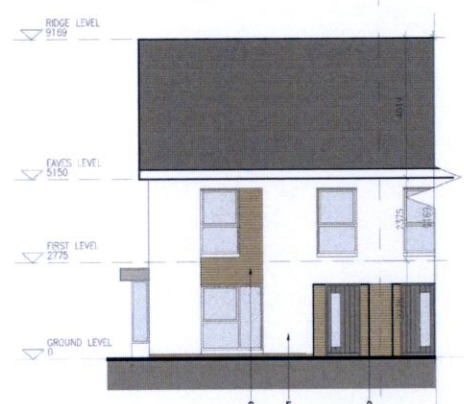


01 Ground Floor Plan



02 First Floor Plan

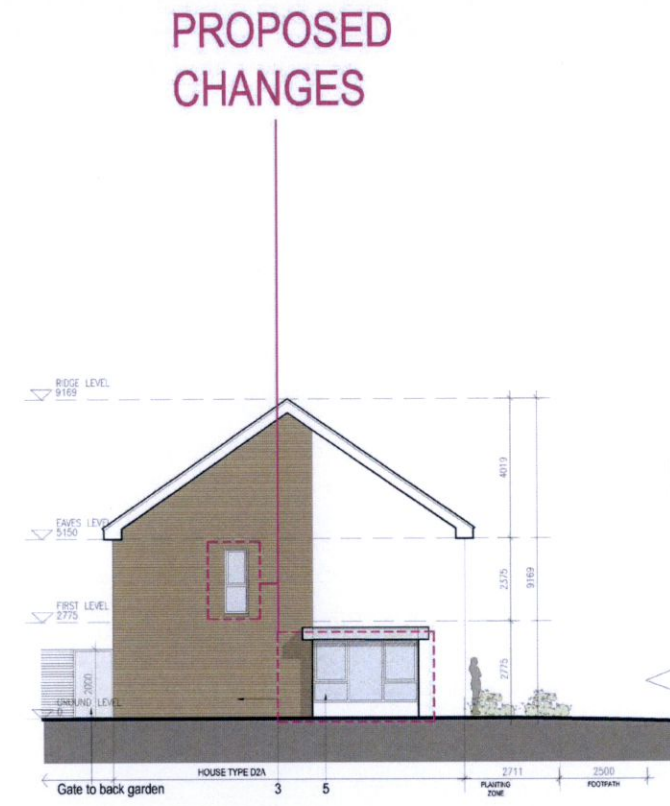
HOUSE TYPE D2A	
Ground floor area	54.7m ²
First floor area	54.2m ²
Total Floor Area (3-bed unit)	108.9m²



03 Front Elevation Type D2A



04 Rear Elevation



05 Side Elevation Type D2A

PROPOSED CHANGES



Indicates unit locations
KEY PLAN [NTS]

Requested AI:

1F. The applicant is requested to provide revised elevations, increasing dual frontage recommended for the following units: - B1 (handed), B4 (handed), B4 (non handed), C1 (handed), C1 (non handed), F2 (handed), F2 (non handed)

Response:

Please see plans and elevations shown in context for house type D2. We note that this particular unit - which only occurs a one location on the plan was shown in error without gable windows to active the eastern elevation. Windows are now shown to this elevation to ensure passive surveillance of the bicycle storage areas adjacent to this residential unit.



06 Contiguous Elevation

TYPE A2-D - 4 BEDROOM TYPE A1-D - 4 BEDROOM TYPE A1-L - 4 BEDROOM TYPE A1-L - 4 BEDROOM TYPE A2-D - 4 BEDROOM TYPE D2-A - 3 BEDROOM FOOTPATH PARKING LOCAL STREET

Western elevation to apartment building 2



**Residential
Ground Floor
Level
62.800m OD**

Requested AI:

1G. The applicant is requested to consider whether own door units can be provided within the apartments.

Response:

Due to the significant falling street levels along the R120 from the top of the bridge to the entrance to the Avenue / Link st - providing own door access to the proposed apartment units is not feasible in the majority of units.

At present a carefully planned soft landscaping buffer has been provided along the R120 for the length of the apartment building - to mitigate against road noise and its impact on the apartment occupants. The change of levels shown are accommodated within this landscaping zone. The buffer zone between the back of footpath and edge of building line also accommodates a series of stepped planters which act as retaining walls to support the change in ground levels and support the nearby road substrate.

Own door access provided onto street for all duplex units onto R120

All Duplex buildings located along the R120 have own door access onto the street - to provide animation and activity along this key boundary.

Approx. 500mm change of level over 3900mm. (1 in 8 slope - not advisable under building regulations)

Approx. 1000mm change of level over 6100mm. (1 in 6 slope - not advisable under building regulations)

Approx. 800mm change of level over 4300mm. (1 in 5 slope - not advisable under building regulations)

Approx. 1400mm change of level over 6300mm. (1 in 9 slope - not advisable under building regulations)

Plant rooms located within this area due to level difference building floor level and footpath outside.



These comments illustrate the height difference between the footpath level and the proposed floor level of the lower level apartments which address the R120.

Own door access to ground floor apartments

Western elevation to apartment building 1 - as submitted for planning permission



Requested AI:

1G. The applicant is requested to consider whether own door units can be provided within the apartments.

Response continued:
As per apartment block 2, a carefully planned soft landscaping buffer has been provided along the R120 for the length of the apartment building - to mitigate against road noise and its impact on the apartment occupants.

Approx. 260mm change of level over 4300mm.
(1 in 16 slope - not advisable under building regulations)

Approx. 360mm change of level over 4300mm.
(1 in 12 slope - not advisable under building regulations)

Approx. 440mm change of level over 4300mm.
(1 in 9 slope - not advisable under building regulations)

Approx. 920mm change of level over 4300mm.
(1 in 5 slope - not advisable under building regulations)

Approx. 980mm change of level over 4300mm.
(1 in 4 slope - not advisable under building regulations)

Approx. 1040mm change of level over 4300mm.
(1 in 4 slope - not advisable under building regulations)

The change of levels shown are accommodated within this landscaping zone. In this apartment block, the buffer zone includes planters and car designed ventilation opes.

In order to service this apartment building, car parking and plant spaces are provided out of sight underneath the building - utilizing the contours of the site to semi-submerge the ground floor parking level.

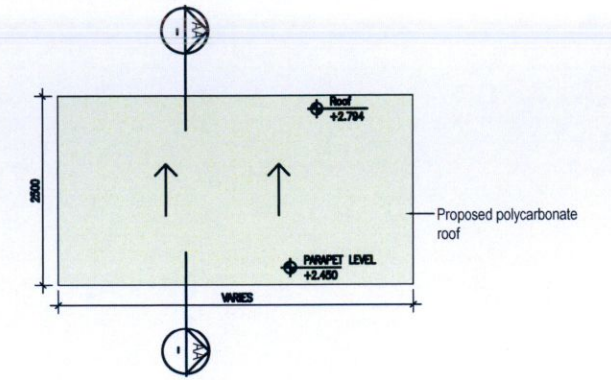
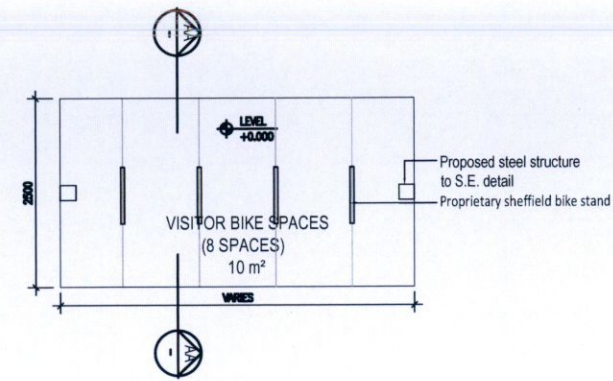
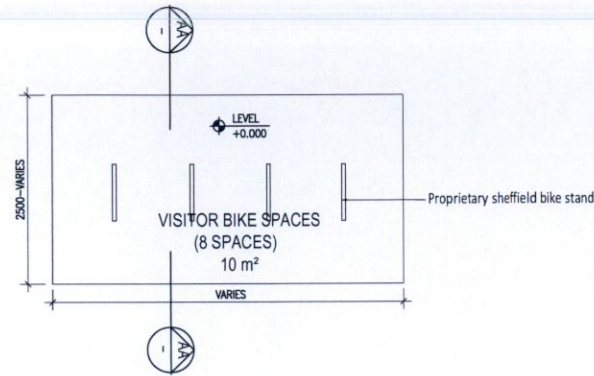
All Duplex buildings located along the R120 have own door access onto the street - to provide animation and activity along this key boundary.



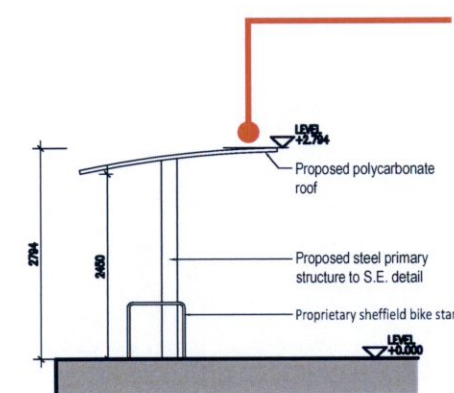
These comments illustrate the height difference between the footpath level and the proposed floor level of the lower level apartments which address the R120.

Typical visitor bicycle parking as submitted

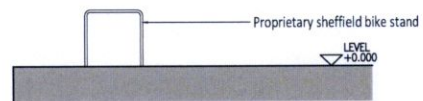
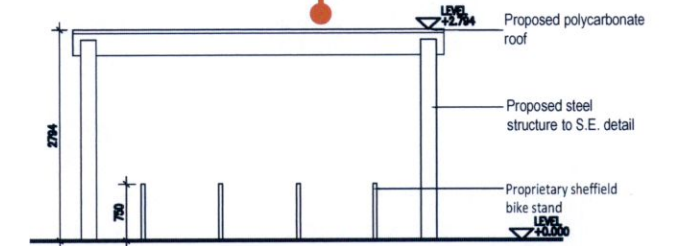
Revised typical visitor bicycle parking - to address RFI request



01 Proposed Visitor Bicycle Shelter Type 4 - Ground Plan
P-702 1:500A1



Canopy added over



02 Proposed Visitor Bicycle Shelter Type 4 - Section
P-702 1:500A1

03 Proposed Visitor Bicycle Shelter Type 4 - Elevation
P-702 1:500A1

Requested AI:

3.3 The applicant to submit revised layout showing visitor bicycle parking covered in line with the National Cycle Manual.

Response:

Please see above updated drawing which now includes a roof for all visitor bicycle parking areas.

Revised parking Plan showing EV parking space locations



Requested AI:

3.2 2. EV car-parking provision must increase to a minimum of 108 no.. spaces in line with min 20% requirement in CDP 2022-28.

Response continued:

The drawing shown to the left illustrates how the 20% EV parking space and universal accessible spaces have been evenly distributed across the proposed application area. In-curtilage parking is no longer proposed within the application - all parking spaces are now shown as managed.

Appendices

Revised Diagrams and CGI images
to demonstrate compliance with SDZ
requirements.

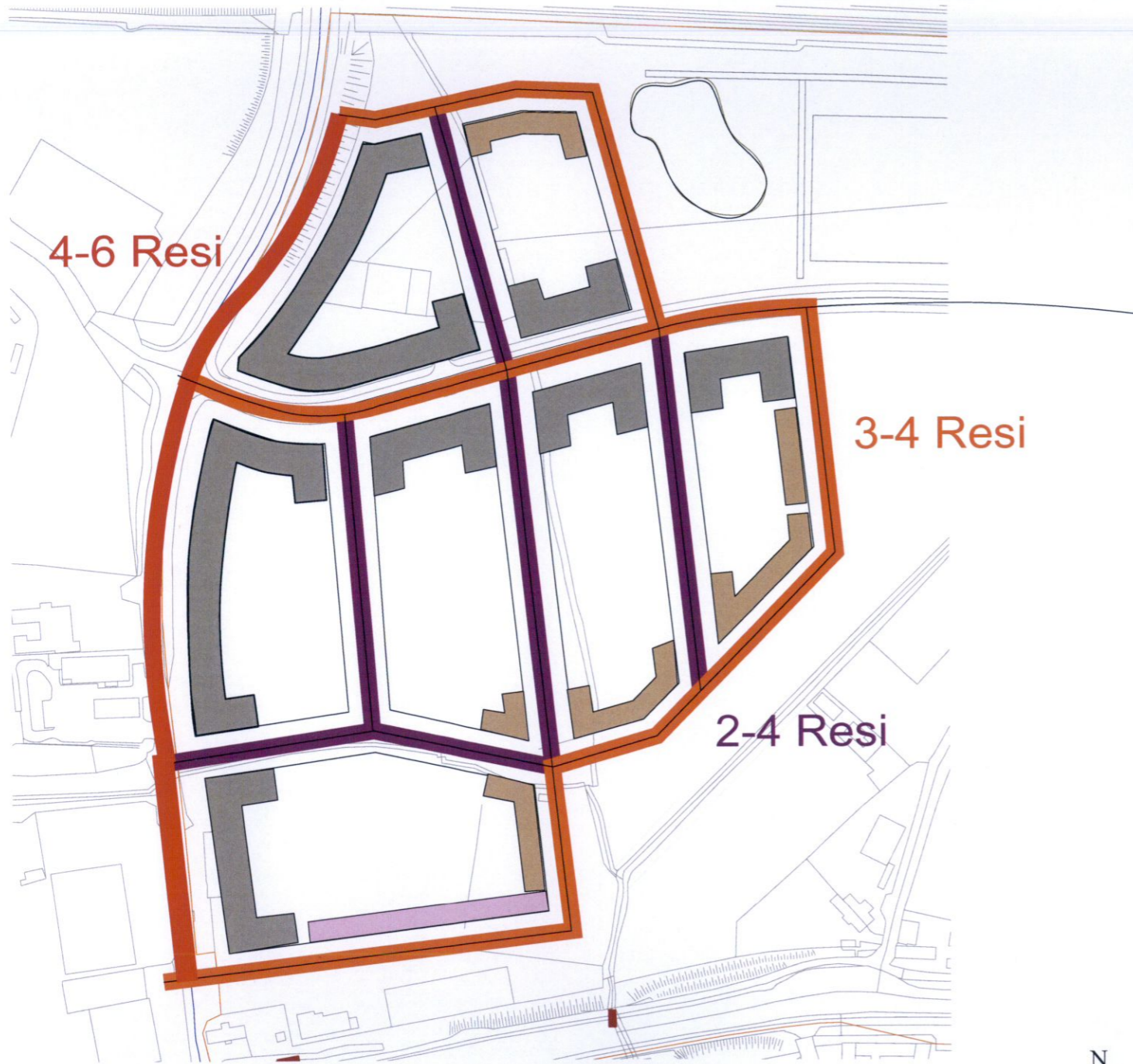


Defined Building Lines
- As set by section 3.3.23 of the SDZ -



Lands not incl. in application
Note indicative layout shown in this area

Proposed Roads Diagram
NTS



Building height Strategy
- As set by section 2.8.6-2.8.8 of the SDZ -

- 4 - 6 Storey Maximum Height
- 3 - 4 Storey Maximum Height
- 2 - 4 Storey Maximum Height



Proposed Height Diagram
NTS

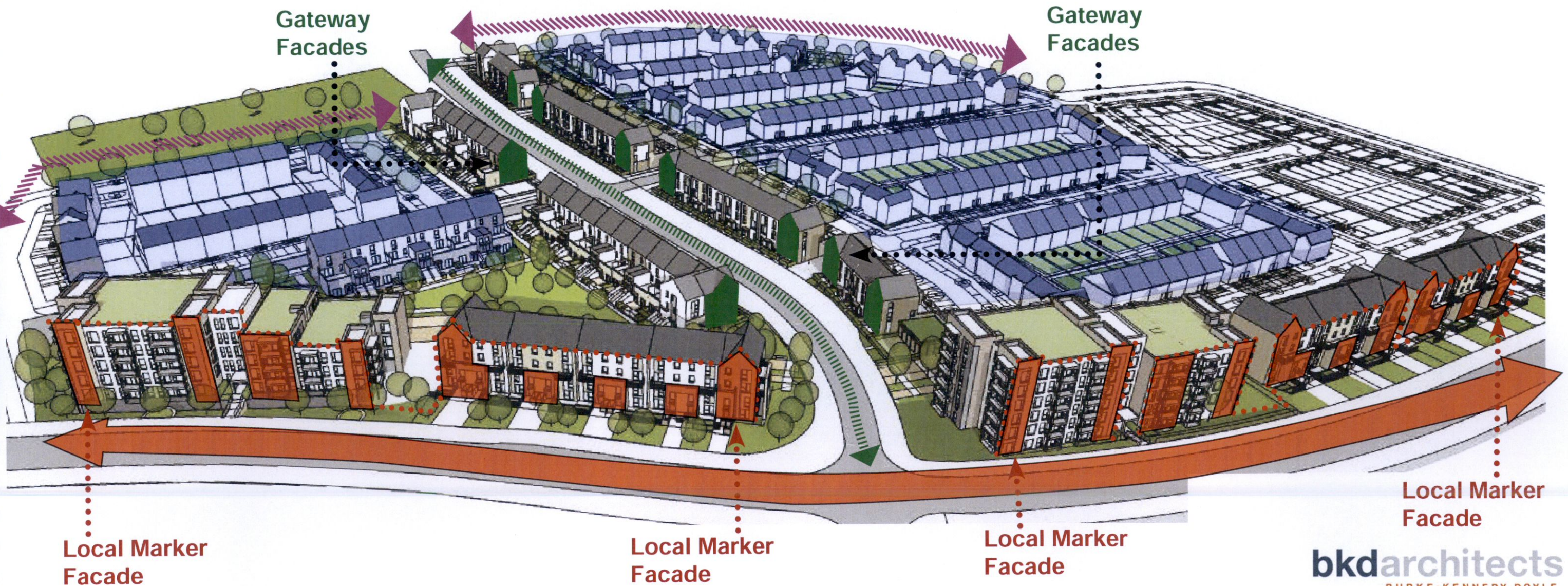
- 4-6 Storey Apartment
- 4 Storey Duplex
- 3 Storey Duplex / House
- 2 Storey House

The design language of the proposed application has been split into 4 distinct hierarchies, each relating to the edge conditions described in the SDZ and the previously approved planning application for the Link St / Avenue, which bisects the scheme.

The 4 languages set up in the proposed development are as follows:

1. **FORM, MASSING AND MATERIAL LANGUAGE OF WESTERN EDGE CONDITION - 4-6 STOREYS - FIRST VISIBLE EDGE TO WESTERN PORTION OF CLONBURRIS SDZ**
2. **FORM, MASSING AND MATERIAL LANGUAGE OF AVENUE / LINK ST - THE PRIMARY ARTERIAL ROUTE WHICH DIVIDES THE ADAMSTOWN EXTENSION DEVELOPMENT**
3. **FORM, MASSING AND MATERIAL LANGUAGE OF NORTH / EAST AND SOUTHERN EDGES - WHICH ADDRESS THE GRIFFEEEN VALLEY PARK**
4. **FORM, MASSING AND MATERIAL LANGUAGE OF CENTRE OF ADAMSTOWN EXTENSION DEVELOPMENT - INCLUDING NEIGHBORHOOD, LOCAL AND INTIMATE STREETS**

(Please note that the western boundary has been redesigned to respond to the requirements of Ai Point 1B and the corresponding relocation of units. As evident below - the western boundary continues to comply with the SDZ requirements for building height & building position).



Revised CGI Views to address A1

CGI view showing principle approach to the development, as one travels over the R120 railway bridge heading south.

The building height is raised to this western edge of the masterplan, to reflect the rising levels of the road. This initial view of the 6 storey apartment building announces the development - proposed as 6 storeys to the north, stepping to 5 and then finally down to 4 storeys to the northern end of the building. This 4 storey corner transitions to a 4 storey Duplex Terrace, to match the same type of units proposed at the southern end of the R120 development elevation. As per the initial planning submission, all level changes along the R120 have been carefully considered to hide parking, plant, bin stores and ancillary accommodation associated with the western apartment buildings.

The same vertical language has been carried through the apartment buildings, to emphasize height along this western edge. As per the initial design submitted, a rhythm of brick volumes, external balconies with vertical supporting posts and render panels create a stepped composition, which introduces the scheme when approached from the west. Throughout the development - a palette of 2 different brick tones and 2 different render colours have been used to compose each elevation.



Revised CGI Views to address A1

CGI view showing entrance junction between R120 and Avenue / Link St. Previously the 4 storey duplex units visible in the image were proposed to the RHS of the junction - and are now shown on the LHS. Similarly - the apartment building shown has swapped from the LHS as previously shown to its current location on the RHS.

As per the original planning submission, all end gables have been designed to include windows for additional passive surveillance. Pedestrian routes, open spaces and building entrances have been carefully considered to maximise activity and supervision, in all areas.



- Render Type 1
- Brick Type 1
- Brick Type 2
- Brick Type 2
- Brick Type 1
- Brick Type 2
- Render Type 2
- Brick Type 1
- Render Type 1



Revised CGI Views to address AI

CGI view showing proposed elevation to R120.

The view below shows the transition for the 4 storey duplexes to the 5 & 6 storey apartment building south of the entrance junction on the R120. Feature gables and parapet corners have been carefully considered to gradually raised the building shoulder height to signify the main entrance to the development on the western approach. This entrance is re-enforced by a pairing of buildings which create a gateway to the wider Clonburris SDZ area.



Brick
Type 1

Render
Type 1

Brick
Type 1

Render
Type 2

Brick
Type 2

Brick Type 2

Render

Brick Type 1

Revised CGI Views to address AI

CGI view showing proposed elevation to Avenue / Link St

The tenant amenity building previously proposed to the north of the Avenue / Link St has now been replaced by a pairing of duplex units - which face a landscaped area of public open space. The angled facade and green pocket park creates an inviting urban environment and access point towards the apartments and duplex which surround and activate the northern triangular plaza.



Brick Type 1 Render

Brick Type 1

Brick Type 2

Render



Revised CGI Views to address A1

CGI view showing the eastern approach to the site from the Griffeen Valley Park.

The only change visible in this view - is the relocated apartment building visible in the background. This 6 storey corner defines a terminus for the western end of the Link St / Avenue and marks the entrance to the Adamstown Extension area and the wider Clonburris SDZ area.



Render

Brick Type 1

Brick Type 2



Revised CGI Views to address AI

CGI view showing proposed internal local street.

There are minor changes to landscaping / planting visible in this view which address landscaping compliance matters identified within the AI. Please refer to the landscape consultants drawings and reports for further detail on the proposed landscaping changes to the application to address the AI.



Render Type 1

Brick Type 1

Render Type 1

Render Type 2



Revised CGI Views to address AI

CGI view showing the language of buildings along eastern boundary of the development, facing the Griffeen Valley Park.

Please note that the swale shown to the bottom of the image between the parking spaces and footpath has increased in width. No other changes proposed as part of the AI response are visible in this view.



Render Type 2

Render Type 1

Brick Type 2

Brick Type 1



Revised CGI Views to address AI

CGI view showing proposed Local Street Type 2 - 'Neighbourhood Street'.

The only change visible in this view - is the relocated apartment building visible in the background. This 6 storey corner defines a terminus for the western end of the this local street separating it from the vehicular traffic on the R120, creating a sheltered safe streetscape and local environment.



Render Type 1

Brick Type 1

Brick Type 2

Render Type 1



Revised CGI Views to address A1

Aerial CGI view taken from north-east of site looking south-west.



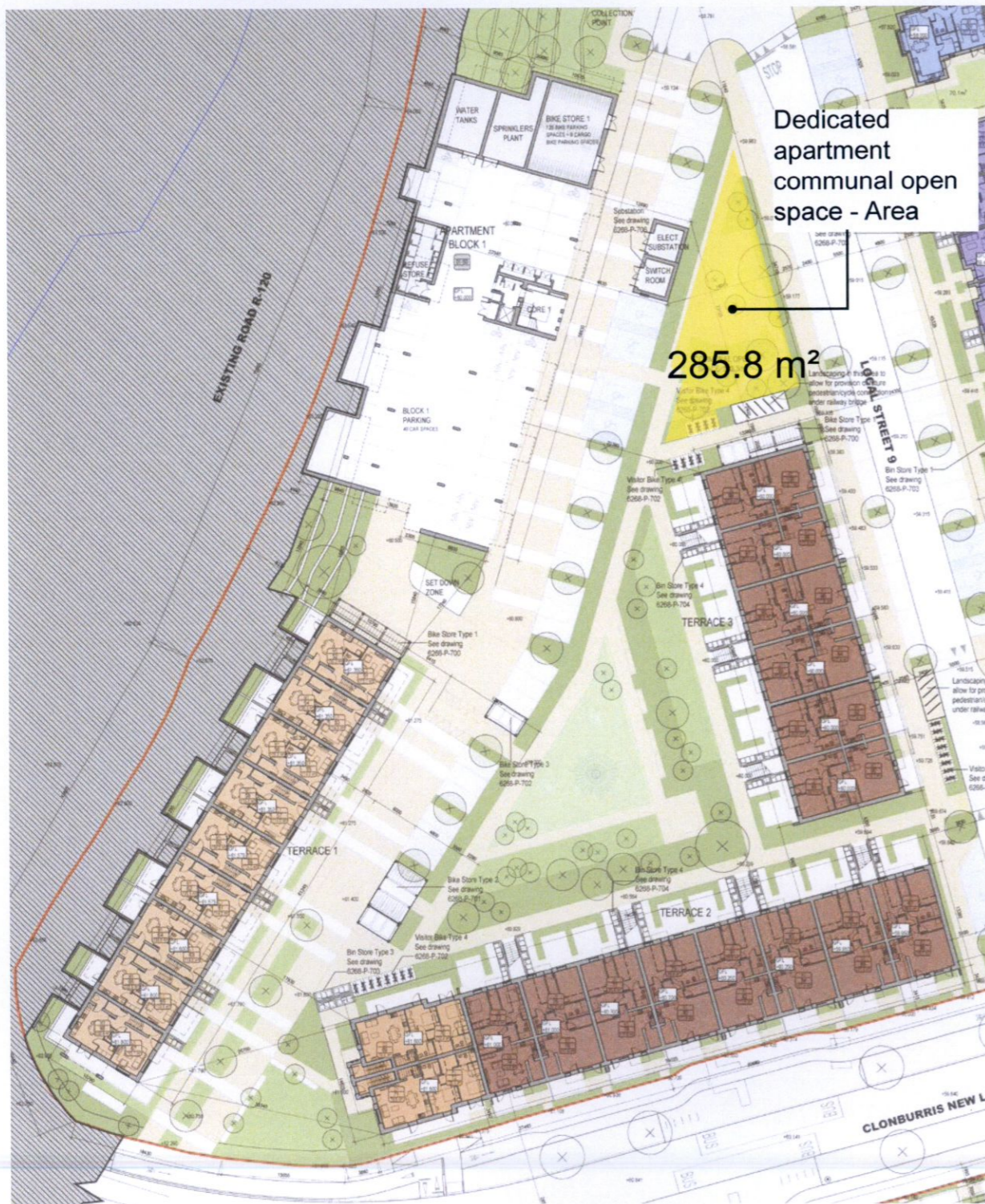
Revised CGI Views to address A1

Aerial CGI view taken from west of R120 looking east.



Private & Communal Open Space Provision (Specific to Apartment Buildings 1 & 2)

Shared apartment communal open space area



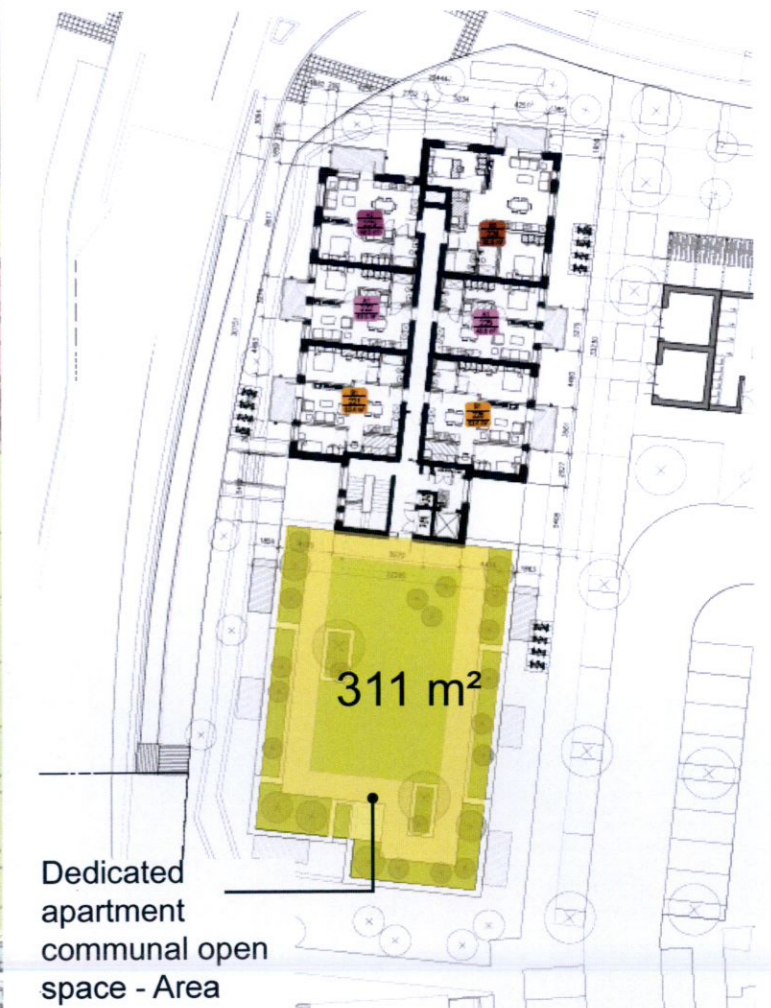
Communal open space for Apartment Blocks 1&2

Shared apartment communal open space area



Communal open space for Apartment Blocks 1&2

Communal space requirements for the apartment building 1 remains unchanged. We note that the overall number of units proposed within apartment building 1 has reduced by 2 no. Apartment building 2 is proposed with two separate communal open spaces areas - one located the north-east of the building at ground level and the other at roof level as an external terrace. The landscape design of these communal spaces has been designed to ensure security and segregation of the communal open spaces from the other nearby units.



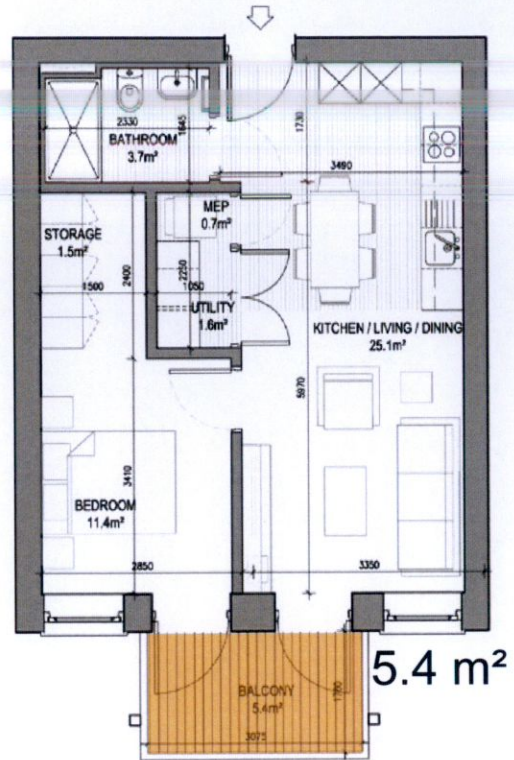
Dedicated apartment communal open space - Area

Private & Communal Open Space Provision - Revised to address AI

(Specific to Apartment Buildings 1 & 2)

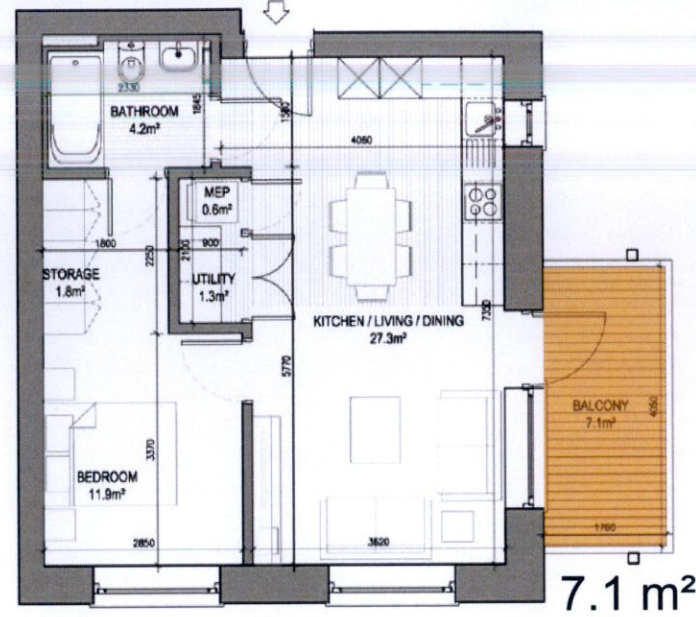
Individual apartment private open space area

Private open space areas for each apartment unit are provided as external balconies accessible from living areas. Areas of private open space proposed for each apartment type are shown colour orange, with the corresponding area indicated.



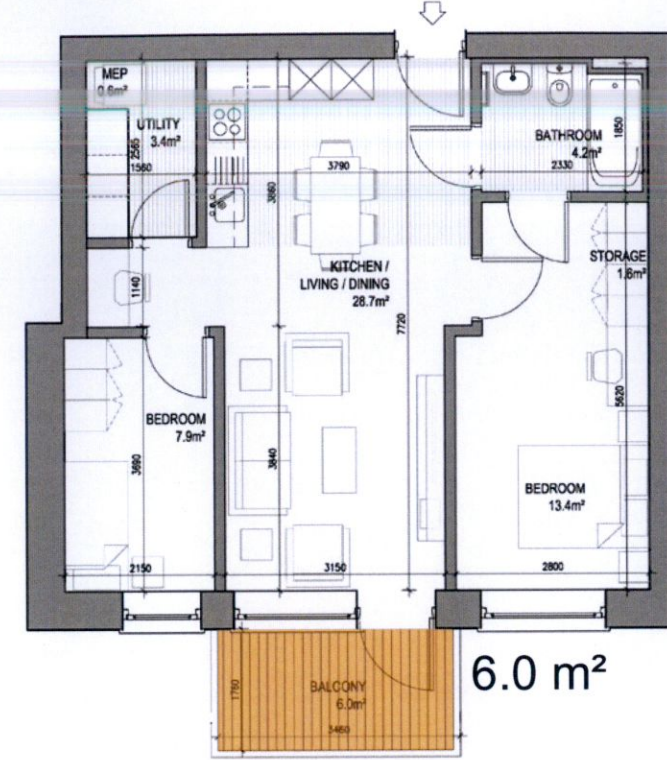
UNIT TYPE A1
1 Bed Unit - Floor Area: 46.59m²
SCALE 1:50

	REQUIRED (m ²)	PROVIDED (m ²)
STORAGE SPACE	3.0	3.1
PRIVATE AMENITY SPACE	0.6	0.7
AGGREGATE BEDROOM AREA	11.4	11.4
AGGREGATE KITCHEN / LIVING / DINING	23.0	25.1



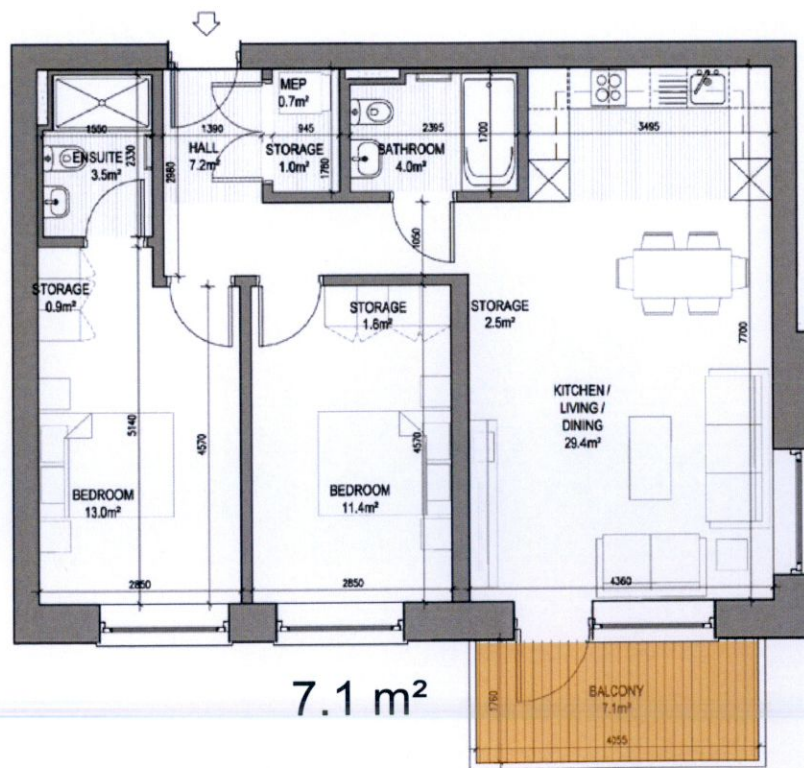
UNIT TYPE A2
1 Bed Unit - Floor Area: 49.57m²
SCALE 1:50

	REQUIRED (m ²)	PROVIDED (m ²)
STORAGE SPACE	3.0	3.1
PRIVATE AMENITY SPACE	0.6	0.7
AGGREGATE BEDROOM AREA	11.4	11.9
AGGREGATE KITCHEN / LIVING / DINING	23.8	27.3



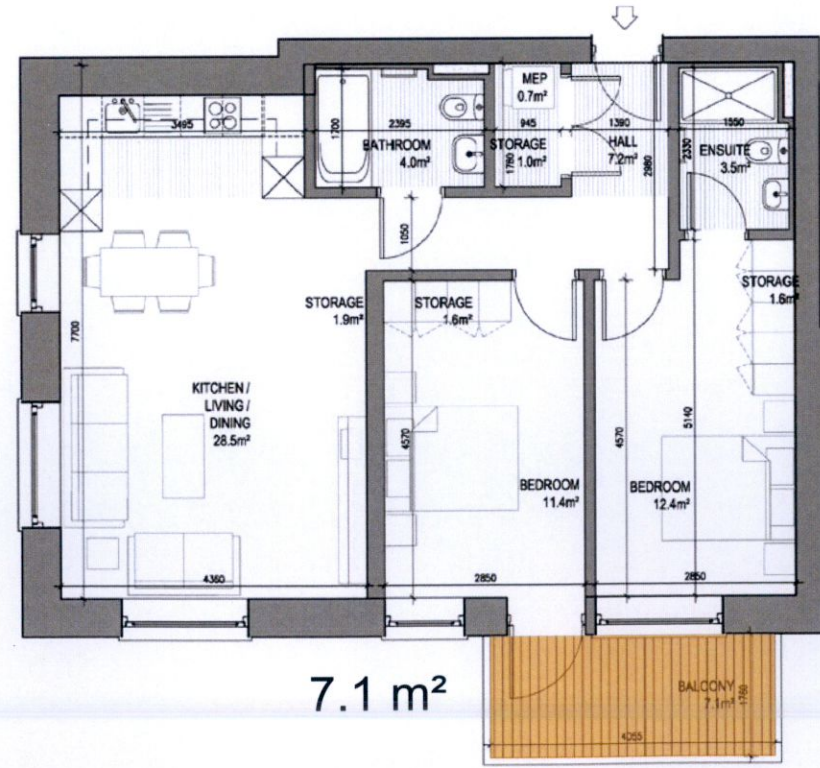
UNIT TYPE B1
2 Bed Unit - Floor Area: 63.37m²
SCALE 1:50

	REQUIRED (m ²)	PROVIDED (m ²)
STORAGE SPACE	5.6	5.0
PRIVATE AMENITY SPACE	0.6	0.6
AGGREGATE BEDROOM AREA	20.1	21.3
AGGREGATE KITCHEN / LIVING / DINING	28.0	28.7



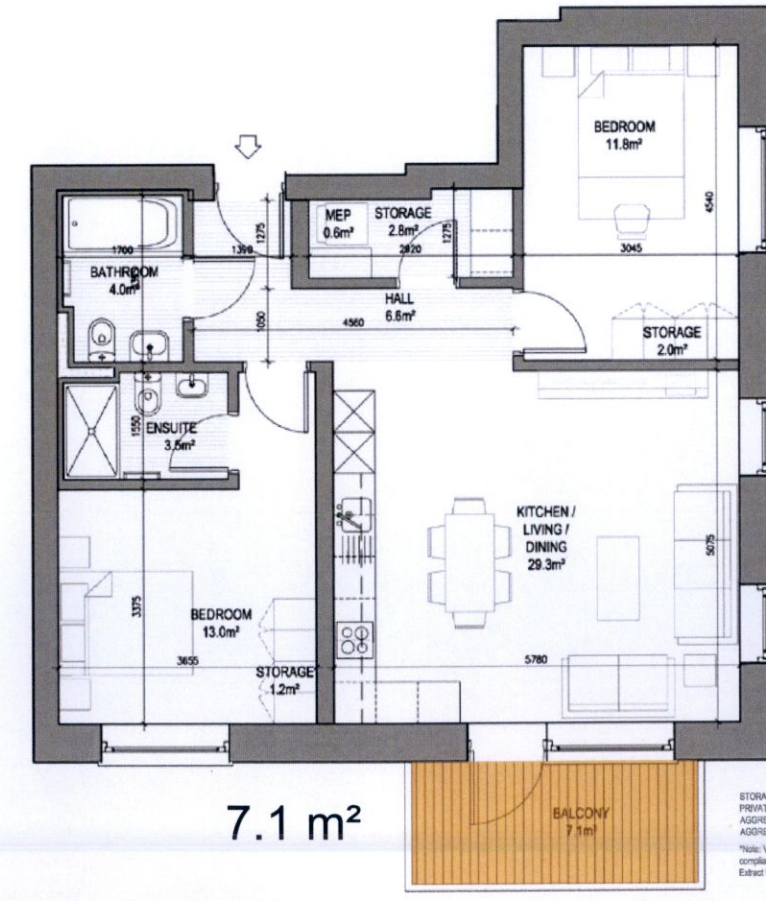
UNIT TYPE B2.1
2 Bed Unit - Floor Area: 80.53m²
SCALE 1:50

	REQUIRED (m ²)	PROVIDED (m ²)
STORAGE SPACE	6.0	6.0
PRIVATE AMENITY SPACE	7.0	7.1
AGGREGATE BEDROOM AREA	24.4	24.4
AGGREGATE KITCHEN / LIVING / DINING	30.0	28.4



UNIT TYPE B2.2
2 Bed Unit - Floor Area: 78.99m²
SCALE 1:50

	REQUIRED (m ²)	PROVIDED (m ²)
STORAGE SPACE	6.0	6.1
PRIVATE AMENITY SPACE	7.0	7.1
AGGREGATE BEDROOM AREA	24.4	23.8
AGGREGATE KITCHEN / LIVING / DINING	30.0	29.4



UNIT TYPE B3
2 Bed Unit - Floor Area: 80.87m²
SCALE 1:50

	REQUIRED (m ²)	PROVIDED (m ²)
STORAGE SPACE	6.0	6.0
PRIVATE AMENITY SPACE	7.0	7.1
AGGREGATE BEDROOM AREA	24.4	24.8
AGGREGATE KITCHEN / LIVING / DINING	30.0	29.3

	REQUIRED (m ²)	PROVIDED (m ²)
STORAGE SPACE	6.0	6.0
PRIVATE AMENITY SPACE	7.0	7.1
AGGREGATE BEDROOM AREA	24.4	24.8
AGGREGATE KITCHEN / LIVING / DINING	30.0	29.3

*Note: Variation of up to 5% can be applied to room areas and widths subject to overall compliance with required minimum overall apartment floor areas.
Extract from: Sustainable Urban Housing: Design Standards for New Apartments. Guidelines for Planning Authorities

Proposed Site Plan Layout (nts) - updated to reflect Ai response



Legend:

- 4/6 storey Apartment building (mix of 1, 2 bed units)
- 4 storey Duplex building (2s 3 bed over 2s 2 bed unit)
- 3 storey Duplex building (2s 3 bed over 1s 2 bed unit)
- 3 storey Duplex building (2s 3 bed over 1s 2 bed unit)
- 3 storey house (4 bed - 7 person)
- 3 storey wide frontage house (4 bed - 6 person)
- 2 storey house (3 bed - 5 person)
- 2 storey house (4 bed - 7 person)
- 2 storey house (3 bed - 5 person)
- 2 storey house (3 bed - 5 person)
- Extent of proposed planning application
- Lands within client ownership

Pitch and Putt Lands (Not part of application area)

Hedges to be retained and enhanced. Local Street 04 Street width to ensure sufficient space is left between hedges and hedges. All trees to be single planted in the vicinity.

SEE PART PLAN 2 FOR DETAIL IN THIS AREA

Proposed Accommodation Schedule - updated to reflect Ai response

PROPOSED EXTENT OF APPLICATION
"CLEAR REAL ESTATE HOLDINGS LIMITED" LANDS ONLY

AREA	Ha
OVERALL SITE AREA WITHIN CURRENT APPLICATION RED-LINE BOUNDARY	8.94
NETT DEVELOPMENT AREA *Excl. Avenue / Link St permitted under SDZ20A-0021	7.51
TOTAL NUMBER OF RESIDENTIAL UNITS [PROPOSED]	395
RESIDENTIAL UNITS PER HECTARE [PROPOSED] (OF DEVELOPABLE AREA)	52.6
PUBLIC OPEN SPACE HECTARES [PROPOSED]	1.45

UNIT TYPES	UNIT TYPE DESCRIPTION	GROSS INTERNAL AREA (m ²) *	NUMBER OF UNITS	TOTAL AREA	BED SPACES	TOTAL BED SPACES
				GIA (m ²)		
HOUSES						
A	4 Bed -Terrace (3 storey)	141.2	(120)	11	1553.2	77
B	4 Bed -Terrace (3 storey)	154.9	(120)	15	2323.5	105
C	4 Bed -Terrace (2 storey)	138.4	(110)	8	1107.2	56
D	3 Bed - Terrace (2 storey)	106.6	(92)	46	4903.6	230
E	3 Bed - Terrace (2 storey)	109.5	(92)	41	4489.5	205
F	3 Bed - Terrace (2 storey)	116.0	(92)	18	2088.0	90
TOTAL HOUSES				139	16465.0	763

RESIDENTIAL UNIT TYPE MIX					
UNIT TYPE	No UNITS	OVERALL (%)			Housing (%)
3-bed house	105	26.6			75.5
4-bed house	34	8.6			24.5
TOTAL UNITS	139				

DUPLEX / APARTMENTS						
J	2 Bed Apartment (1 storey)	85.6 - 87.0	(73)	44	3775	176
K	3 Bed Duplex (2 storeys)	117.9 - 119.7	(90)	44	5212.5	220
L	2 Bed Duplex (2 storeys)	113 - 114	(73)	21	2388.1	84
M	3 Bed Duplex (2 storeys)	135.5	(90)	21	2844.6	105
O	2 Bed Apartment (1 storeys)	79.5	(73)	6	477	24
P	3 Bed Duplex (2 storeys)	125.7	(90)	6	754.2	30
A1/A2	1 bed/2 people Apartments	48.9 - 53.9	(45 - 49.5)	55	2617	110
B1	2bed/3people Apartments	64.2 - 64.7	(63)	22	1394.8	66
B2/B3	2 bed/4people Apartments	81.5 - 81.9	(73 - 80.3)	37	2982.5	148
TOTAL DUPLEX / APARTMENTS UNITS				256	22445.7	963

DUPLEX / APARTMENTS UNIT TYPE MIX					
	No UNITS	OVERALL (%)			(%)
2 Bed Apartment (1 storey)	50	12.7			19.5
3 Bed Duplex (2 storeys)	71	18.0			27.7
2 Bed Duplex (2 storeys)	21	5.3			8.2
1 bed/2 people Apartments	55	13.9			21.5
2bed/3people Apartments	22	5.6			8.6
2 bed/4people Apartments	37	9.4			14.5
TOTAL DUPLEX / APT UNITS	256				

	UNITS	AREA	BEDS
TOTAL (EXTENT OF PROPOSED APPLICATION) *	395	39828	1726

* NOTE - FIGURE SHOWN INCLUDES
RESIDENTIAL ACCOMMODATION + ANCILLARY ACCOMMODATION

Building Footprint	Area (m ²)
Total Proposed Building Footprint *	19054.4
Plot Ratio (Total GFA / Overall Site)	0.53
Site Coverage (Building footprint / Site Area)	26%

Ancillary Residential	No. of bikes	Area (m ²)
Bicycle Store - Duplex Terrace 1	51	39.4
Bicycle Store - Duplex Terrace 2	47	43.3
Bicycle Store - Duplex Terrace 3	34	27.4
Bicycle Store - Duplex Terrace 4	47	43.3
Bicycle Store - Duplex Terrace 5	34	27.4
Bicycle Store - Duplex Terrace 6	47	43.3
Bicycle Store - Duplex Terrace 7	34	27.4
Bicycle Store - Duplex Terrace 8	47	43.3
Bicycle Store - Duplex Terrace 9	37	39.7
Bicycle Store - Duplex Terrace 10	47	43.3
Bicycle Store - Apartment Block 02	103	94.3
Bicycle Store - Apartment Block 01	129	87.2
Total Bicycle Store Areas		559.3

Ancillary Service

Ancillary Service	No. of buildings	Area (m ²)
Bin store Block 01	0	36.3
Bin store Block 02	1	37.6
ESB Substation + Switchroom	2	83.56
ESB Kiosk	2	10.74
Plant Room Block 01	0	93.9
Plant Room Block 02	0	96
Total Ancillary Service Areas		358.1

Total Ancillary Accommodation (Bike stores + Ancillary Services)	917.4
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