

**FURTHER INFORMATION RESPONSE
TILE 2 - SDZ22A0018**



PREPARED FOR

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PREPARED BY



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**IN RESPECT OF A
PROPOSED MIXED USE RESIDENTIAL DEVELOPMENT
AT
CLONBURRIS TILE 2
July 2023**

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DOCUMENT CONTROL SHEET

| | |
|-----------------|------------------------------|
| Client: | Cairn Homes Properties Ltd. |
| Project Title: | Tile 2 - SDZ22A/0018 |
| Document Title: | Further Information Response |

| Rev. | Status | Author(s) | Reviewed By | Approved By | Issue Date |
|-------|--------|-----------|-------------|-------------|------------|
| DV1 | Draft | RK | RK | RK | 28-6-2023 |
| Final | Final | RK | EF/RK | RK | 21-7-2023 |

1.0 INTRODUCTION

On behalf of the applicant, Cairn Homes Properties Ltd., we wish to respond to the request for further information issued by the Planning Authority on the 17th of February 2023 for a proposed residential development at Tile 2 - SDZ22A0018, Clonburris.

The Revised Site Layout Plan prepared by Altu Architects shows the overall layout in context.

The revised scheme includes the following amendments to the layout:

- Additional c. 262.5 sq. m of retail frontage added to scheme resulting in an increase of retail area from 887.5sq. m to 1,150 sq. m.
- Additional 13 no. Residential units to scheme with 11 no. located at street level with own door to increase extent of active uses to increase the overall number of units to 607 apartments.
- New design approach to Landmark Employment Building to create a distinct urban marker.
- All associated Landscaping amendments.

This response to the request for further information has carefully considered the items raised by South Dublin County Council. I

In accordance with the Further Information Request, this response has been prepared and informed by discussions with South Dublin County Council held on the 19th of May 2023 where the principle of the design changes were favourably received. .

Please find the following documents (6 no. copies) enclosed in support of this submission:

| |
|--|
| JSA Cover Letter |
| Little Rainbows Creche Letter |
| Cairn Commercial Letter |
| JSA Further Information Response Document |
| Altu Further Information Response Document |
| Altu Drawings |
| DBFL Response Doc |
| DBFL Drawings |
| DBFL Stage 2 Road Safety Audit |
| Murray Assoc. Drawings |
| Murray Assoc Response Doc. |
| 3 D Design Bureau A3 Booklet |
| IES Response Doc |
| IES Report |
| IAC Archaeology Report |
| O'Dwyer Jones Aviation Report |
| Byrne Environmental Technical Note |

2.0 ITEM - NO. 1 REQUEST FOR FURTHER INFORMATION - PHASING/DENSITY

2.1 ITEM NO. 1(A) - PHASING/DENSITY

Item no. 1(A) states:

"In order to ensure compliance with the scheme, the applicant is requested to provide the following detail: The applicant has summarised the phasing requirements applicable up to Phase 1A 0-1, 000 units. There are a total of 1,989 units either granted permission or included in planning applications currently under consideration. If all of these units were granted, constructed and occupied, they would take up the vast majority of the phasing allowance up to 2, 000 units when the Phase 1B (1, 001-2, 000 units) requirements are required to be delivered. The applicant is requested to provide details on Phase 1B."

2.2 RESPONSE TO ITEM NO. 1(A)

In response to this item, it is important to note that the phasing thresholds are triggered by the occupation of units rather than the granting of planning permissions and therefore is a matter for ongoing monitoring and review by the Development Agency.

While there have been a number of recent planning applications permitted and currently in the planning system; to date, no dwellings have been occupied in the Clonburris SDZ. Having regard to this, the phasing requirements are within Phase 1A (0-1,000 residential units). Notwithstanding this, in the interests of providing an update to the Planning Authority, progress in respect of the delivery of the elements of Phase 1A and 1B are provided. It is intended that this information will be updated with the planning applications that are lodged.

In summary, some 944 no. units have been permitted across the 2 no. catchments comprising 681 no. in Clonburris and 263 no. in Kishoge. A further 1,045 no. units are at Further Information stage or clarification of further information.

In relation to envisaged occupation of the current Tiles which Cairn have either received planning permission or are at Further Information the following is noted:

- Permitted Tile 1 (SDZ21A/0022) - c. 100 units occupied by the end of 2023. 563 no. units complete/occupied by the end of Q4 2024.
- Proposed Tile 2 (SDZ22A0018) - c. 220 units occupied by Q4 2025. 594 no. units complete/occupied by the end of Q3 2027.
- Proposed Tile 3 (SDZ22A0018) - c. 80 occupied by Q4 2024. 157 no. units complete/occupied by Q3 of 2025.
- Proposed Tile 4 and Tile 5 (under consideration) c. 250 occupied by Q4 2025 with 315 no. units complete/occupied by Q3 of 2027.

Table 2.1: Indicative Timelines for Completion/Occupation of Units by Cairn

| | | 2023 | 2024 | 2025 | 2026 | 2027 |
|-------------------|--------------|------------|------------|--------------|--------------|--------------|
| Tile 1 | 563 | 100 | 463 | | | |
| Tile 2 | 607 | | | 226 | 250 | 131 |
| Tile 3 | 157 | | 80 | 77 | | |
| Tile 4 and Tile 5 | 565 | | | 250 | | 315 |
| Total | 1,892 | 100 | 543 | 553 | 250 | 446 |
| Cumulative | N/A | 100 | 643 | 1,196 | 1,446 | 1,892 |

Note: Delivery indicative and may be subject to market demand.

Table 4.2 of the Scheme identifies the infrastructure required to be linked to the delivery of residential development and phased in accordance with the construction of residential units. Table 4.3 sets out the phasing for the overall scheme which is set out below in Table 1 with commentary on the considerable progress made to date by South Dublin County Council and the landowners/stakeholders in the Clonburris SDZ.

2.2.1 Phasing in Planning Scheme

As set out in the Planning Scheme, “the phasing programme is based on the premise that the number of dwelling units that may be constructed and occupied in each phase of development and is dependent on a predetermined amount of works to provide infrastructure, services, facilities and amenities having been completed to serve each phase.”

Table 4.3 of the Planning Scheme sets out the phasing programme for the development of the SDZ lands. A number of important prior to commencement plans / strategies are required to be prepared. These are a Surface Water Management Plan to implement the Surface Water Strategy, a Parks and Landscape Strategy, and Water and Wastewater Plans. These Strategies have been prepared and agreed with South Dublin County Council.

| Summary of Phasing Programme | | |
|---|--|---|
| Element of Phasing Approach | Application | Description |
| 1. Phasing Table (See Section 4.3) | Applicable to all Planning Scheme development | Strategic Phasing Requirements |
| 2. Place Making Requirements (See Section 4.4) | Applies to the construction of residential units within the catchments of Clonburris Retail Core and Kishoge Retail Core | Secondary Phasing Requirement – Place Making Requirement Table is a supporting table to Phasing Table |
| 3. Local Level Infrastructure (See Section 4.5) | Local level physical infrastructure | Secondary Phasing Requirement – Local Level Infrastructure Table is a supporting table to Phasing Table |

Figure 2.1: Site Layout SDZ20A/0021 – Roads and Drainage Infrastructure & Phasing



It is noted that the Planning Scheme does not specify a commencement location for the construction of residential units for any phase of residential development stating that “The location for commencing development in the Planning Scheme and within Development Areas is flexible. However, apart from exceptional circumstances, when development commences in a Development Area, subsequent permissions and development in the subject Development Area shall be required to integrate with the previous development in a sequential way.”

The Phasing Table (Table 4.3) for the Planning Scheme details a schedule and programme to plan and deliver strategic infrastructure and amenities on a phased basis, in tandem with the development of residential units in the Planning Scheme. The phasing programme is based on the premise that the

number of dwelling units that may be constructed and occupied in each phase of development is dependent on a predetermined amount of works to provide infrastructure, services, facilities and amenities having been completed to serve each phase. As such, in the event that the minimum requirements of the Phasing Table are not delivered for a particular phase, a restriction on the construction and occupation of residential units in the next phase will apply.

With reference to the above, the proposed sequencing of development within Clonburris South West Development Area by Cairn is predicated on an east to west build out, which includes Clonburris Urban Centre (CUC1 – Tile 2) and the development of the parks, (Grand Canal, Urban Square, and open space in Tile 1), as well as the roads and infrastructure works (under SDZ20A/0021), which has permission and has commenced development and will be constructed in 2 phases. The first infrastructure phase 1A will be completed in September 2023, and the remainder Stage 1B will be completed in April 2025 (with Tiles 4 and 5 complete in August 2024).

Table 2.2: Permitted and Proposed Applications Clonburris

| Reg. Ref. | Development Area | No of Units Proposed | No of Units Permitted | Planning Status | No. of Units Constructed/status |
|---------------------------------------|--|----------------------|-----------------------|--------------------------------------|---------------------------------|
| SDZ21A/0022 (Tile 1) | Clonburris South West | 563 (as permitted) | 563 (as permitted) | Granted | Commenced |
| SDZ22A/0017 (Tile 3) | Clonburris South West | 157 | 157 | Granted | 0 |
| SDZ22A/0018 (Tile 2 and Tile 3) | Clonburris Urban Centre and South West | 607 | | Further Information | 0 |
| SDZ22A/0010 | Clonburris Urban Centre and South East | 294 | 294 | Granted | 0 |
| SDCC Part 8 application | Canal Extension | 118 | 118 | Approved | 0 |
| Sub Total Clonburris Catchment | | 1,739 | 1,132 | | 0 |
| SDCC Part 8 application | Kishoge South West | 263 | 263 | Approved | 0 |
| Sub Total Kishoge Catchment | | 263 | 263 | | 0 |
| SDZ23A/0004 | Adamstown Extension | 385 | | Further Information (6th April 2023) | 0 |
| Sub Total Adamstown Catchment | | 385 | | | 0 |
| Overall | | 2,387 | 1,395 | | 0 |

Table 2.3: Phasing Table

| Phase 0 Prior to commencement of development | |
|---|--|
| Prior to the commencement of development, strategic district level measures and detailed design shall be prepared by the developer(s) and agreed with South Dublin County Council through a Surface Water Management Plan to implement the prepared Surface Water Strategy. | This has been approved by SDCC. |
| Prior to the commencement of development, a strategic level Parks and Landscape Strategy (incorporating a Biodiversity Management Plan) shall be prepared by the developer(s) and agreed in writing with South Dublin County Council. | This has been approved by SDCC. |
| Prior to the commencement of development, detailed Water and Wastewater plans for the Planning Scheme shall be prepared by the developers and agreed with Irish Water and South Dublin County Council. | This has been approved by SDCC. |
| Phase 1A 0-1,000 Residential Units Constructed and Occupied | |
| Retail Core development in accordance with the Place Making Requirements. | The applicant is progressing with the preparation of applications for the provision of retail units and is committed to providing retail facilities in accordance with the Clonburris SDZ requirements. It is noted that Tile 2 includes some retail and commercial floorspace which is considered appropriate in the interim. |
| Development shall accord with the Local Level Infrastructure Requirements in relation to agreeing timeline for the opening of Kishoge Train Station & completion of the realignment of Lock Road (R120) | Not Applicable to CUC and CSW Development Areas It is noted that Irish Rail have confirmed it is their intention to open Kishoge train station by Q1 2024. |
| Provision of Water and Wastewater infrastructure on a pro rata basis in accordance with detailed plans agreed with Irish Water and SDCC. | Noted. |
| The planning of works for the provision of community floor space and the availability of childcare spaces | Permitted Tile 1 (SDZ21A/0022) included a creche (534 sq. m) and community multi-purpose floorspace (c. 74 sq. m). Proposed Tile 2 (SDZ22A0018) includes a creche of c. 609 sq. m. |
| 1B 1,001 – 2,000 (Table 4.3 Phasing Table) Residential Units constructed and occupied | |
| | Commentary |

| | |
|---|---|
| Planning permission for appropriate elements of Clonburris Retail Core, including retail provision, employment, residential, community floorspace and urban hub. | The Clonburris Retail Core is not required at this point in time. Cairn anticipate a planning application for the Clonburris Retail Core in Q3 2024/ Q1 2025. |
| Planning permission for Kishoge Retail Core, including retail provision, employment, community floorspace and residential | Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced with South Dublin County Council. |
| Planning and commencement of works at Griffeen Valley Park Extension or Na Cluainte (North and South) in accordance with the prepared and agreed strategic Parks and Landscape Strategy. | It is noted the Strategic Parks and Landscape Strategy has been approved which provides the framework for the Griffen Valley Park Extension and the Na Cluainte North and South) parks. Cairn will provide further detail prior to occupation of 2,000 th unit. |
| Agree and make available a school site to the Department of Education (in accordance with requirement of the Department of Education and SDCC) | Cairn has progressed a site with the Department of Education and Skills for a school site north of site at Kishoge and planning permission is in place (under SDZ22A/0011). |
| Provision of a minimum 1,000 sqm net convenience floorspace and 500 sqm net comparison/ retail services floorspace (As part of the Place Making Requirements for delivery of Kishoge) | Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced with South Dublin County Council. The Phasing of the Urban Core has been the subject of extensive discussions with SDCC. |
| Provision of a minimum of 500 sqm of community services floorspace (e.g. childcare or health centre) (At Kishoge or Clonburris – subject to location of residential units constructed). | Permitted Tile 1 (SDZ21A/0022) included a creche (534 sq. m) and community multi-purpose floorspace (c. 74 sq. m). Proposed Tile 2 (SDZ22A0018) includes a creche of c. 609 sq. m. SDZ22A/0010 (currently at Clarification of Further Information includes a creche c. 520 sq. m). Future Tile 6 Kishoge (KUC-S4) (to be lodged in Q2 will include a creche. |
| Design and planning permission for District Park Hub at Clonburris Little. | The District Park hub is located in Kishoge South East (KSE-S1 and KSE-S2; Tile 7 and Tile 8) respectively and Cairn are not constructing in that location in the short term. The District Park Hub will be delivered as part of the development of that area in due course and in tandem with Na Cluainte park. It is noted Community space c. 74 sq. m was permitted in Tile 1. |
| Planning and commencement of Link Streets with associated undergrounded services in Development Areas where construction of residential units commences or commenced in Phase 1 (A or B). | The Clonburris Southern Link Street (CSLS) has planning permission (Roads and Drainage Infrastructure works application - SDZ20A/0021) and stage 1A (between the Fonthill Road and the western edge of Tile 1 and 3) is anticipated to be completed in September 2023 with works currently underway. The remainder of the |

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| | CSLS is currently out to tender and will be awarded in June 2023 and completed in April 2025. The element serving CSWS1 (Tile 4) and CSWS2 (Tile 5) is anticipated to be completed in August 2024. |
| Provide/demonstrate availability of childcare spaces in the catchment in accordance with Section 2.7.3. | As above. |
| District and catchment level measures from an agreed Surface Water Management Plan to accommodate construction within the subject development areas of construction | Part of CIL works and will be delivered as part of the construction of the Roads and Drainage Infrastructure works (under SDZ20A/0021). Appropriate District surface water attenuation is incorporated and is anticipated to be completed as per timelines above. |
| Retail Core development in accordance with the Place Making Requirements | Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced. |
| Development shall accord with Local Infrastructure Requirements in relation to opening of Kishoge Railway Station | Not required for Clonburris Catchment but Cairn are in discussions with Irish Rail and South Dublin County Council. It is anticipated that the relevant elements will be in place by the end of 2023. |
| Provision of Water and Wastewater infrastructure in accordance with detailed plans agreed with Irish Water and SDCC on a pro rata basis. | Delivery as set out above in relation to Roads and Drainage Infrastructure works (under SDZ20A/0021). |
| Phase 2 2,001 – 4,000 (Table 4.3 Phasing Table) Residential Units constructed and occupied | Commentary |
| Provision of District Park Hub at Clonburris Little | The District Park hub is located in Kishoge South East (KSE-S1 and KSE-S2; Tile 7 and Tile 8) respectively and Cairn are not constructing in that location in the short term. The District Park Hub will be delivered as part of the development of that area in due course. It is noted Community space c. 74 sq. m was permitted in Tile 1. The District Park Hub is to be delivered in tandem with the park (to service the park). |
| Completion of Link Streets and associated junctions with associated undergrounded in development areas where residential units are constructed (includes bridges where relevant). | The Clonburris Southern Link Street (CSLS) has planning permission (Roads and Drainage Infrastructure works application - SDZ20A/0021) and stage 1A (between the Fonthill Road and the western edge of Tile 1 and 3) is anticipated to be completed in September 2023. The remainder of the CSLS is currently out to tender and will be awarded in June 2023 and completed in April 2025. The element serving CSWS1 (Tile 4) and CSWS2 (Tile 5) is anticipated to be completed in August 2024. |

| | |
|---|---|
| Planning and commencement of works to Griffen or Na Cluainte (alternate option to park commenced in Phase 1B) in accordance with the prepared and agreed strategic Parks and Landscape Strategy | It is noted the Strategic Parks and Landscape Strategy has been approved which provides the framework for the Griffen Valley Park Extension and the Na Cluainte North and South) parks. Cairn will provide further detail prior to occupation. |
| Completion of works to Park commenced in Phase 1B at Griffen or Na Cluainte. | As above. |
| Agree and make available sites to the Department of Education for Schools (in accordance with requirement of the Department of Education and SDCC). | Cairn has progressed a site with the Department of Education and Skills for a school site north of site at Kishoge and planning permission is in place (under SDZ22A/0011). |
| Provide/demonstrate availability of childcare spaces in the catchment in accordance with Section 2.7.3 including the delivery of a minimum of one purpose built facility in Kishoge or Clonburris Urban Centre. | Permitted Tile 1 (SDZ21A/0022) included a creche (534 sq. m) Proposed Tile 2 in the Clonburris Urban Centre (SDZ22A0018) includes a creche of c. 609 sq. m. |
| District and catchment level measures from the Surface Water Management Plan to accommodate construction within the subject development areas. | Part of CIL works and will be delivered as part of the construction of the Roads and Drainage Infrastructure works (under SDZ20A/0021). Appropriate District surface water attenuation is incorporated and is anticipated to be completed as per timelines above. |
| Retail Core development in accordance with the Place Making Requirements. | Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced. |
| Provision of Water and Wastewater infrastructure in accordance with detailed plans agreed with Irish Water and SDCC on a pro rata basis. | Delivery as set out above in relation to Roads and Drainage Infrastructure works (under SDZ20A/0021). |

2.3 ITEM NO. 1(B) - NET DEVELOPMENT AREA

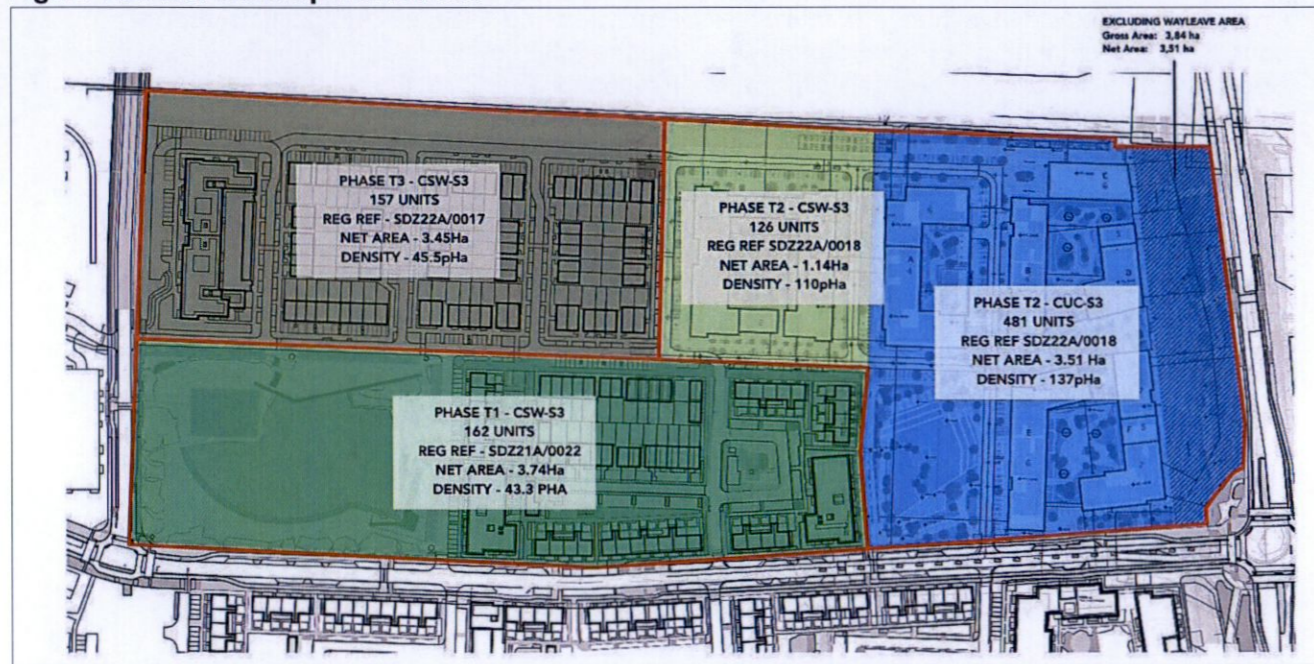
Item no 1(B) states:

"b. The applicant is requested to set out the net development area for CUC-S3 and the net development area for CSW-S3. The applicant is requested to detail how the total net development area for CSW-S3 is met through applications SDZ21A/0022, SDZ22A/0017 and SDZ22A/0018."

2.4 RESPONSE TO ITEM NO. 1(B) - NET DEVELOPMENT AREA

In response to this item, the CSW S3 lands have been subject of 3 no. separate applications comprising Phase Tile 1 (SDZ21A/0022), Phase Tile 2 (SDZ22A/0018) and Phase Tile 3 (SDZ22A/0017) and comprise 8.33 hectares (net) in total (with a net density of 53.4 units per hectare).

Figure 2.2: Net Development Areas



Source: Altu Architect's Response

Under Phase T1, 3.74 hectares of CSW-S3 were incorporated and 162 units and a local park were proposed. Under Phase T2, 1.14 hectares of CSW-S3 were incorporated and 126 units were proposed. As part of T3, 157 units and the local green corridor were proposed on the remaining 3.45 hectares.

2.5 ITEM NO. 1 (C) – STRATEGIC PEDESTRIAN/CYCLE ROUTE

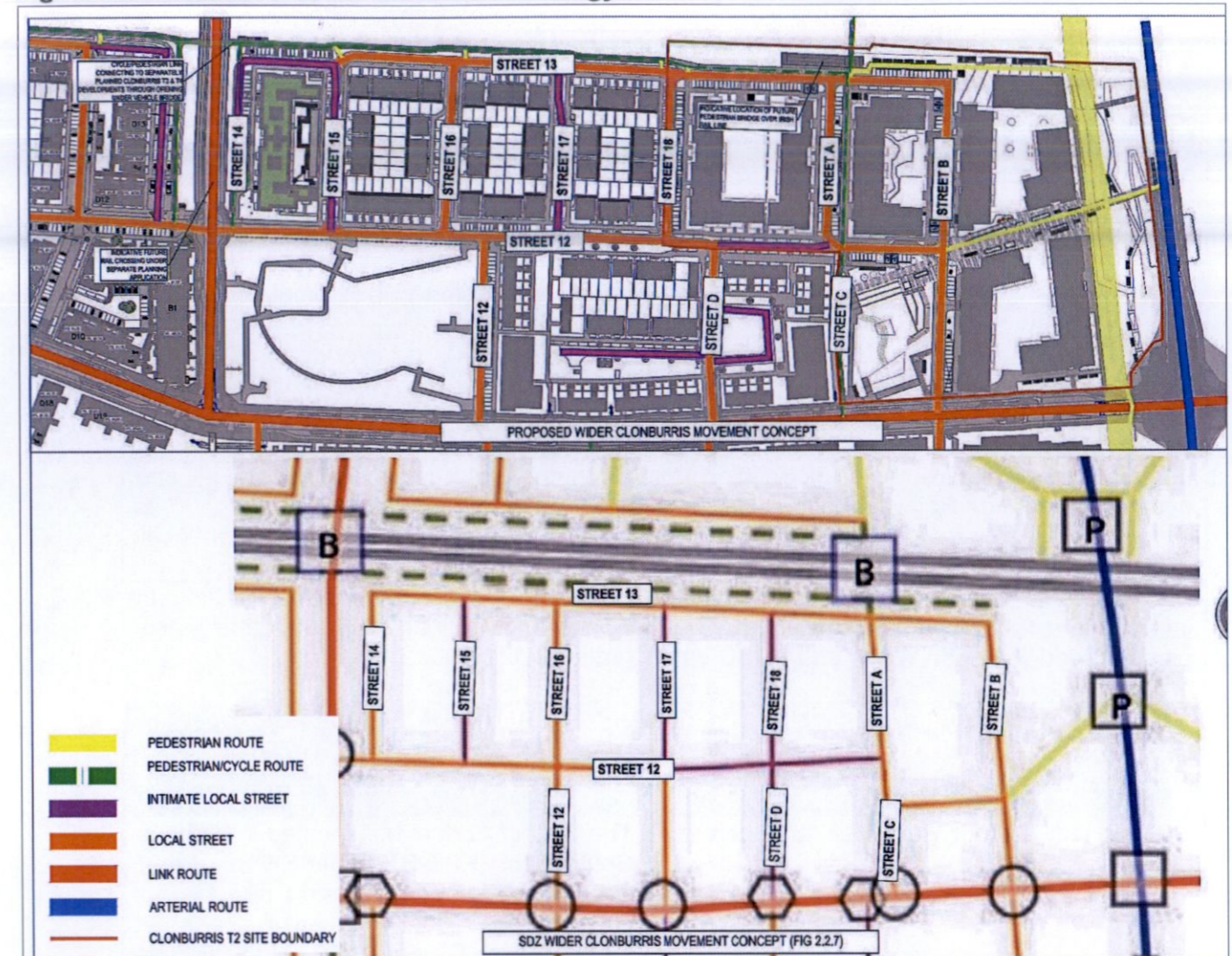
Item no. 1(c) states:

"c. The applicant is requested to demonstrate that the strategic pedestrian / cycle route to the north of the site, along the railway, links to the cycle track proposed on the adjacent sites."

DBFL have prepared a comparison drawing between the wider Clonburris movement concept as set out in the Clonburris SDZ, figure 2.2.7 and the proposed wider Clonburris movement concept. Refer to the enclosed drawing CLB-1B-95-SW-DTM-DR-DBFL-CE-1002, indicating the continuous shared pedestrian/cycle route, linking the subject Clonburris T2 development and the adjacent proposed developments. The proposed Clonburris T3 development directly west of Clonburris T2 (planning

ref.SDZ22A/0017) has been granted planning permission as well as Clonburris T1 directly to the south (planning ref. SDZ21A/0022), which is currently under construction.

Figure 2.3: Clonburris Wider Movement Strategy



Source: DBFL Drawing CLB-1B-95-SW-DTM-DR-DBFL-CE-1002

2.6 ITEM NO. 1 (D) – STREET DESIGN

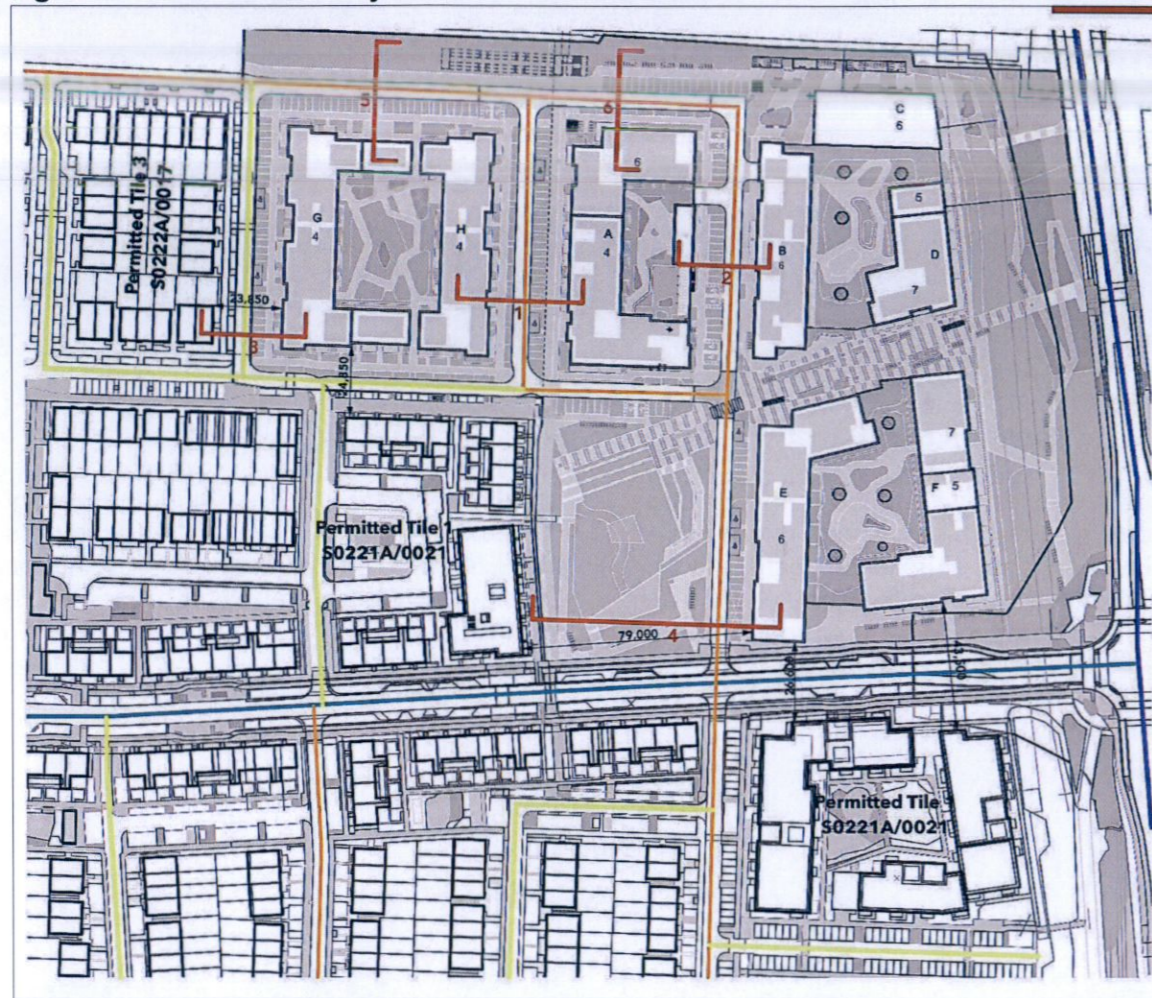
Item no. 1(d) states:

"d. Street design: The applicant is requested to provide a clear layout plan indicating: - The road typology - Width between building frontages (including areas with planning permission or under consideration applications) - Carriageway width - Footpath width - Privacy strip width (if present) - Location of street trees. Cross sections should also be provided where relevant."

We refer the Planning Authority to the enclosed ALTU architects Further Information response which includes details of road typologies, carriageway width, footpath widths and privacy strips are set out.

The site Layout Plan has been revised to include additional dimensions. The road typology is shown on the included diagram in line with the requirements of the SDZ Planning Scheme and also on a separate Movement Strategy drawing prepared by DBFL consulting engineers. The following pages include a series of 3d street cross sections with dimensions of each element to help demonstrate adherence to the requirements of the SDZ Planning Scheme.

Figure 2.4: Street Hierarchy and Section Locations



STREETS:

- existing arterial route
- proposed link route
- proposed local street
- proposed intimate local street

An example of some of a local street is set out below.

Figure 2.5: Local Street Type A



Local street Type A

3.0 ITEM NO. 2 - REQUEST FOR FURTHER INFORMATION – PERIMETER BLOCKS

3.1 ITEM NO. 2(A) – PERIMETER BLOCK

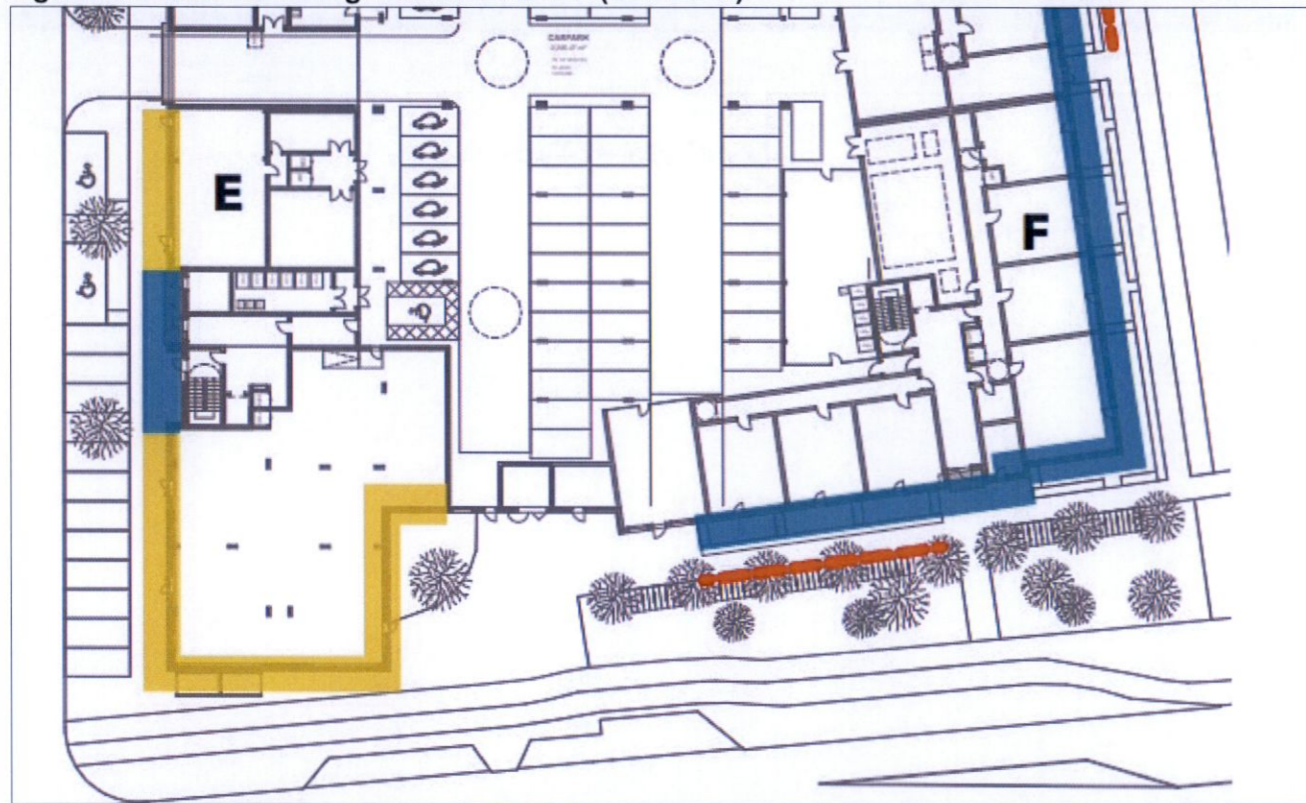
Item no. 2(a) states:

“It is noted that the scheme envisages a perimeter block to the south, where blocks E and F are located. It is noted that there is a break in this frontage at present and also that Block F is set off the southern link road. There do not appear to be any physical constraints to providing a strong frontage at this location. The applicant is requested to redesign this area to provide building frontage to the link street.”

3.2 RESPONSE TO ITEM 2(A) – PERIMETER BLOCK

In response to this item, the frontage along the perimeter block to the south at Blocks E and F has been increased to reduce significantly the break in frontage through the inclusion of 3 no. own door apartments. Block E and F are designed as a perimeter block fully enclosed at street level, with a narrow break on the south elevation above podium level to allow sunlight/daylight to the landscaped internal courtyard and improve the dual aspect ratio to the apartments in this location.

Figure 3.1: Active Frontage Blocks E and F (Southern)



Overall, it is proposed to provide some 13 no. additional apartments to provide expanded active frontages throughout the scheme.

The effect of the design change is to now provide some 607 no. apartments compared to 594 no. apartments submitted previously as set out:

Table 3.1: Overall Proposed Dwelling Mix (As Lodged)

| | 1-bedroom | 2-bedroom | 2-bedroom (3 person) | 3-bedroom | Overall |
|------------|-----------|-----------|----------------------|-----------|---------|
| Apartments | 255 | 273 | 34 | 32 | 594 |
| % | 43% | 46% | 5.7% | 5.4% | 100% |

Source: Altu Architects Schedule of Areas

Table 3.2: Overall Proposed Dwelling Mix (Further Information)

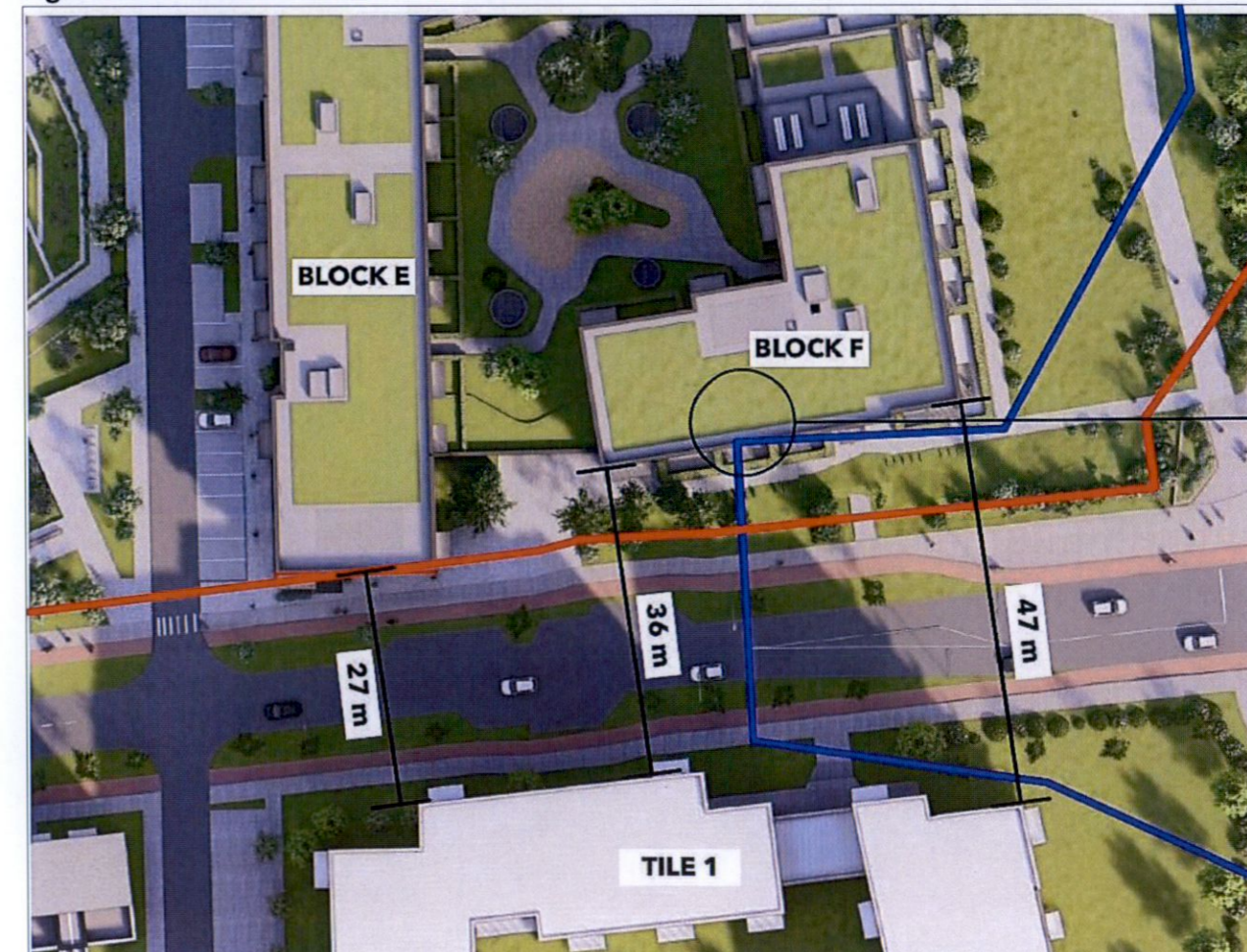
| | 1-bedroom | 2-bedroom | 2-bedroom (3 person) | 3-bedroom | Overall |
|------------|-----------|-----------|----------------------|-----------|---------|
| Apartments | 264 | 271 | 40 | 32 | 607 |
| % | 43.5% | 44.6% | 6.6% | 5.3% | 100% |

Source: Altu Architects Schedule of Areas

3.2.1 Offset to Southern Link Street

With reference to the offset to the Southern Link Street, the southern building line of Block E and F is tight to the Applicant’s ownership line and mirrors the profile of Block A in permitted Tile 1, which also graduates outwards to the east to respect the ownership line. Both buildings create an urban entrance gateway to Clonburris from Fonthill Road and the distance between the blocks is an appropriately scaled urban entrance to this new neighbourhood. From an urban design perspective the mirrored layout is preferred on either side of the entrance.

Figure 3.2: Offset to Southern Link Street



Source: Altu RFI Document

The apartment buildings on either side of the Link Street move closer together tracking the land ownership line to establish a building line along this street. Additional own door residential units have been introduced to the ground floor of Block F to bring added activity at street level. Well-designed landscaping completes a strong and attractive frontage from the linear park on the east to the urban square to the west of Blocks E & F.

3.3 ITEM NO. 2(B) – STREET NETWORK

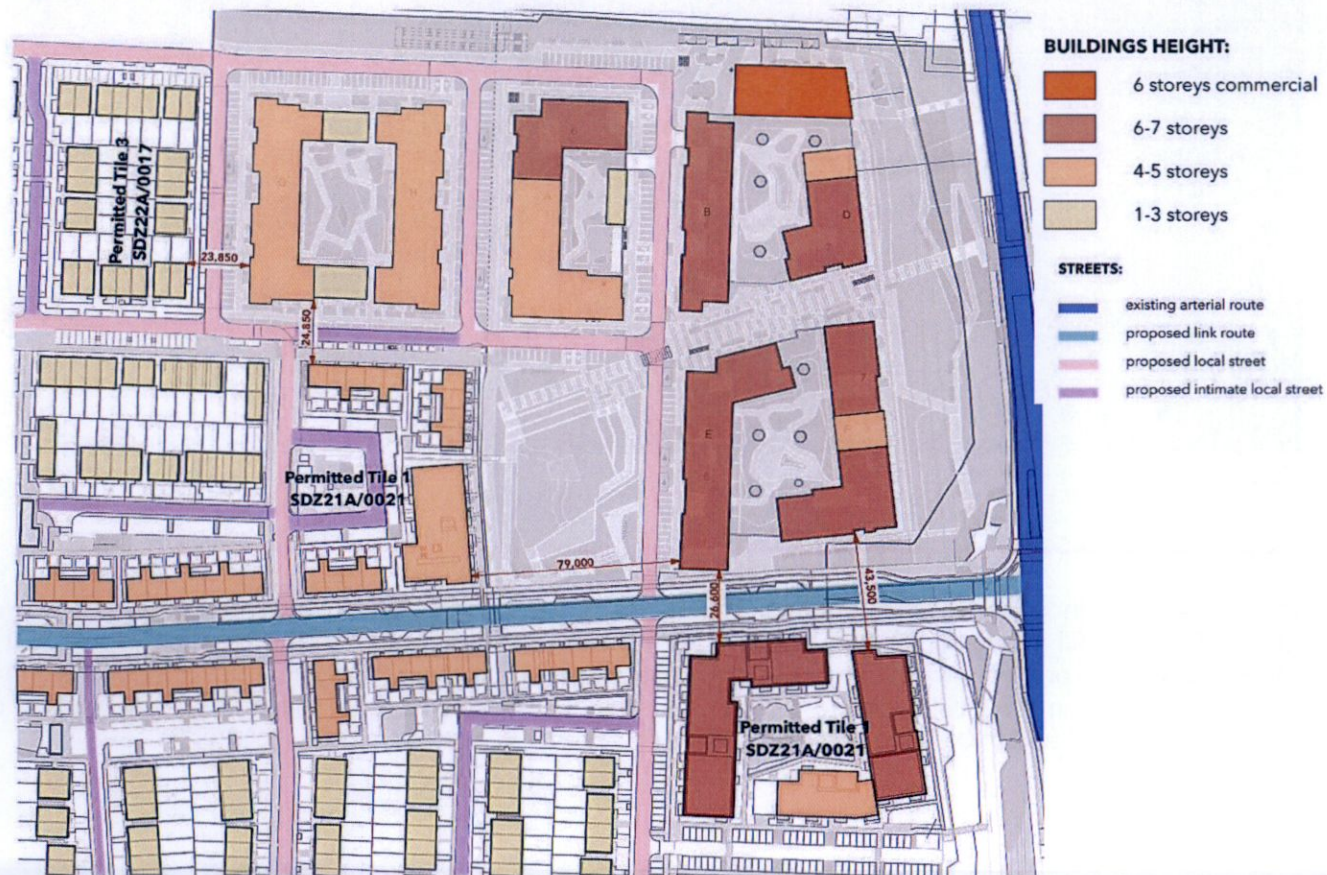
Item no. 2(b) states:

“The applicant is requested to provide a layout plan indicating how the proposed street network, frontage distances and heights fit with the permitted context or that which is in development”

3.4 RESPONSE TO ITEM 2(B) – STREET NETWORK

In response to this item, as set out in the ALTU FIR response document, the neighbouring diagram indicates how the proposed development knits with the permitted context and how the proposed street network, frontage distances and heights respond to the SDZ Planning Scheme requirements. Proposed Tile 2 (CUC-S3) is adjacent to Tile 1 (CSW-S3) and Tile 3 (CSW-S3) which are also being developed by Cairn Homes. During the design process coordination meetings were held between the respective Design Teams to ensure that each neighbouring development will be designed with respect to heights and distances between opposing buildings in accordance with the requirements of the SDZ Planning Scheme.

Figure 3.3: Heights and Road Hierarchy



Source; Altu Architects

3.5 ITEM NO. 2(C) – ACTIVE FRONTAGE

Item no. 2(c) states:

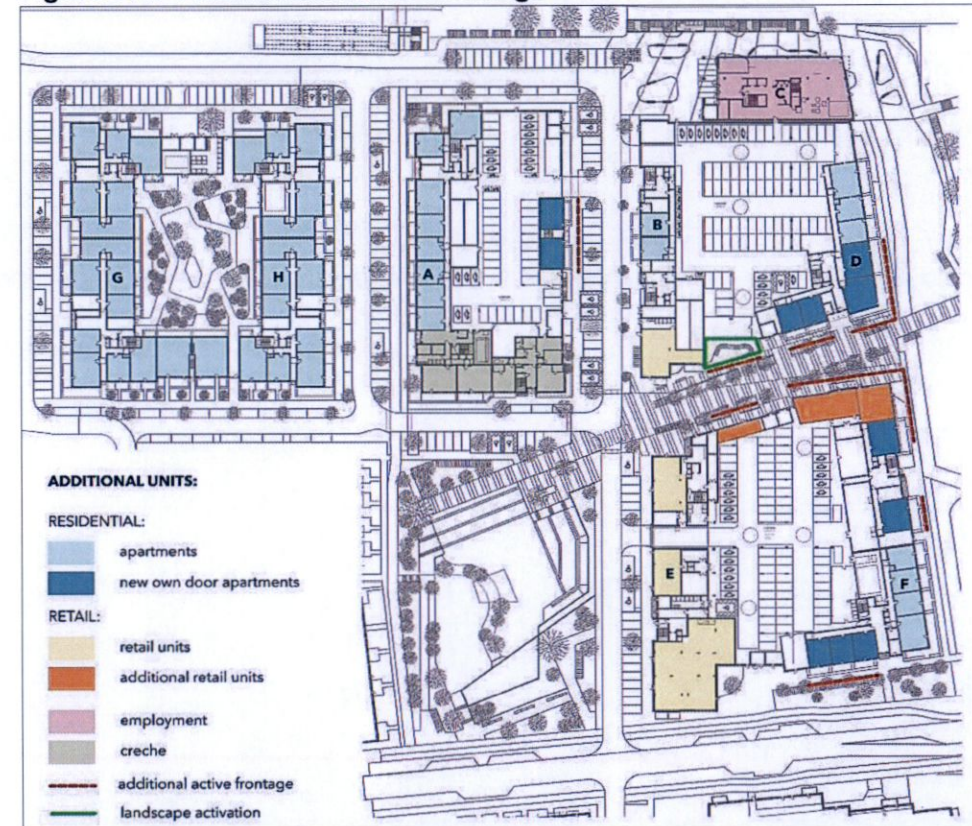
“There are concerns with the level of active uses provided along secondary shopping frontage areas and also along the southern link street frontage. The applicant is requested to provide a revised layout, which provides more appropriate uses along the secondary retail frontage and also activates the area along the link street.”

3.6 RESPONSE TO ITEM 2(C) – ACTIVE FRONTAGE

In response to this item, we refer the Planning Authority to the enclosed ALTU response document which notes that 3 no. additional retail units have been added to the norther edge of Blocks E and F fronting the pedestrian street to provide an increased cluster of retail frontage on the secondary retail street. The inclusion of these units increases the retail frontage from 887.5 sq. m to 1,150 sq. m.

Further active frontage is provided through the inclusion of ground floor own door access apartments along the southern frontage of Block D.

Figure 3.4: Additional Active Frontage



Source: ALTU Architects Response Document

As part of the strategy for activating the space between Blocks A and B, a café garden terrace is proposed to activate the space. This terrace is heavily planted with biodiverse and textured pollinator planting species which will enhance the streetscape and furnished with bespoke furniture including a feature bench and mobile tables. The terrace space with feature paving and space for several outdoor tables, which can spill out from the corner café unit. As this space is south facing it will have a pleasant aspect and add positively to the animation and atmosphere of the street. This space is intended to be a focus in

the emerging community, which will allow a thriving business to establish while creating a place for new neighbours and friends to meet in a central spot in the new town.

Figure 3.5: Landscape Activation



Extract from Landscape Plan: Café garden terrace

Other changes to the building frontages have been made in the architecture and building usages to result in additional activation of the south side of the street with a bike hub and additional retail. The landscape has been adapted to ensure that people can freely access the building entrances and retail windows. Please see Drawing no. 1868_PL_P_01, reference images and sketch below and associated drawings which indicate revisions to the landscape design including the café garden terrace.



Indicative Reference Images: Café Garden Terrace with bespoke furniture

It is noted 3 no. own door ground floor dwellings have been introduced to the south elevation of Block F to help activate the southern link street.

Figure 3.6: Pedestrian Street Activation



Source: 3D Design Bureau

3.6.1 Landscape enhancements

In addition, the landscape design has been enhanced in respect of active frontage. Please see Drawing no. 1868_PL_P_01 and associated drawings which indicate revisions to the landscape design to enhance the streetscapes around the retail frontage and southern link street frontage. This entails additional planting and pavement treatments to reduce the size of the paved areas and add greater variety of treatment, interest and texture, to achieve a finer urban grain in terms of detail.

3.7 ITEM NO. 2(D) – ACTIVE FRONTAGE: BLOCKS A & B

Item no. 2 (d) states:

“There are concerns regarding the street between Blocks A and B, where there are few ground floor units and a significant level of plant / cycle storage etc. There are also concerns regarding the level of active uses along the pedestrianised street. The applicant is requested to consider more active uses (including the provision of more apartments) along this street.”

3.8 RESPONSE TO ITEM 2(D) – ACTIVE FRONTAGE: BLOCKS A & B

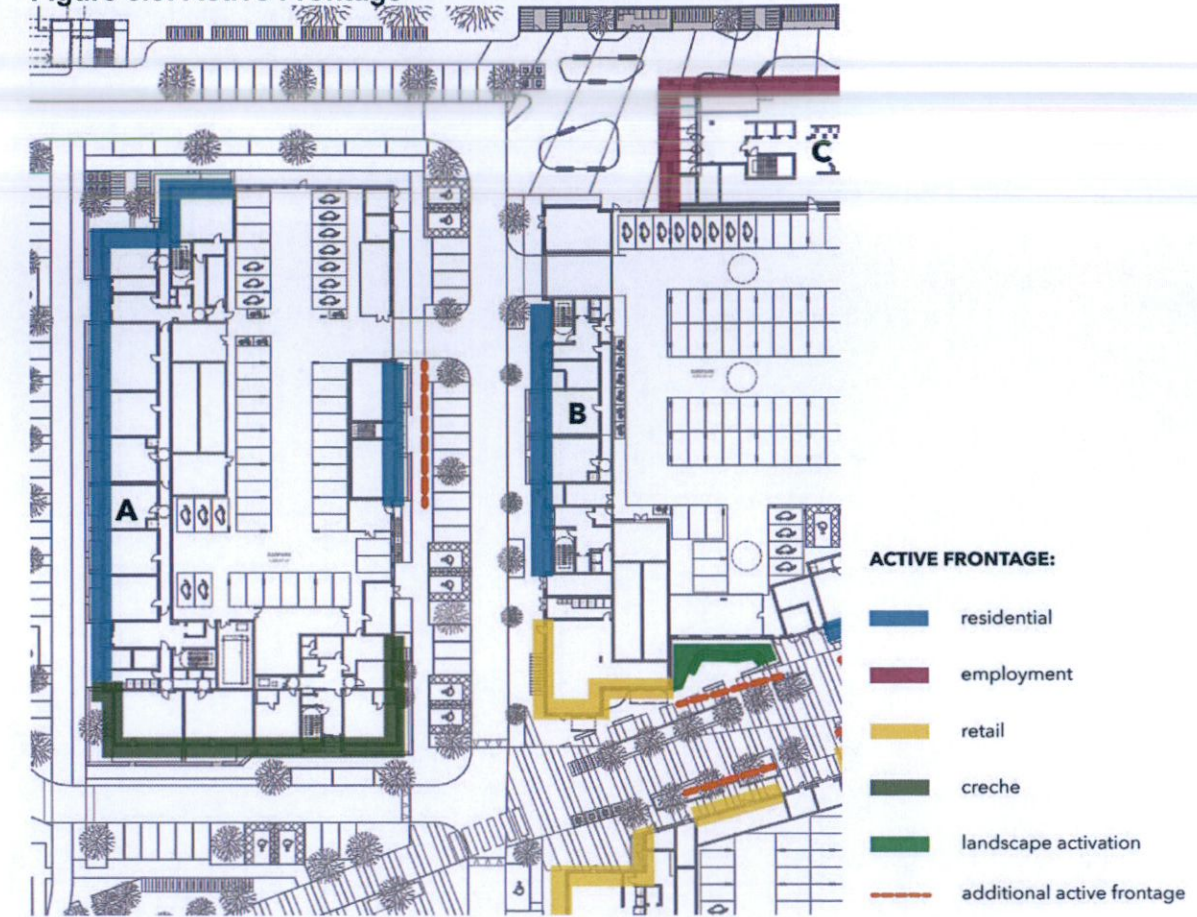
In response to the above, the design has been amended to include 2 no. ground floor own door apartments in Block A, with a further 2 no. above; activating the street along with including the necessary natural ventilation to the under-croft car park. The southern part of the block is activated by the creche and Block B is activated with retail and residential along with employment to the north.

Figure 3.7: Additional frontage



Source: 3D Design Bureau

Figure 3.8: Active Frontage



Source: ALTU

3.9 ITEM NO. 2(E) – ACTIVE FRONTAGE & SHOPFRONT DESIGN

Item no. 2(e) states:

“The applicant is requested to give further consideration to the design of the ground floors of Blocks B, D, E and F and articulate the ground floor main façade, including in terms of shopfront design where areas of retail are proposed and articulation of the main entrances.”

3.10 RESPONSE TO ITEM 2(E) – ACTIVE FRONTAGE & SHOPFRONT DESIGN

In response to this item, as set out above, the applicant has included 9 no. ground floor apartments as well as 3 no. retail units within Blocks B, D, E and F (with 2 no. additional within Block A) as well as landscape activation adjacent to the corner café unit along the southern side of Block B.

Figure 3.9: Active Frontage Block B and Block E



Source: 3D Design Bureau

The articulation and design will provide a high quality pedestrian/retail precinct within this phase of the Clonburris Urban Centre.

3.11 ITEM NO. 2(F) – WASTE STORAGE

Item no. 2(f) states:

“Access to private waste storage in residential schemes should be restricted to residents only - the applicant is requested to confirm that the scheme meets this requirement.”

We refer the Planning Authority to the ALTU response document which details that waste and recycling management for the residents will be provided via 4 no. communally accessed container systems with underground compactors for general waste and recycling. These are situated in 4 no. separate public locations to cater for the waste needs of residents. Blocks G and H will be served with a bin store, with access restricted to residents only.

Dedicated bin stores are provided to retail units, the employment building and the creche. We confirm that access will be restricted to these units.

The RFI response note from Byrne Environmental, included in the submission, provides further detail.

4.0 ITEM NO. 3 REQUEST FOR FURTHER INFORMATION - RETAIL

4.1 ITEM NO. 3 (A) – RETAIL FRONTAGES

Item no. 3(a) states:

“It is noted that, whilst some retail is provided at ground floor, a significant portion of the shopping streets (secondary retail frontage in this instance) are not retail use. Whilst the provision of other uses, such as the creche is welcomed, it is apparent that a significant portion of shopping frontage is dedicated to residential amenities and areas for plant or cycle storage. The applicant is requested to provide more active / flexible space along the secondary frontages.”

4.2 RESPONSE TO ITEM 3(A) – RETAIL FRONTAGES

In response to this item, the ground floor level of the main pedestrian route between the Blocks has been amended so that the previously proposed residential amenity uses are proposed to be replaced with 3 no. additional retail/commercial units amounting to an increase of some 262 sq. m.

Figure 4.1: Retail/Commercial Uses



It is further noted that the height of the proposed own door residential units has been designed for potential future retail/commercial use, should the demand for additional retail/commercial units arise in the future. In addition, 11 no. ground floor apartments have been provided to increase the active floorspace along the streets.

Figure 4.2: Pedestrian Route and Retail Frontage



Source: 3D Design Bureau

4.3 ITEM NO. 3 (B) – RETAIL FLOORSPACE BREAKDOWN

Item no. 3(b) states:

“In accordance with Section 2.5.7 Planning applications for retail development shall provide a general breakdown of retail floorspace to be provided and should have regard to the accompanying Retail Study and the indicative breakdown of floorspace into convenience, comparison and retail services. The applicant is requested to provide this information.”

4.4 RESPONSE TO ITEM 3(B) – RETAIL FLOORSPACE BREAKDOWN

In response to this item, we would note that the proposed development proposes c. 1,150 sq. m of retail floorspace. In addition, it is proposed to provide a commercial creche of c. 609 sq. m.

With reference to unit no. 4, this unit will be a convenience outlet with an associated off licence and deli counter.

It is noted that Section 2.5.3 of the Planning Scheme outlines:

“Clonburris Urban Centre – Core Retail Area The Scheme provides one major retail District Centre at Clonburris Urban Centre, focused on the transport interchange at Fonthill Train Station. For the purpose of retail policy, a Core Retail Area for the Clonburris Urban Centre and Kishoge Urban centres are defined in the Function Map for the Planning Scheme (see Figure 2.5.1). Gross floor area up to a maximum of 12,920 sqm shall be provided within Clonburris Core Retail Area. It should accommodate a minimum of

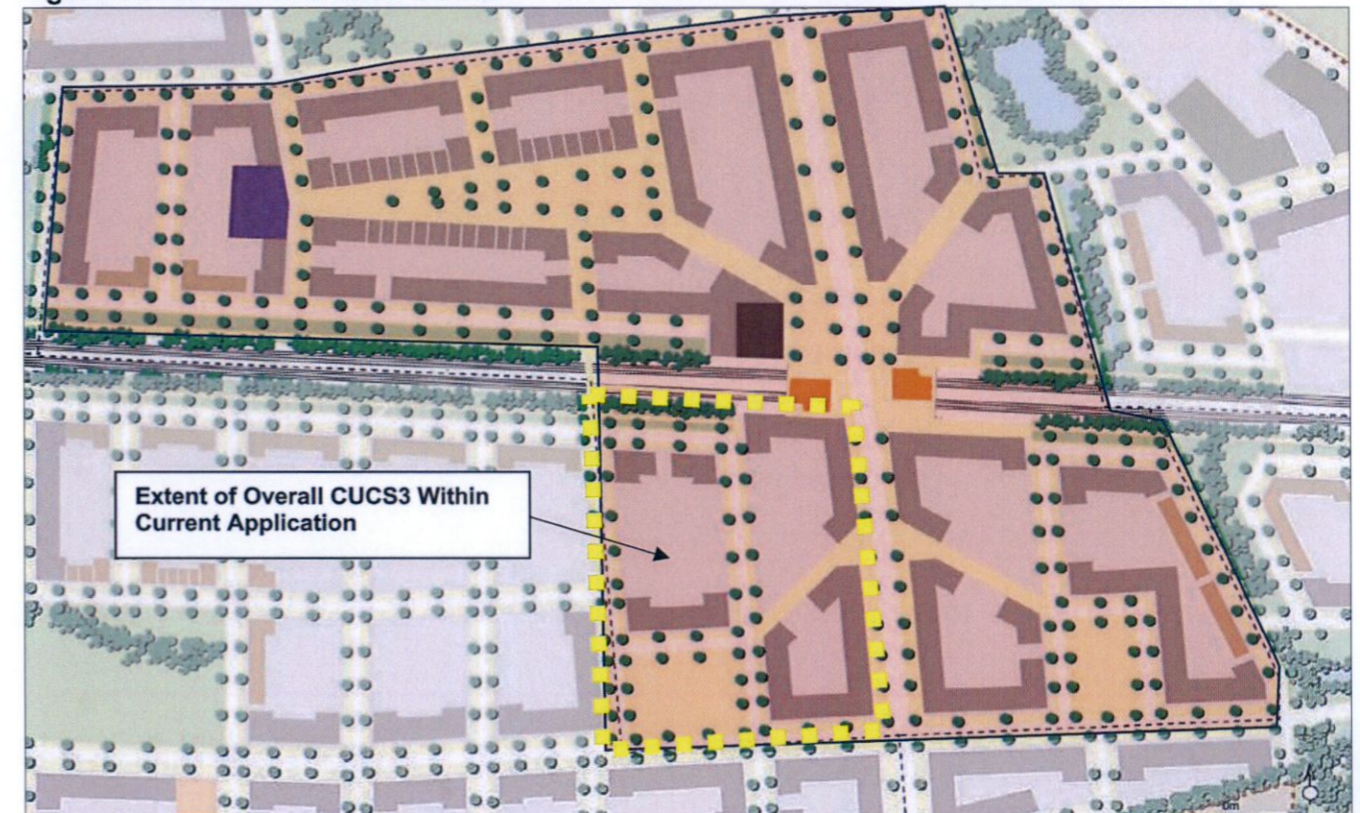
one large supermarket, with supporting comparison goods floor area. Community, leisure, employment, civic and cultural uses should be provided in addition to retail. As a minimum, a District Centre must include one large supermarket and units suitable for use as individual shops and retail service outlets, such as newsagent, pharmacy, doctor’s/ dentist’s surgery, bank and/or estate agency uses. The retail units should be provided principally at ground level in the centre.”

It is noted that this application is the first element proposed by Cairn in respect of the lands in the wider Clonburris Urban Centre, located to the north which Cairn is the owner. Separately, an element of CUC S4 was included in the permitted development SDZ22A/0010 located to the east of the Fonthill Road by Kellend Homes.

Arising from the request for further information, the proposed number and extent of commercial/retail floorspace has been increased by c. 262.5 sq. m resulting in an increase of retail area from 887.5sq. m to 1,150 sq. m.

Under Table 2.13.1 of the Planning Scheme the non-residential floorspace parameters for the CUC S3 sub sector are maximum retail floorspace of 1,600 sq. m and minimum non-retail floorspace of some 4,500 sq. m. The proposed development provides 4,515 sq. m of commercial/office floorspace in Block C and retail/retail services floorspace of c. 1,150 sq. m which is in compliance with Table 2.13.1 of the Planning Scheme.

Figure 4.3: Extent of Overall CUCS3



Source: Clonburris Planning Scheme

It is also noted that in addition to the retail and commercial uses proposed as part of Tile 2, Tile 1 provided commercial office space of c. 626 sq. m.

The following provides details of envisaged uses of the retail/commercial uses:

Table 4.1: Retail/Commercial Uses

| Unit no. | Sq. m | Use |
|------------|-------|---|
| Unit no. 1 | 161 | Café/Restaurant (Retail Services) |
| Unit no. 2 | 137 | Comparison (Pharmacy) |
| Unit no. 3 | 104 | Takeaway |
| Unit no. 4 | 476 | Convenience (with Off Licence and Deli) |
| Unit no. 5 | 132 | Comparison |
| Unit no. 6 | 94 | Comparison |
| Unit no. 7 | 50 | Retail Services |
| | 1,150 | |

Figure 4.4: Breakdown of Uses



4.5 ITEM NO. 3 (C) – RETAIL DESIGN MANUAL

Item no. 3(c) states:

“The applicant is requested to provide a statement that indicated compliance with the 10 design criteria contained within the Retail Design Manual (2012).”

4.6 RESPONSE TO ITEM 3(C) – RETAIL DESIGN MANUAL

In response to this item it is noted that the Retail Planning Guidelines 2012, and the accompanying Retail Design Manual, are *“intended to provide a planning framework for future development of the retail sector in a way which meets the needs of modern shopping formats while contributing to protecting, supporting and promoting the attractiveness and competitiveness of city and town centres as places to live, work, shop and visit.”*

The Retail Design Manual therefore sets out key principles of urban design which might form the framework for policies to promote quality design in development plans and local area plans.

One of the key messages of the Retail Planning Guidelines is that a high level of design quality can make an important contribution to the future health of city and town centres. The Guidelines are therefore accompanied by a Retail Design Manual which sets out key principles of urban design which might form the framework for policies to promote high quality design in development plans and local area plans.

The proposed development has been designed in accordance with the 10 design principles of the manual, as follows:

4.6.1 Design Quality

The proposed development is designed to a high-quality and is finished with high-grade materials throughout and will provide a strong, positive frontage within this element of the Clonburris Urban Centre coupled with the proposed public civic amenity space adjacent.

Building design as opposed to building height is the key determinant in producing an acceptable Landmark Building. Therefore it should be designed in a manner that is distinctive from surrounding buildings both in terms of architectural treatment and use of materials. The office use is situated in the landmark building adjacent to the railway station, in Block C. Architectural diversity and a vibrant mix of finishes, colour and detailing is used to identify the building within the urban centre (CUC-S3)

The buildings are designed to reflect their uses with quality materials and finishes that will endure the test of time. The existing context of the residential areas in the wider surrounding area is characterised by low to medium density housing. These residential areas are typically 2 storeys and finished in a mix of brick and dash. Sectors CUC-S3 and CSW-S3 are located south of a railway line and a main road to the east. The change in scale and density proposed for these sectors will immediately introduce a new character areas within the wider context. The selection of materials will add to this distinctiveness and respectful of this locality and will be used to provide visual variety within the proposed development. A robust and timeless material such as brick will be used as the primary material for the residential buildings. A selection of colours will add variety and will be used to emphasise verticality and create a distinction between these high density building blocks. Retail units and creche building at ground floor levels will have large windows and brick façade.

The main street provides the necessary integration to the whole development, thus becoming its highlight. Through the use of an organizing, unifying paving scheme, itself derived from the multiple facade orientations present, and the adoption of a traffic calmed solution where the main road intersects it, the street acquires its own character that contributes greatly to the sense of place, creating a strong axis with multiple focal points.

Material selection is an important part of placemaking. High quality and varied materials are proposed throughout the scheme. External spaces are designed to minimise hazards or impediments to access or movement. Hard landscape surfaces are chosen for accessibility, slip resistance and to be free draining. The footpaths will generally be permeable paving blocks, with concrete block areas at thresholds and private paths. Permeable surfacing, including resin-bound aggregate or paving blocks, will be used for SuDS reasons and to highlight and aid in wayfinding through the site.

Within the podium courtyards, the primary materials are permeable paving, in a variety of tones to reflect and integrate the building and the landscape. Play spaces will be surfaced with a safe surfacing material, compliant with EN1176/1177. Site furniture would include stone cladded raised planter walls, timber or timber composite seats with steel frames, standard stainless steel cycle stands, cycle shelters and other street furniture as may be required for functional reasons, such as bollards.

4.6.2 Site & Location

The site is located in the South-eastern section of Clonburris SDZ (Strategic Development Zone), Clonburris Character Areas CUC-S3 and CSW-S3 within the Clonburris Planning Scheme.

The Clonburris SDZ Planning Scheme 2019 as it relates to the subject site as outlined, recognises the assets of the existing site, in particular, the Clondalkin/Fonthill Railway Station, a public transport hub. The primary aim of the Clonburris SDZ is to create a vibrant, diverse and sustainable community in the west of Dublin, centred on the provision of a choice of high quality homes with access to a range of services and amenities. The zoned lands are strategically located adjacent to the Kildare/Dublin train line and bounded on the southern side by the Grand Canal which continues east towards the centre of Dublin and to the west.

4.6.3 Character & Context

Several aspects including density, urban structure requirement, place making, and enclosure have been taken into consideration when setting out the parameters for the development of the CUCS3 lands. The development area is divided into 5 separate distinctive character areas, each with their unique quality and mix of uses.

The Clonburris SDZ Planning Scheme identifies the requirement for a landmark building to be located at the north-east corner of the site, adjacent to the Train Station. The proposed development intends to fulfil this criteria through height distinction, material selection and use.

The urban form of the building and façade treatment is designed to make it distinctive from neighbouring blocks. The Employment Building (Block C) is 6 storeys of office use with a higher floor to floor height than residential use. The landmark building has been consciously set apart from adjoining blocks, with Block D modulating down to the north to express Block C's height to Fonthill Road. The articulation of the solid to void in the façade with an extended parapet detail further enhances the verticality of the building's appearance.

4.6.4 Vitality & Viability

The positioning of the different uses within the site has been carefully considered to balance the need to create a clear hierarchy within the public realm with the aspiration to allow the streets and squares to be shared by all users throughout different times of the day. A significant quantum of ground floor active uses such as cafes & restaurants, own door residences, convenience/service retail, grocery stores, crèche, leisure uses, offices and residential entrances will provide a vibrant mix of ground plane activity. Residential uses are provided around all the streets and squares to ensure a level of activity throughout the day and evening. The creche is located at ground and first floor levels in Block A, in close proximity to the urban square with private amenity space at first floor podium level.

A number of retail units are proposed at ground floor level to animate and encourage street activities, within Block B and E. Retail units and creche will have large windows on ground floor encouraging passive supervision. Surveillance from ground and upper floors is excellent across the development with active elevations overlooking all peripheral areas.

4.6.5 Access & Connectivity

The provision of appropriate links which deliver a high level of permeability is a fundamental consideration in the layout. The street network is set out to take maximum advantage and leads to the significant public square to the southwest of the development sector. Clonburris Square, as identified in the Planning Scheme is in close proximity to the railway station and acts as focal point for local, district and wider area bus, cycle and pedestrian movement. Pedestrians are able to move freely throughout the site between the different streets and Urban square. New pedestrian access to the Fonthill road is proposed to connect

with the proposed bus stop and to the existing train station. Changes in level are part M compliant. The streets have been designed in a hierarchy of local streets with pedestrian and cyclists in mind. The majority of the car parking is located in close proximity to the apartment entrances and within the perimeter of the apartment blocks. A small number of on street parking is located adjacent to retail units and away from pedestrian desire lines.

The retail elements of the development are intended to be serviced at restricted times of the day. Servicing will be by means of the dedicated delivery set-down areas in front of Block E.

Car-parking is provided at surface and at ground level in under-croft parking separately below Block A, Block B&D and Block E&F. There are dedicated surface parking spaces for various uses -Office, Retail and Creche. E-Charging spaces are provided as well as electrical infrastructure for selected spaces.

Bicycle parking is provided throughout the development at street level for visitors and users. There is adequate secured bicycle storage provided within under croft parking below each of the residential buildings. There is additional standalone bicycle store for Block C- office building.

4.6.6 Density & Mixed-Use

The proposed development comprises the southwest element (CUC S3) of the new Clonburris Urban Centre located at the eastern end of the Planning Scheme, focussed on the train station at Fonthill. Ultimately as set out in the Planning Scheme, the area will contain a diverse, mixed use development, containing the main retail, commercial and civic uses for the new urban centre based around a public plaza and a transport interchange on Fonthill Road North.

For CUCS3, the proposed (net) density of the development is 133 in a series of 5 no. blocks which provides a focus for the Planning Scheme and also reduces urban sprawl. The proposed development will contribute to the creation of a mixed-use urban environment in CUC S3 where the proposed development also includes some 6,012 sq. m in retail and commercial uses, which includes a creche (609 sq. m), offices (4,515 sq. m), and 4 no. retail units.

The location of the proposed development adjacent to the train station and also bus services in addition to the excellent pedestrian and cyclist connectivity will enhance the sustainability of the proposed development; reducing the proportion of shopping trips made by car in favour of walking, cycling and public transport resulting in a well-designed mixed-use development combining retail, residential and commercial uses.

The use of high-quality materials and the creation of a urban square will further enhances the layout and the creation of a sense of place for CUC S3.

4.6.7 Public Realm

The design intent is to create a high quality and appropriate landscape for future residents, which will meet their recreational needs and provide an attractive visual setting and associated social amenity spaces. The principles of inclusivity for all age groups, universal accessibility and sustainable development are applied to ensure an inclusive and environmentally responsible design solution.

There are three main public open spaces identified within the T2 Lands. The Urban Space (0.52ha), a portion of the linear park (0.72ha), both connected by the Main Street Axis, totalling 1.42ha. Out of a net developable area of circa 4.5ha, Public Open Space accounts for a total of 32% of the subject site.

The proposed development promotes a variety of uses across the site including; a great range of dwelling typologies, a creche and a number of local commercial/retail units. Central to the scheme is Public Open Space. This is the schemes main square and local meeting space - there is secure playground, outdoor

sitting area, bike parking. This open space is surrounded by 4 and 6 storey high buildings with active frontages.

Open spaces are designed for, passive supervision over all areas from dwellings. The scheme is designed to create a series of character areas and visual opening experiences for all to enjoy. A children's playground is accompanied by outside sitting areas designed for supervision. It is located in the hearts of open spaces, far from vehicular traffic.

The main open space elements comprise of an urban space to the south-west and a linear park on the eastern side of the site, connected by the main axis avenue and local streets. Further landscape works include communal amenity areas for the apartments and associated public realm landscape treatments.

4.6.8 Built Form

The CUC-S3 height designations in the Planning Scheme ranges from 3-8 storeys. In the interest of placemaking landmark building was pointed in the designated local node.

A range of 4 to 6 storeys building heights for residential blocks are also proposed to meet the requirement for height and density as prescribed in the document. The northwestern part of Sector CSW-S3 which adjoins CUC-S3, predominantly residential, building height will range from 2 to 4 storeys.

The retail units are incorporated into the layout at ground floor level providing animation and activity along key streets which is an aspiration of the Planning Scheme.

The architectural form and place making which includes a landmark building adjacent to the train station along with the use of durable and high-quality materials and finishes will contribute to the character and quality of both the development itself and the local environment generally.

4.6.9 Environmental Responsibility

Biodiversity measures included will follow the recommendations of the Clonburris Planning Scheme Framework and the Ecologist's recommendations.

The green roof system proposed allows for enhanced biodiversity with native sedum species, wildflowers and shrubs. This results in a more place-specific green roof and enhances the biodiversity value significantly over a standard extensive green roof, which typically has non-native sedum only. The green roofs could be a combination of intensive and extensive roofs.

The extensive roofs will have shallow soil substrate with native sedum species and the intensive areas will have deeper soil and larger plants such as native grasses and shrubs (e.g. Carex, Rosa, Cornus). See diagrams below for details. Micro-mounds for mining pollinator insects and insect hotel / log piles, etc. can also be incorporated.

Planting proposals form a vital part of the strategy for the site, in accordance with County Development Plan objectives and national policy on biodiversity. Green Infrastructure is a term that is used to describe the interconnected networks of land and water that sustain environmental quality and enhance the quality of our lives. The Europe - a Union's Biodiversity Strategy recognises the application of Green Infrastructure policies as a way to maintain bio - diversity and ecosystems in the wider landscape. Green Infrastructure networks operate on many scales, from the national to local, and the protection and enhance - ment of these networks has the ability to positively affect communities into the future, especially in terms of climate change, sustainable development and spatial planning.

In relation to the promotion of active travel, a package of integrated mitigation measures has been identified to off-set the additional local demand that the proposed mixed-use development could potentially generate as a result of the forecast increase in vehicle movements by residents, commuters,

and visitors of the development. The strategy includes specific measures for both the construction and operational stages of the proposed development.

As set out in the EIAR submitted with the application, a management regime will be implemented by the development's management company to control access to the on-site car parking spaces thereby actively managing the availability of on-site car parking for residents of the development.

Infrastructure measures identified to reduce reliance of private vehicles include the provision of ample secure cycle parking on site and ensuring a design which promotes permeability for pedestrians and cyclists to, through and from the development. The high level of high-frequency public transport facilities (Dublin Bus, Irish Rail) will also act as a powerful mobility management measure, as residents can rely on public transport over the private vehicle.

With regard to waste management, the Resource and Waste Management Plan (RWMP) prepared by Byrne Environmental (included with the application) specifically identifies that waste materials generated by construction activities will be managed according to the EPA's Best Practice Guidelines for the preparation of resource management plans for construction and demolition projects, April 2021.

The Operational Waste Management Plan (OWMP) prepared by Byrne Environmental (included with the application) demonstrates how the required infrastructure will be incorporated into the design and operation of the development to ensure that domestic and commercial wastes will be managed to maximise the quantity of waste segregated at source and to reduce the generation of un-recyclable mixed waste streams.

4.6.10 Sustainable Construction

The apartment area generally designed in an efficient plan format, balancing the ratio between floor area and external wall which allows for an efficient and sustainable layout while also creating an efficient thermal envelope. The apartments will be constructed to building regulations standards delivering a high level of energy efficiency in use in line with Part L's near zero energy target.

A building lifecycle report has been prepared and included as part of the planning submission.

The proposed residential aspects of the development will comply with Part L 2022 (Dwellings), and Part L 2022 (Buildings Other Than Dwellings) for non-residential areas. As part of the development's efforts to further reduce energy consumption, the project is targeting a minimum A3 BER (Building Energy Rating) across the development.

The development will be designed and operated with the aim of a reduction in waste generation through construction and operation. Where possible waste streams will be separated on site and recycled or re-used. Where possible local materials will be specified, and in addition materials that contain recycled content will be considered as preferable.

With increasing costs associated with potable water use, the proposed development will incorporate measures to reduce water usage through the appropriate selection of low consumption sanitary fittings, leak detection systems and water monitoring facilities.

As part of the sustainable design strategy, consideration of occupants and staff will be an integral part of the design process. As the productivity and well-being of building users depends strongly on the quality of the indoor environment, the following aspects will be addressed: • Adequate ventilation and filtration; • Low-emitting materials; and • Natural daylight and views to the external environment.

The OCSC Energy & Sustainability Report confirms that if the energy and sustainability strategy is successfully implemented, the proposed Clonburris T2 Residential Development will satisfy all Part L and BER requirements.

5.0 ITEM NO. 4 REQUEST FOR FURTHER INFORMATION – ECONOMIC DEVELOPMENT

5.1 ITEM NO. 4(A) – EMPLOYMENT FLOORSPACE

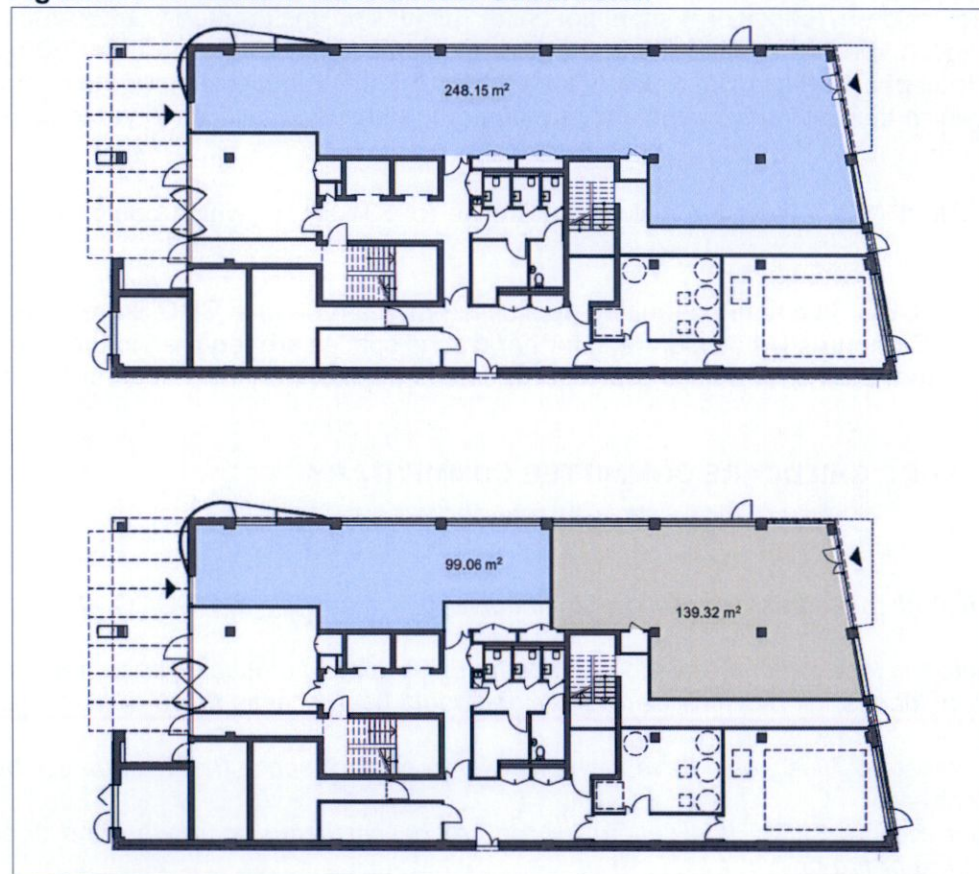
Item no. 4(a) states:

“In accordance with Section 2.6.3 As a minimum, 10% of the employment floorspace provided in the Urban Centres shall be in the form of small-medium sized units/ incubation units of between 100 – 300sq.m. It is noted that the commercial building generally provides large open plan office space in excess of this. The applicant is requested to indicate that 10% of the proposed floorspace meets the above requirement.”

5.2 RESPONSE TO ITEM 4(A) – EMPLOYMENT FLOORSPACE

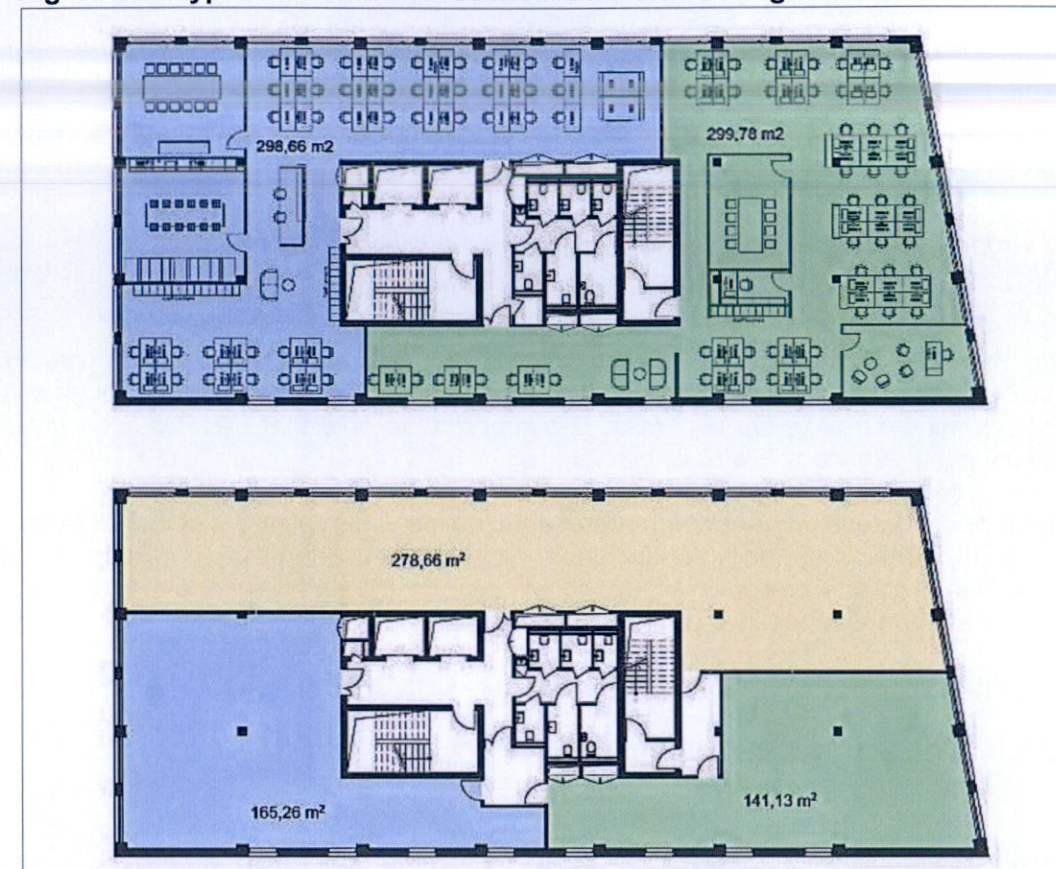
In response to this item, we refer the Planning Authority to the enclosed ALTU Further Information response document included with this submission. The Employment Building (Block C) has been designed for flexibility in subdivision which can provide small-medium sized units/ incubation units of between 100-300sqm.

Figure 5.1: Potential Ground Floor Subdivision



Source: Altu Architects

Figure 5.2: Typical Two and Three Incubator Unit Configurations



The layouts show that either a 2 unit or 3-unit configuration can be readily provided, one floor of the Block C which would equate to providing the 10% requirement (c. 13%).

It is noted that the ground floor can be sub-divided and own door units accommodated, subject to demand.

5.3 ITEM NO. 4(B) – COMMERCIAL USES

Item no. 4(B) states:

“b. Commercial uses such as hotels, hostels, bed and breakfast, funeral homes, public houses, nursing homes, garden centres, conference centres and healthcare facilities etc are generally permitted in principle in the Urban Centres and will form part of meeting the minimum employment floorspace in the Planning Scheme. The applicant is requested to consider if a wider range of uses could be provided in this instance.”

5.4 RESPONSE TO ITEM 4(B) – COMMERCIAL USES

In response to this item, we refer the Planning Authority to the enclosed letter from the Cairn commercial team which provides a rationale for the current quantum of commercial/retail uses in CUC S3.

We refer the Planning Authority to the enclosed letter from Cairn’s property team which notes:

Viability has been a major challenge across all “user” reviews, review current market requirements was essential to make the best decision. Viability is important in both economic and activity fronts. It is not Cairn’s wish to have vacant commercial units as this will impact the residents / community’s experience.

Below summarises our assessment of these uses in relation to T2:

- Hotels / Living – Currently no market requirements and serious viability issues, will be considered in T9
- Public Houses – Considering for T9, will need a large portion of Clonburris developed before this can be considered
- Nursing / Care Home – not an option for landmark location, this is a consideration for T6 and T9
- Garden Centres / larger retail box’s – due to land use guidance this is not an option
- Healthcare Facilities – No engagement from HSE, a viable use however not for the Landmark location.

Regarding retail uses, we have increased the number of retail units from 4 to 6 following the FI. Consideration needs to be given to active frontage verse vacant and unviable units. As it stands viability is a concern as we believe we are now at the very max. It is important to note that Tile 2 is not a major retail core and as such we should be sensitive to its viability.

It is submitted that the proposed development which represents the first Cairn element of the Clonburris Urban Centre, has carefully considered the potential viable commercial uses in the short term as the wider Clonburris residential precincts are built out, which will provide the sufficient scale and ‘critical mass’ for the commercial elements to become viable and sustainable.

6.0 ITEM NO. 5 REQUEST FOR FURTHER INFORMATION - CRECHE

6.1 ITEM NO. 5 (A) – TABLE 2.7.2 CRECHE PROVISION

Item no. 5 (A) states:

“a. Table 2.7.2. requires 200 childcare places for Clonburris Urban Centre. The applicant is requested to set out the approximate number of childcare spaces created and how the remaining spaces could be delivered within the remainder of the CUC Development Area.”

6.2 RESPONSE TO ITEM 5 (A) - TABLE 2.7.2 CRECHE PROVISION

In response to this item, we note that section 2.7 of the Planning Scheme notes the following in relation to the key principles for community facilities and public services:

“To predominantly direct healthcare facilities, childcare facilities and community buildings into the urban centres and to locations that are accessible by public transport, walking and cycling.”

Section 2.7.3 states that:

“There are a network of suitable locations in the Scheme to accommodate the diverse and changing community facilities, namely Clonburris Urban Centre, Kishoge Urban Centre and 4 Local Nodes at Clonburris Little, Cappagh, Grange and Gallanstown. These locations should accommodate a range of small-scale non-residential uses such as childcare facilities, community facilities, individual or small groups of local shops and/or local bus stops consistent with the urban form of the Planning Scheme.”

It is considered that a requirement for pro-rata childcare provision will not necessarily meet the needs of an evolving population or ensure sufficient flexibility in the longer-term and that market factors and government policy will have a greater role. The Scheme requires an approach to early childhood care that combines area based and market led provision. The Scheme requires a minimum of one purpose built facility in the Clonburris Urban Centre and one purpose built facility in Kishoge Urban Centre.

The minimum requirements for childcare spaces across the lands shall be in accordance with Table 2.7.2. The phasing requires the developer to demonstrate available provision is provided in the catchment. The Scheme will also support proposals for early childhood care and education use within employment floorspace, community buildings such as community centres and schools.

The Scheme supports the co-location of purpose-built childcare facilities with primary schools and supports the provision of purpose-built childcare facilities adjacent to existing or planned primary schools. Early childhood care in residential units should only be provided at an appropriate scale to complement purpose-built facilities and are subject to appropriate safeguards. Childcare facilities should be within walking and cycling distance of educational, community facilities and employment locations.

Table 2.7.2 | Childcare Infrastructure Minimum Capacity

| Area | Childcare Places (full day care) |
|-------------------------|----------------------------------|
| Clonburris Urban Centre | 200 |
| Kishoge Urban Centre | 100 |
| Total in Scheme | 900 |

From table 2.72 above, some 900 childcare spaces are required (noted as a minimum capacity).

It is noted that this application Tile 2 (SDZ22A0018) proposes a creche of c. 609 sq. m which could accommodate some 152 no. children, (based on 4 sq. m per child). In addition, the granted SDZ22A/0010 includes a creche c. 599 sq. m, located in CUC S4 to the east, could accommodate a further 150 no. children, giving an overall total of c. 302 spaces, which is above the minimum requirement for the overall Clonburris Urban Centre, which includes significant lands to the north and east, which have yet to come on stream.

In addition, the permitted Tile 1 (SDZ21A/0022) included a creche (c. 534 sq. m) which could provide some 133 no. places.

Noting that this application for CUC S3 and the permitted application SDZ22A/0010 in CUC S4 represent a small amount of the overall Clonburris Urban Centre, if further creche spaces are required in the future they could be provided, and that the no. of places provided to date is appropriate and will cater for the needs of future residents.

6.3 ITEM NO. 5 (B) – SDCC CHILDCARE COMMITTEE COMMENTARY

Item no. 5(b) states:

“b. The applicant is requested to address the following observations made by the SDCC childcare committee:

- The plans are unclear as to the location of the outdoor space for the proposed creche. Please note that per the 2016 Childcare Regulations, all new childcare services should have access to an outdoor play space.
- Universal design would indicate babies would have A small covered outdoor space that is separate but adjacent to the main outdoor area.
- We are concerned about the fire safety implications of the creche not being on the ground floor, especially in the event of a fire or fire drill.
- The proposed room for the 0-1 years is relatively small. There is a considerable demand already in the area for baby childcare spaces. The Clonburris development is highly likely to increase this demand. The toilet would not be required because the baby area has a designated nappy changing area. While we recognise removing the toilets reduces the opportunity for a change of use, we feel that it would increase the space available for babies in the 0-1 age group
- We could not view the Ground floor floor plans; therefore, we cannot determine if there is suitable space for storage, an office, or facilities for staff.

- Are there dedicated parking spaces for the creche?
- Can you clarify the function of the milk area in the rooms designed for 2-3 year olds?
- Natural light is advisable in all rooms. However, will there be sufficient natural light with only one window in the baby room and windows on one side of Homebase 3?
- In relation to the sleep rooms, Homebase will need the space to cater for 7 cots or 5 cots if all children are over 9 months. Homebase 2 if catering for 18 children will require a sleep room size to accommodate 9 cots.
- As the plans indicate a full daycare service, it would be required to serve hot meals. The plans do not show Kitchen facilities.
- We have also observed that there are none of the following indicated in the plans:
 - Separate laundry facilities to avoid cross-contamination
 - Staff toilets and a disability toilet
 - Chef should have separate toilet facilities.
 - The rooms for the 2-3-year-olds should also have changing facilities.”

6.4 RESPONSE TO ITEM 5(B) – SDCC CHILDCARE COMMITTEE COMMENTARY

In response to this item, we refer the Planning Authority to the enclosed letter from Little Rainbows who own and operate five early years services located in Artane, Baldoyle, Belmayne, Donaghmede and Santry; with 2 no. additional creches to come into operation over the next year.

As outlined in the letter, “Little Rainbows have reviewed the plans and consider that the layout would be generally appropriate for a creche operator. Notwithstanding this, it is noted that the layout for a particular operator would be subject to the particular mix of children which in turn impacts on the final layout. In addition, it is noted that the final layout of the scheme will be required to comply with the relevant 2016 Childcare Regulations, Fire Safety in Preschools (1999), Tulsa fit-for-purpose requirements and the Universal Design Guidelines for Early Learning & Care Settings.”

It is highlighted that Circular letter PL 3 /2016 states the following in relation to the early years facilities operating under the Early Childhood Care and Education (ECCE) Scheme and the implementation of the Childcare Facilities Guidelines for Planning Authorities 2001:

“In light of the foregoing, planning authorities are requested to exclude matters relating to childcare facility standards outlined in Appendix 1 of the Childcare Facilities Planning Guidelines 2001– including the minimum floor area requirements per child - from their consideration of planning applications relating to childcare facilities and to solely focus on planning related considerations that fall within the remit of the Planning and Development Act 2000, as amended, in the determination of such planning applications.”
To conclude, it is submitted that the drawings provided by Altu Architects provide an indicative workable layout over 2 no. floors which could be finalised in due course to comply with the relevant 2016 Childcare Regulations, Tulsa requirements and operator layout.”

7.0 ITEM NO. 6 REQUEST FOR FURTHER INFORMATION – LANDMARK BUILDING

7.1 ITEM NO. 6 (A) - LANDMARK BUILDING DESIGN EVOLUTION

Item no. 6(a) states:

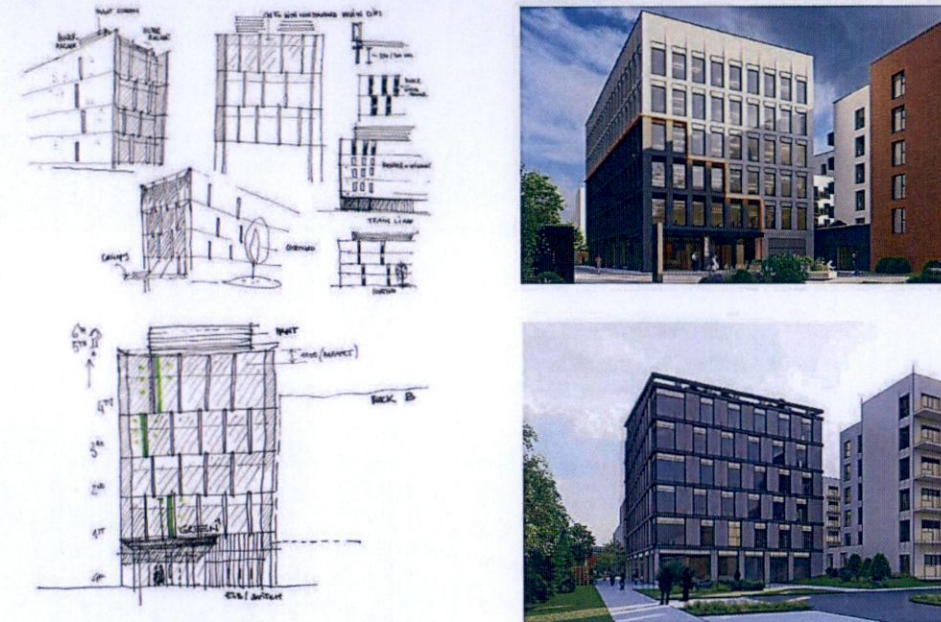
“a. The applicant is requested to detail the design evolution of the landmark building and how the current proposal was deemed the most acceptable design. The applicant should set out details of any design competitions.”

7.2 RESPONSE TO ITEM 6(A) - LANDMARK BUILDING DESIGN EVOLUTION

In response Item no. 6(a) we refer the Planning Authority to the enclosed ALTU Response document which details the design evolution and change to the proposed design.

The design process to select the best and most appropriate design of the Landmark Building was carried out by a group of designers from ALTU architects. Internal workshops and design reviews were organized before presenting various options to the Client and Planning Authority. Working in a BIM environment various options were tested in 3D and numerous CGIs and virtual massing models were created, to study how the proposed building will work in the wider context. Initial proposals with smaller windows, different proportions and more formal articulation of the facade were abandoned as not meeting Landmark building criteria.

Figure 7.1: Design Evolution/Development



Source: Altu Architects

The applicant team has carefully considered the request above and a further evolution of the design approach to the Landmark Building is now proposed. The rationale is to create a stronger vertical emphasis with a clearly defined articulated frame to present a contemporary node building adjacent to the Fonthill Train Station.

The Landmark Building sits on the north-east corner of the site fronting the main railway corridor and will be visible from a distance. As such, the design approach is to create a distinct and coherent form that will act as a key marker for the urban core. The architectural language is completely different to the surrounding brick and render clad residential buildings, adding to its distinctiveness as a landmark. To the east, north and west sides, a glazed façade sits behind a crisply detailed fibre cement frame which continues above the office levels to provide additional verticality and a visual enclosure to the roof area. The depth of the frame also provides a level of solar shading to the office floorplates. A louvred plant screen sits inboard of this to conceal the building's plant and lift motor room. A different fenestration treatment is applied to the south elevation, with a reduction of glazing and a greater ratio of solid façade to provide a level of privacy to the residential courtyard of Blocks B & D and reduce solar gain to the office floors. At ground level, full height glazing is proposed to the office areas and the entrances are signalled by protruding curved elements clad in coloured aluminium panels. A redesign of the landscaping is proposed to create a distinct sense of place, with quality hard and soft landscaping of a more civic nature than the surrounding residential buildings. Please refer to Murray & Associates design statement for more detail. As the building is located at the north of the site, it's increased height in relation to neighbouring blocks will not have an adverse effect on their daylight or sunlight.

Figure 7.2: Landmark Building As Lodged and Further Information



Source: Altu Architects

Figure 7.3: Landmark Building (Further Information)



Source: Altu Architects

7.3 ITEM NO. 6 (B) - LANDMARK BUILDING -PUBLIC REALM

Item no. 6(b) states:

“b. Landmark Buildings should be designed in a manner that is distinctive from surrounding buildings both in terms of architectural treatment and use of materials. To further emphasise their place-making function, Landmark Buildings shall incorporate high quality public realm treatment in terms of surrounding street planting, furniture, lighting and materials etc. The design of such buildings shall therefore be based on a coherent design concept that is clearly communicated via a Design Statement and Landscape Plan.”

In addition to the above and the requirement of Section 2.8.2, Design Statements for Landmark Buildings shall also analyse and illustrate the impact of the proposed development in relation to its immediate and wider context including views/vistas within and beyond the SDZ lands and in terms of sunlight and daylight effects. The applicant is requested to demonstrate compliance with the above.”

7.4 RESPONSE TO ITEM 6(B) – LANDMARK BUILDING -PUBLIC REALM

We refer the Planning Authority to the Murray Associates response document which includes details on the revised public realm surrounding the proposed landmark building. The following changes have been made:

- Landscape design proposals around the landmark building have been substantially revised to create an area with unique character and identity as a setting for the Landmark Building.
- The ground pattern picks up on the oblique geometry of the eastern elevation of the building and ‘fans’ out from there, creating a distinctive plane of contrasting materials, which will be selected to be unique in the urban core area.
- A series of island and edge planters with curved, informal forms will create relief within the hard landscape areas, and will be planted with textured, diverse planting.
- Fire access has also been considered and the route has been revised to allow for greater planting and detail around the landmark building.
- Shared surface paths around the building will allow for continuous and convenient cycle and pedestrian access through the space.
- Please see Drawing no. 1868_PL_P_01, reference images and sketch below, and associated drawings which indicate revisions to the landscape design in the area surrounding the landmark building.

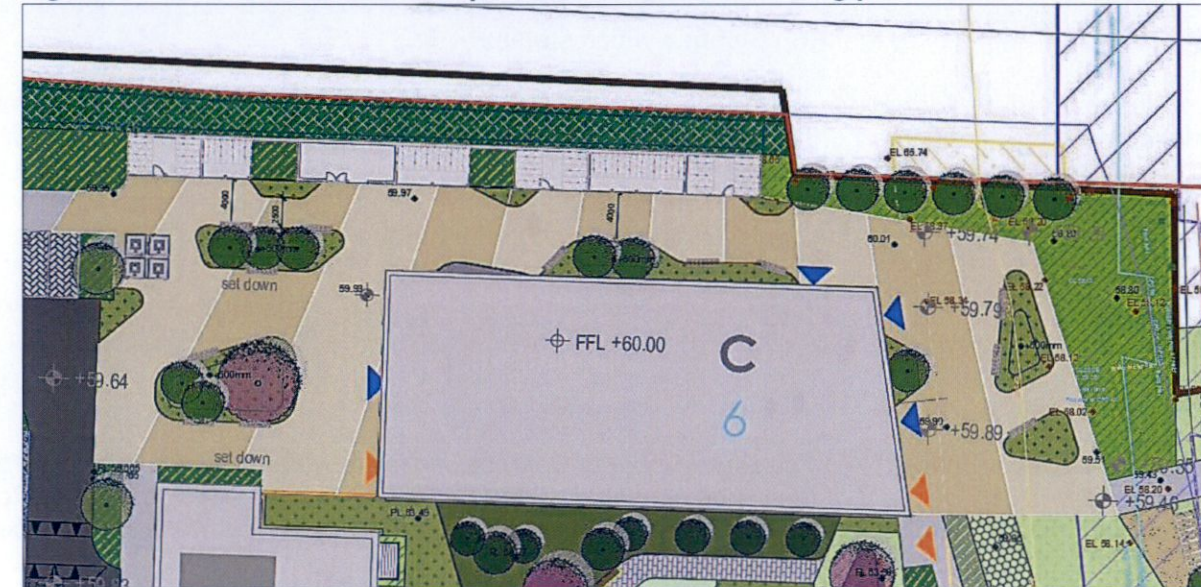


Indicative Reference Images: Island planters in patterned hard landscape context



Indicative Reference Images: Details of island planters with integrated seating

Figure 7.4: Extract from Landscape Plan: Landmark building public realm treatment



8.0 ITEM NO. 7 REQUEST FOR FURTHER INFORMATION – SUNLIGHT AND DAYLIGHT

8.1 ITEM NO. 7 (A) – SUNLIGHT TO AMENITY AREAS

Item 7 (a) states:

“a. Sunlight to amenity – 2 hours of sunlight on March 21st over 50% standard met for existing amenity spaces. Only 92% of proposed amenity spaces meet the requirement. The applicant states all amenity areas will be quality spaces in terms of sunlight, however, no justification is proposed for this deviation, nor is any mitigation proposed. The applicant is requested to provide further justification / mitigation.”

8.2 RESPONSE TO ITEM 7 (A) – SUNLIGHT TO AMENITY AREAS

The IES response outlines that the amenity spaces are significantly in excess of the BRE standards stating:

“As per the BRE Guide, the minimum requirements for amenity spaces to appear adequately sunlit throughout the year is to have at least 50% of the gardens or amenity spaces receive at least 2 hours of sunlight on March 21st. In the case of this project the communal amenities spaces provided are well surpassing this minimum threshold and achieving 2 hours of sunlight on March 21st for 92% of the combined area, therefore exceeding the BRE recommendations. In our opinion there would be no

requirement for a justification or mitigation for the remaining 8% given this high performance with regards to sunlight.”

8.3 ITEM NO. 7 (B) – SUNLIGHT TO AMENITY AREAS

Item 7 (a) states:

“b. Daylight existing buildings – neighbouring dwellings of Phase 1A and 3 assessed. 97% have a proposed VSC not less than 0.8 times their former value. 3 points drop below this value. The justification provided states 2 are dual aspect apartments and have large windows and have windows on a secondary elevation. Also, Phase 1A was tested with a future massing for Phase 2 in place. The applicant is requested to set out the details of the third property that falls below the standard.”

8.4 RESPONSE TO ITEM NO. 7 (B) – VSC VALUES

We refer the Planning Authority to the IES response which states:

“In response to this further information request, as per Section 2.1.6 of the BRE Guide, if the VSC value in the proposed scheme is less than 0.8 time its value (that of the existing) however it is between 15% and 27%, adequate daylight should still be expected if larger than conventional windows are being adopted. Window no.24 has a Proposed VSC value of 20.29 and has larger than conventional windows (total glazed panels of 2.8m by width and 2.25m by height), therefore adequate daylight would be expected within this space.”

9.0 ITEM NO. 8 - REQUEST FOR FURTHER INFORMATION – AVIATION

Item no. 8 states:

“No comments have been received from the Department of Defence. No large water bodies are proposed and it is not considered any element of the proposal would attract birds. The highest building appears to be approx. 85 OD. The applicant is requested to confirm that none of the DoD surfaces would be breached.”

9.1 RESPONSE TO ITEM 8

In response to this item, we refer the Planning Authority to the enclosed report prepared by O'Dwyer & Jones Design Partnership Aviation Planning Consultants which confirms:

“The development site lies under three of Casement Aerodrome’s “Obstacle Limitation Surface”, the lowest of which is Casement’s “Inner Horizontal Surface” at 131.6m OD. However, the site is low-lying site in relation to the Aerodrome, and the highest proposed element – a parapet at 86.15m OD – is lower than all of Casement’s runways, and more than 45m lower than Casement’s “Inner Horizontal Surface”. It is confirmed that no part of the proposed development will breach any of Casement Aerodrome’s “Obstacle Limitation Surfaces.”

10.0 ITEM NO. 9 - REQUEST FOR FURTHER INFORMATION – ROADS

We refer the Planning Authority to the enclosed DBFL Response document and associated drawings which is summarised below.

10.1 ITEM NO. 9 (1) – CONSTRUCTION/LOADING DETAILS

Item no. 9 (1) states

1. The applicant is requested to submit construction/loading specifications for the reinforced grass strip along Block D and F east facades

10.2 RESPONSE TO ITEM 9 (1)

In response to this item, we refer the Planning Authority to the DBFL response which outlines that detail of the proposed reinforced grass product and proposed build-up used is included on the enclosed drawing CLB-CLB-1B-95-SW-DTM-DR-DBFL-CE-5003, Figure E. The reinforced grass as proposed is suitable for a 30 ton fire tender.

“Paved Areas (Grasscrete) Specification Clause: “The Contractor shall install a Grasscrete paved surface as per location shown on drawing 200012-DBFL-RD-SPDR-C-1001 and installed as per drawing CLB-1B-95-SW-DTM-DR-DBFL-CE-5003. Grasscrete formers type GC2, 150mm deep laid on a consolidated sub-base with a 50mm blinding layer of sharp sand. Steel mesh reinforcement to BS4483 reference A252. Concrete 30MN/m2 at 28 days with air entrainment of 3%. 10mm maximum aggregate and a 25mm slump placed around formers and mesh and levelled to tops of formers. After 48 hours melt exposed tops of formers and fill with soil. Following settlement sow Grassmix No GC1 at a rate of 50g/m2 and top up with fine friable topsoil, apply fertiliser as necessary. The Grasscrete shall be installed on a 250mm subbase. Expansion joints shall be incorporated at maximum 10 x 10m centres and shall consist of 25mm wide foamboard filler with 20mm diameter x 300mm long sawn mild steel dowels at 400mm centres with cap and debond to one side. Note 1 The capping thickness is subject to finished level and in-situ testing A maximum CBR value of 2.5% has been assumed at design stage for all areas. Note 2. The requirement for use of geogrid in the capping layers for pavement construction is subject to in-situ testing.”

10.3 ITEM NO. 9 (2) – LOCAL STREETS

Item no. 9 (2) states

2. The applicant should provide justification for the inclusion of Local Streets to South and West of Block G as the SDZ plan designates these streets as a Homezones.

10.4 RESPONSE TO ITEM 9 (2)

Refer to the movement concept drawing CLB-1B-95-SW-DTM-DR-DBFL-CE-1002 showing a comparison between the wider Clonburris proposed movement concept and the movement concept in the Clonburris SDZ. The SDZ movement diagram indicates 4 Local Streets north of Street 12 and 3 Intimate Local Streets north of Street 12, matching the number of Street types proposed for the subject development based on Street hierarchy. The SDZ indicates Street 14 as a Local Street, though it was recommended through a further information request for the adjacent development Clonburris T3, which has been granted Planning, that Street 14 be converted to an Intimate Local Street (Homezone).

Street 18 is proposed as an Intimate Local Street (Homezone) to maintain the 4 Local Streets north of Street 12 and to maintain the Local Street Link north from Street D (Clonburris T1) since Street C has been converted to a Cycle Link instead of a Local Street. While recognising the desire to match the exact street hierarchy as shown in the SDZ movement diagram, adjacent development phases, natural topography and tie-in locations to existing infrastructure has informed the proposed movement diagram which satisfies the overall objectives of the SDZ movement concept.

10.5 ITEM NO. 9 (3) – SIGHTLINES

Item no. 9 (3) states

3. The applicant is requested to submit a revised layout showing adequate sightlines for the development’s junctions.

10.6 RESPONSE TO ITEM 9 (3)

All sight lines have been indicated on the enclosed roads drawing CLB-1B-95-SW-DTM-DR-DBFL-CE-1001 and care has been taken to ensure there are no obstructions to these sight lines.

10.7 ITEM NO. 9 (4) – ROAD SAFETY AUDIT

Item no. 9 (4) states

4. The applicant shall submit a Stage 1 Road Safety Audit

10.8 RESPONSE TO ITEM 9 (4)

The Road Safety Audit is enclosed in this submission and all items have been taken into account for the revised Roads Layout drawing and associated details.

10.9 ITEM NO. 9 (5) – BICYCLE PARKING

Item no. 9 (5) states

5. The applicant is requested to submit a revised layout showing a minimum Bicycle Parking total of 1,362 no. Bicycle Parking spaces as per Table 12.23 of the SDCC Development Plan 2022-2028.

10.10 RESPONSE TO ITEM 9 (5)

As set out in the ALTU Architects Schedule of Areas, the overall bicycle spaces now proposed comprise the following:

Table 10.1: Bicycle Parking Locations and Breakdown

| Bicycle Parking Location | | Breakdown |
|--------------------------|-----------|--------------|
| On Surface Residential | | 314 |
| On Surface Creche | | 12 |
| On Surface | OFFICE | 72 |
| On Surface | RETAIL | 46 |
| Undercroft | BLOCK A | 230 |
| Undercroft | BLOCK B&D | 252 |
| Undercroft | BLOCK E&F | 432 |
| Bike store | BLOCK G&H | 190 |
| Total | | 1,548 |

10.11 ITEM NO. 9 (6) – BIN COLLECTION POINTS

Item no. 9 (6) states

6. The applicant is requested to submit a revised drawing showing the proposed Bin Collection Points located in areas that can be easily accessed at any time by refuse collectors. Bin Collection Points should not be located behind parking bays. This should be accompanied by a swept path analysis showing the collection route of a refuse vehicle.

10.12 RESPONSE TO ITEM 9 (6)

All bin store locations have been indicated on the enclosed road layout CLB-1B-95-SW-DTM-DR-DBFL-CE1001. Refuse vehicle routes to these bin store locations have been tracked to ensure adequate access for refuse collectors.

10.13 ITEM NO. 9 (7) – ROAD SURFACE MATERIALS

Item no. 9 (7) states

7. The applicant is requested to clarify the road surface materials at the junction between Blocks A, B & E, which is part of the Main Street Axis. All items and areas for taking in charge shall be undertaken to a taking in charge standard.

10.14 RESPONSE TO ITEM 9 (7)

The junction between Blocks A, B & E have been revised as well as the proposed construction materials to ensure the design is to SDCC “taking-in-charge” standard as shown on the roads layout drawing CLB-1B-95- SW-DTM-DR-DBFL-CE-1001 and the construction details drawing CLB-1B-95-SW-DTM-DR-DBFL-CE-5003. The revised junction shows a raised intersection with a hot rolled asphalt surfacing with white chips rolled in. The proposed road crossing includes appropriate tactiles and dropped kerbs at crossing points.

11.0 ITEM NO. 10 - REQUEST FOR FURTHER INFORMATION – WATER SERVICES

We refer the Planning Authority to the enclosed DBFL Response document and associated drawings which is summarised below.

11.1 ITEM NO. 10 (A) – ATTENUATION

Item 10(a) states:

“The proposed local surface water attenuation of 975m3 is undersized by approximately 85% for urban areas for a 1 in 100 year storm event. The applicant is requested to submit a report to show revised attenuation calculations use for proposed site at Clonburris.”

11.2 RESPONSE TO ITEM NO. 10 (A) – ATTENUATION

As set out in the DBFL response, the proposed attenuation pond provides a storage volume of 2300m3 with a maximum water level of 59.00m AOD. However, the top of water level for the attenuation pond is 58.366m AOD for the 1:00 year storm event, requiring a storage volume of 1382m3. Surface water storage calculations are provided in Appendix C of the Infrastructure Design Report. Further details for the proposed attenuation pond are shown on the provided planning drawing CLB-1B-94-SW-DTM-DR-DBFL-CE-5005.

The allowable attenuation outflow rate Qbar is 3.1l/s required by the Clonburris Surface Water Management Plan.

11.3 ITEM NO. 10 (B) – SURFACE WATER ATTENUATION

Item 10 (b) states:

“b. The applicant is requested submit a drawing with increased surface water attenuation and show the surface water layout to include connection to main surface water sewer in spine road. Clarify on drawing where proposed subsequent surface water attenuation systems will be.”

11.4 RESPONSE TO ITEM NO. 10 (B) – SURFACE WATER ATTENUATION

As detailed in the “Item 10(a)”, sufficient surface water attenuation storage has been provided within the attenuation pond as proposed on drawings CLB-1B-94-SW-DTM-DR-DBFL-CE-5005 and CLB-1B-94-SWDTM-DR-DBFL-CE-1001. The tie-in points to the downstream surface water network and regional attenuation ponds are as required by the greater Clonburris SDZ Surface Water Management Plan.

11.5 ITEM NO. 10 (C) – SURFACE WATER ATTENUATION

Item 10 (c) states:

“c. The applicant is requested submit up to date surface water drawings. Show how surface water layout drawings compare to drawings at pre designed stage of Clonburris Site.”

11.6 RESPONSE TO ITEM NO. 10 (C) – SURFACE WATER ATTENUATION

See response to Item 10(b) contained in the DBFL Response document included with this submission.

11.7 ITEM NO. 10 (D) – SUDS

Item 10(d) states:

“d. The applicant is requested include SuDS (Sustainable Drainage Systems) in proposed development such as a Green Roof, Swales, permeable paving and other such SuDS. Examples of SuDS can be found in the SDCC SuDS Guide.”

11.8 RESPONSE TO ITEM NO. 10 (D) – SUDS

Green roofs, permeable paving, SuDS tree pits and a large SuDS attenuation pond are provided as part of a suite of the proposed SuDS measures shown on drawing CLB-1B-94-SW-DTM-DR-DBFL-CE-1001. Details of the proposed SuDS features are shown on drawing CLB-1B-94-SW-DTM-DR-DBFL-CE-5003 to CLB-1B-94-SW-DTM-DR-DBFL-CE-5004. The functioning of these SuDS elements are also described in section 3.3 of the Infrastructure Design Report.

11.9 ITEM NO. 10 (E) – CONSULTATION

Item 10(e) states:

“e. The applicant is requested contact water services in SDCC to discuss above issues before resubmitting additional information required.”

11.10 ITEM NO. 10 (E) – CONSULTATION

DBFL met with Brian Harkin as requested on 20/03/2023 to discuss all FI items and agree on all FI responses set out in the DBFL RFI response document and drawings.

12.0 ITEM NO. 11 - REQUEST FOR FURTHER INFORMATION – ARCHAEOLOGY

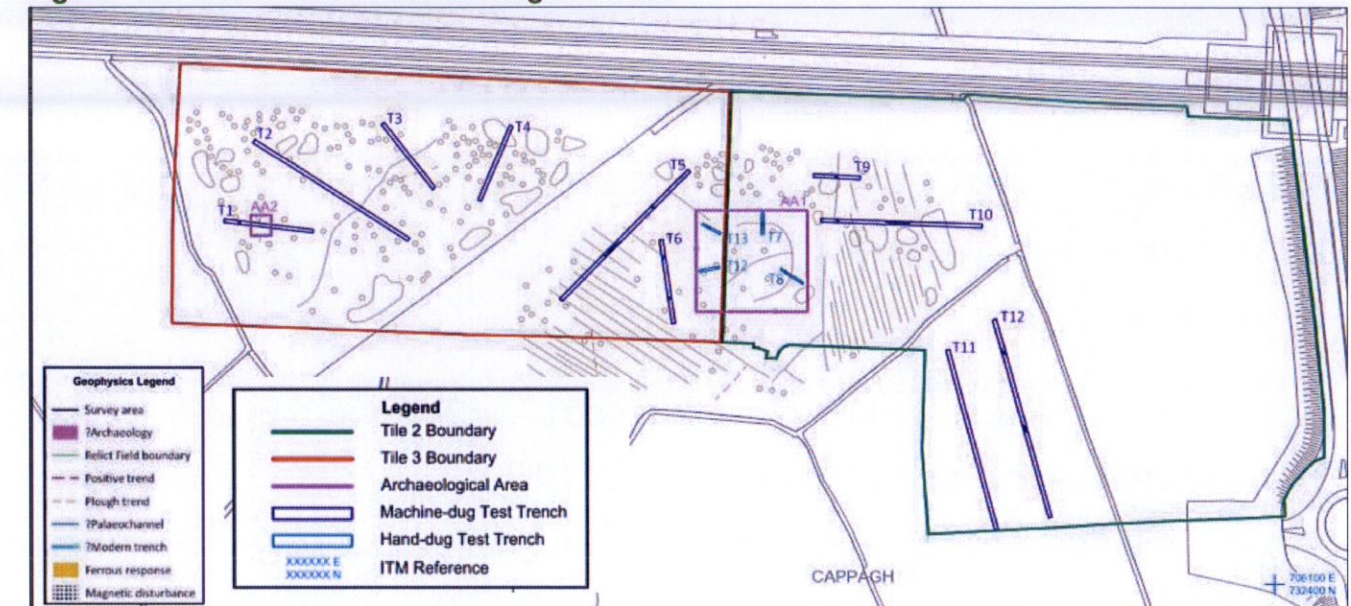
12.1 RESPONSE TO ITEM 11

We refer the Planning Authority to the enclosed Archaeology Assessment prepared by IAC, included with the Further Information Response.

The IAC Report addresses both Tiles 2 and permitted Tile 3 (SDZ22A/0017) as the testing area is located across the boundary of the 2 no. applications.

The assessment was carried out by Fergal Murtagh under excavation licence 22E0719 and metal detection licence 22R0359 during November 2022 and March 2023. It follows a previous geophysical survey report carried out in June 2022 (Dowling 2022). Two additional hand dug trenches were excavated within Tile 3, during March 2023.

Figure 12.1: Detail view of Archaeological Areas



With reference to Tile 2 the IAC report outlines the following:

“Tile 2 Ground disturbances associated with the proposed development may have an adverse impact on the potential archaeological remains identified in Archaeological Area 1, which may relate to c. 80% of the recorded enclosure site DU017-036.

It is acknowledged that the preservation in-situ of archaeological remains is the best form of long-term conservation; however, due to the layout and density requirements of the approved SDZ, it is not possible to facilitate the preservation of AA1. Although the site is a recorded monument, no significant or diagnostic remains have been identified during testing. It is recommended that an area measuring c. 50m x 55m be opened around the possible enclosure DU017-0036 identified in Trenches 7, 8 and 13 (within the full extent of Tile 2) in order to facilitate its preservation by record of AA1. This should be carried out by an archaeologist under licence from the National Monuments Service of the DoHGLH.

Ground disturbances may have an adverse impact on previously unrecorded small or isolated archaeological features or deposits that have the potential to survive beneath the current ground level and outside the footprint of the excavated test trenches.

It is recommended that all topsoil stripping in Tile 2 (with the exception of the disturbed eastern extent) be monitored by a suitably qualified archaeologist. If any features of archaeological potential are discovered during the course of the works further archaeological mitigation may be required, such as preservation in situ or by record. Any further mitigation will require approval from the National Monuments Service of the DoHGLH.”

13.0 ITEM NO. 12 - REQUEST FOR FURTHER INFORMATION – PARKS

13.1 ITEM NO. 12 (A) – REVISED MASTERPLAN

*“a. Impact on existing hedgerows and biodiversity:
Given the removal of hedgerows across this site additional efforts are required to maximise the ecological potential of the retained green corridors especially along the railway line where insufficient space has been allocated.*

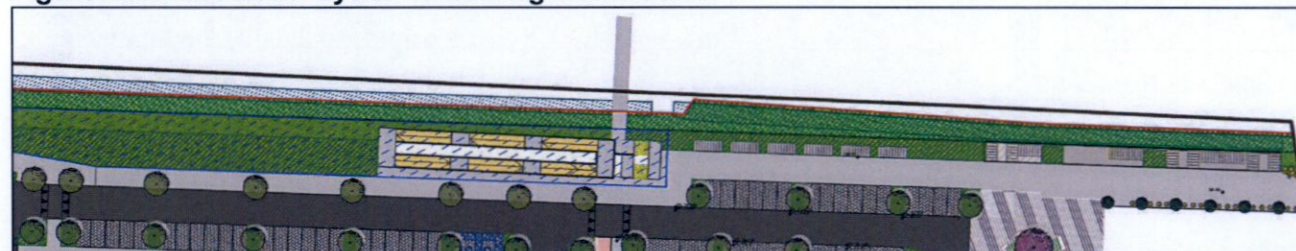
The applicant is requested to submit a revised detailed landscape plan, to be agreed with Public Realm, with full works specification, that accords with the specifications and requirements of Council’s Public Realm Section. The revised landscape plan shall strengthen the northern railway corridor. Currently the Strategic Green Corridor stops at Block H but it is required to continue to Block B. Parking should be removed from this area. Improved accessibility to the main open space is required and further details are required regarding the play provision.”

13.1.1 Additional Planting along Corridor

We refer the Planning Authority to the enclosed Murray Associates response document and drawings which show that the area allocated to planting along the railway line has been increased where possible. The cycle parking in these areas will also be on reinforced grass paving, and planting specifications modified to include more biodiverse and native perennials, grasses, ferns and hedgerow plants where feasible. Please see extracts below which compare the previous and updated layouts and Drawing no. 1868_PL_P_01 / associated drawings which indicate revisions to the landscape design in this area.

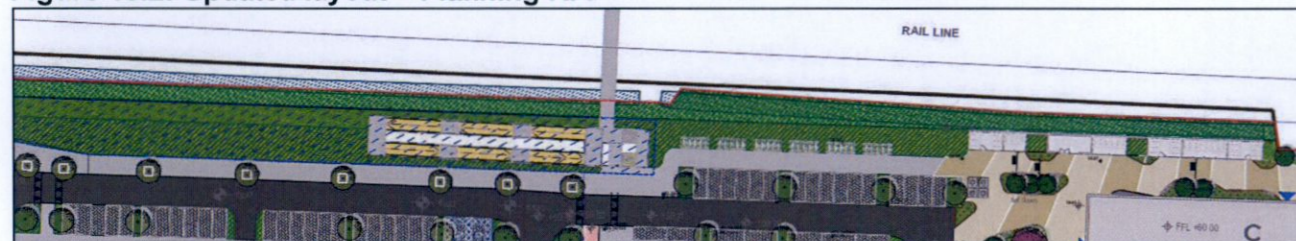
Large stands of existing vegetation are retained on the eastern boundary and these areas are proposed to be planted with significant quantities of new trees (c.85no. individual new native trees and specimen shrubs), as well as large areas of native wildflowers and pollinator perennials. Note that no changes have been made in this area in response to the RAI, as we consider that it meets the required standard in terms of biodiversity.

Figure 13.1: Previous layout – Planning submission



Source: Murray Assoc.

Figure 13.2: Updated layout – Planning RFI



(Note: shows that the path has been narrowed to the minimum for a shared path and additional planting included where possible, as well as reinforced grass for cycle parking spaces.)

13.1.2 Landscape Specifications

Shrub planting has been omitted from the specifications for the project for public realm areas that will be taken in charge. Where groundcover planting is proposed, it is now formed from dense perennial planting, hedgerow, meadow grass or amenity lawn grass as appropriate which accords with parameters set for planting specifications provided by South Dublin Co. Council Parks Department for the adjoining permitted development, Phase T1A (Reg. Ref. SDZ21A/0022).

Figure 13.3: Inclusion of Accessible Route



The area allocated to planting along the railway line has been increased where possible. The cycle parking in these areas will also be on reinforced grass paving, and planting specifications modified to include more biodiverse and native perennials, grasses, ferns and hedgerow plants where feasible. Please see extracts above which compare the previous and updated layouts and Drawing no. 1868_PL_P_01 / associated drawings which indicate revisions to the landscape design in this area.

An additional accessible route has been incorporated into the layout of the main open space, on the eastern side. Please see extracts below which compare the previous and updated layouts, diagram showing accessible routes and Drawing no. 1868_PL_P_01 / associated drawings which indicate revisions to the landscape design in this area.

13.2 RESPONSE TO ITEM NO. 12 (A) – REVISED MASTERPLAN

13.3 ITEM NO. 12 (B) – STRATEGIC GREEN CORRIDORS

b. the applicant is requested to set out measures to strengthen the Strategic and Local Green Infrastructure connections within the site and to clearly identify links to adjacent existing / approved corridors.

13.4 RESPONSE TO ITEM NO. 12 (B) – STRATEGIC GREEN CORRIDORS

We refer the Planning Authority to the Murray Associates response document which details that Additional trees and planted areas have been incorporated into the design of the Strategic and Local Green Infrastructure connections on the northern and eastern boundaries, as described above and also along the north-south roads where possible.

13.5 ITEM NO. 12 (C) – SDCC GUIDANCE

c. Modifications are required to comply with SDCC’s Guidance. These changes include swale widths, driveway details and water flow into tree pits. The applicant is requested to submit revised details that indicate compliance.

13.6 RESPONSE TO ITEM NO. 12 (C) – SDCC GUIDANCE

The Murray Associates response notes that there are no swales or driveways in the current application. SuDS tree pits have been incorporated into the scheme and coordinated fully with DBFL Engineers. Almost all street trees will contribute to attenuation of stormwater. Details of water flow and details of the proposed tree pits can be seen on DBFL’s drawings and have also been reviewed and coordinated with this office. Please see extract from DBFL’s plans below which gives an indication of the water flow and tree pit types utilised. The proposals also include for extensive areas of permeable paving which will further attenuate water flow.

Please also see DBFL Engineers’ drawings and documents for full details of SuDS proposals and drainage.

13.7 ITEM NO. 12 (D) – NATURAL PLAY

d. The concept of using natural play and equipment is acceptable, however, the applicant is requested to provide further details.

13.8 RESPONSE TO ITEM NO. 12 (D) – NATURAL PLAY

As set out in the Murray Associates response document, in this phase of development, the emphasis in the public realm is on playful space to facilitate natural play, rather than play equipment or typical natural play elements such as logs or stepping stones. In the main open space, the tiered changes in level, open grass areas, swale channel / running water and ‘stage’ area are all considered to create opportunities for natural, exploratory and imaginative children’s play. Similarly, in the open spaces to the eastern boundary, the slopes and plantations create ideal places for informal play and exploration.

Furthermore, the natural play areas within the first floor level residents’ courtyards will be equipped with natural play equipment and elements. The detailed design of these spaces will be provided following planning. It is not possible to provide definite design proposals for all elements of the play area at this stage due to the health and safety requirements for playground design. Input from a specialist play designer and management companies will be required to inform the detailed design of these elements, which will not be taken in charge. In this way, the play provision can be bespoke and considered.

13.9 ITEM NO. 12 (E) – NATURAL PLAY

e. Accessibility of main open space/park: Wheelchair access from the main street axis/multi-function plaza into the park is by a very indirect.

13.10 RESPONSE TO ITEM NO. 12 (E) – NATURAL PLAY

We refer the Planning Authority to the enclosed Murray Associates response document which notes that an additional accessible route has been incorporated into the layout of the main open space, on the eastern side. Drawing no. 1868_PL_P_01 / associated drawings indicates revisions to the landscape design in this area.

14.0 ITEM NO. 13 - REQUEST FOR FURTHER INFORMATION –EIAR

Item no. 13 states:

“Further information is required relating to a number of matters that may impact the EIAR. As such, where relevant an update to the EIAR should be provided via additional information.”

14.1 RESPONSE TO ITEM NO. 13 – EIAR

This section of the assessment sets out the main environmental issues in relation to the proposed alterations. The topics in the original Environmental Impact Assessment Report submitted with the application are reviewed in Table 8.1 below in the context of the proposed further information submission.

Table 14.1: Topics From Environmental Impact Assessment Report

| Chapter | Title | Content |
|---------|---|---|
| 1 | Introduction and Methodology | No new considerations arise in relation to impacts on the environment. |
| 2 | Project Description and Alternatives Examined | The description of the proposed alterations are outlined in Section 4 above. The proposed amendments are minor in nature and relate to the inclusion of additional residential units (in place of plant and amenity areas), revisions to design of landmark building, additional retail floorspace as well as associated revisions to landscaping. |
| 3 | Population and Human Health | The proposed alterations do not alter the findings of Chapter 3 of the EIAR. The proposed mitigation within the original EIAR remain in place and there are no proposed changes to these measures. The proposed alteration will have a positive impact on population through the provision of 13 no. additional units of accommodation. |
| 4 | Biodiversity | The proposed alterations do not alter the findings of Chapter 4 of the EIAR. The proposed mitigation within the original EIAR remain in place and there are no proposed changes to these measures. |
| 5 | Land and Soils | The proposed alterations do not alter the findings of Chapter 5 of the EIAR. The proposed mitigation within the original EIAR remain in place and there are no proposed changes to these measures. |
| 6 | Water | There is no material change or additional impacts previously identified in the Water chapter of the original EIAR and no additional mitigation measures are required. |

| Chapter | Title | Content |
|---------|------------------------------------|---|
| | | The engineering detail enclosed from DBFL confirms no material change to the watermain, storm and foul drainage requirements. |
| 7 | Air Quality and Climate | There are no material change or additional impacts previously identified in the Air Quality & Climate chapter of the original EIAR and no additional mitigation measures are required. |
| 8 | Noise and Vibration | There are no material change or additional impacts previously identified in the Noise and Vibration chapter of the original EIAR and no additional mitigation measures are required. |
| 9 | Landscape & Visual Impact | No new considerations arise in relation to impacts on the environment. Any potential impact on landscape and visual amenities would be negligible with no material changes to footprint of development along with amendments to the landmark building finishes and appearance. It is considered that the proposed amendments will not result in any significant change to the overall appearance of the proposed scheme or to the nature or scale of the landscape and visual impacts as assessed in EIAR Chapter 9. |
| 10 | Material Assets – Utilities | No new considerations arise in relation to impacts on the environment in respect of utilities to those previously identified in the Utilities chapter of the original EIAR and no additional mitigation measures are required. |
| 11 | Material Assets – Waste Management | There are no material change or additional impacts previously identified in the Waste Management chapter of the original EIAR and no additional mitigation measures are required. |
| 12 | Material Assets – Traffic | No material change relative to parking with no additional issues arising. |

| Chapter | Title | Content |
|---------|--|---|
| 13 | Archaeology, Architectural and Cultural Heritage | The proposed alterations do not alter the findings of Chapter 13 of the EIAR. Additional testing was undertaken in the interim and are set out in the report by IAC included with the Further Information submission. The proposed mitigation within the original EIAR remain in place and there are no proposed changes to these measures. |
| 14 | Interactions of the Foregoing | It should be noted that the detail in relation to interactions between environmental factors is covered in each chapter of the main EIAR document. With regard to the amendments to the proposed development, it should be noted that in conjunction with each member of the project team, the design team, in association with John Spain Associates, have coordinated the overall application submission, as demonstrated in the accompanying documentation and it can be confirmed that no significant impacts have arisen in respect to the interactions with the different environmental factors, as confirmed by relevant consultants noted in the foregoing table. |
| 15 | Risk Management | There are no material alterations within the proposed development which would alter the finding of Chapter 14 of the original EIAR. |
| 15 | Summary of Mitigation and Monitoring Measures | The mitigation measures set out in the original EIAR submitted will also apply to the amended scheme for the subject lands and in the main no additional mitigation measures are necessary. |

Having regard to the above it is considered that the extent and character of the alterations requested are such that the alterations, were they to be made, would be unlikely to have significant effects on the environment. It is considered no new considerations arise in relation to impacts on the environment which were not considered in the assessment of impacts under the parent permission (Planning Reg. Ref. SDZ21A/0022). Having regard to the modest nature of the proposed development, and the distance of the site from nearby sensitive receptors, there is no real likelihood of significant effects on the environment arising from the proposed development. The need for environmental impact assessment can, therefore, be excluded.

15.0 CONCLUSIONS

It is considered that the above, read in combination with the accompanying inputs by Altu Architects, DBFL Consulting Engineers, and CSR Landscape Architects positively addresses the points raised by the Planning Authority in the Further Information Request.

The proposed development is considered to constitute proper and sustainable development and will contribute to a high-quality vibrant and mixed use residential environment. On this basis, it is respectfully submitted that the proposals are granted planning permission.