

**FURTHER INFORMATION RESPONSE
TILE 3 - SDZ22A/0017**



PREPARED FOR

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**IN RESPECT OF A
PROPOSED RESIDENTIAL DEVELOPMENT AT
CLONBURRIS TILE 3
April 2023**

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1.0 INTRODUCTION

On behalf of the applicant, Cairn Homes Properties Ltd., we wish to respond to the request for further information issued by the Planning Authority on the 7th of February 2023 for a proposed residential development at Tile 3 - SDZ22A/0017, Clonburriss.

The Revised Site Layout Plan prepared by MCORM Architects shows the overall layout in context.

The revised scheme includes the following architectural amendments to the layout:

- All amendments to streets outlined in the request for further information have been incorporated. Footpath widths
- have been increased to 2.5m and street widths as set out in the planning scheme are proposed.
- Parking Arrangements on Streets 12, 14, 15, 16, 17 & 18 have been amended in line with recommendations in the RSA relating to appropriate sightlines and road crossing points. Street 14 revised to increase width and provide uninterrupted green corridor linking local green corridor to local park.
- Apartment Block Moved east to increase distance between Block and future Bridge.
- Apartment Block Revised to increase quantum of brick proposed.
- Proposals for Fine Urban Grain revised.

The civil engineering amendments relate to the following:

- Updated cycle track alignment connecting Clonburriss T3 to the Link Street
- Updated bicycle stand/storage locations as indicated on the Roads Layout
- Updated footpath widths across the entire site to 2.5m minimum
- Street 14 changed to 4.8m wide Homezone instead of a Local Street
- General inclusion of tactile paving at all pedestrian crossing points and desire lines
- Inclusion of regular pedestrian footpath connections from road carriageway to footpaths behind parking spaces for house bin store access
- Rearranged house parking arrangement near all road junctions to avoid clashes with sight lines

Please find the following documents (6 no. copies) enclosed in support of this submission:

- Revised Drawings by MCORM Architects
- Response to Further Information – MCORM Architects
- Revised Drawings by DBFL Consulting Engineers
- Response to Further Information – DBFL Consulting Engineers
- Road Safety Audit – DBFL Consulting Engineers.
- Revised Landscaping drawings – CSR
- Response to Further Information – CSR
- IES Further Information Daylight/Sunlight Report
- CGIs by 3D Design Bureau (in MCORM Response).
- Archaeology Assessment prepared by IAC.

This response to the request for further information has carefully considered the items raised by South Dublin County Council. In accordance with the Further Information Request, this response has been prepared and informed by discussions with South Dublin County Council.

2.0 ITEM - NO. 1 REQUEST FOR FURTHER INFORMATION - PHASING/DENSITY

2.1 ITEM NO. 1(A) - PHASING/DENSITY

Item no. 1(A) states:

“In order to ensure compliance with the scheme, the applicant is requested to provide the following detail: The applicant has summarised the phasing requirements applicable up to Phase 1A 0-1, 000 units. There are a total of 1,995 units either granted permission or included in planning applications currently under consideration. If all of these units were granted, constructed and occupied, they would take up the vast majority of the phasing allowance up to 2, 000 units when the Phase 1B (1, 001-2, 000 units) requirements are required to be delivered. The applicant is requested to provide details on Phase 1B.”

2.2 RESPONSE TO ITEM NO. 1(A)

In response to this item, it is important to note that the phasing thresholds are triggered by the occupation of units rather than the granting of planning permissions and therefore is a matter for ongoing monitoring and review by the Development Agency.

While there have been a number of recent planning applications permitted and currently in the planning system; to date, no dwellings have been occupied in the Clonburriss SDZ. Having regard to this, the phasing requirements are within Phase 1A (0-1,000 residential units). Notwithstanding this, in the interests of providing an update to the Planning Authority, progress in respect of the delivery of the elements of Phase 1A and 1B are provided. It is intended that this information will be updated with the planning applications that are lodged.

In summary, some 944 no. units have been permitted across the 2 no. catchments comprising 681 no. in Clonburriss and 263 no. in Kishoge. A further 1,045 no. units are at Further Information stage or clarification of further information.

In relation to envisaged occupation of the current Tiles which Cairn have either received planning permission or are at Further Information the following is noted:

- Permitted Tile 1 (SDZ21A/0022) - c. 100 units occupied by the end of 2023. 563 no. units complete/occupied by the end of Q4 2024.
- Proposed Tile 2 (SDZ22A0018) - c. 220 units occupied by Q4 2025. 594 no. units complete/occupied by the end of Q3 2027.
- Proposed Tile 3 (SDZ22A0018) - c. 80 occupied by Q4 2024. 157 no. units complete/occupied by Q3 of 2025.

Table 2.1 – Indicative Timelines for Completion/Occupation of Units by Cairn

		2023	2024	2025	2026	2027
Tile 1	563	100	463			
Tile 2	594			220	250	124
Tile 3	157		80	77		
Total	1,314	100	543	297		
Cumulative	N/A	100	643	940	1,190	1,314

Note: Delivery indicative and may be subject to market demand.

Table 4.2 of the Scheme identifies the infrastructure required to be linked to the delivery of residential development and phased in accordance with the construction of residential units. Table 4.3 sets out the phasing for the overall scheme which is set out below in Table 1 with commentary on the considerable progress made to date by South Dublin County Council and the landowners/stakeholders in the Clonburriss SDZ.

2.2.1 Phasing in Planning Scheme

As set out in the Planning Scheme, “the phasing programme is based on the premise that the number of dwelling units that may be constructed and occupied in each phase of development and is dependent on a predetermined amount of works to provide infrastructure, services, facilities and amenities having been completed to serve each phase.”

Table 4.3 of the Planning Scheme sets out the phasing programme for the development of the SDZ lands. A number of important prior to commencement plans / strategies are required to be prepared. These are a Surface Water Management Plan to implement the Surface Water Strategy, a Parks and Landscape Strategy, and Water and Wastewater Plans. These Strategies have been prepared and agreed with South Dublin County Council.

Summary of Phasing Programme		
Element of Phasing Approach	Application	Description
1. Phasing Table (See Section 4.3)	Applicable to all Planning Scheme development	Strategic Phasing Requirements
2. Place Making Requirements (See Section 4.4)	Applies to the construction of residential units within the catchments of Clonburris Retail Core and Kishoge Retail Core	Secondary Phasing Requirement - Place Making Requirement Table is a supporting table to Phasing Table
3. Local Level Infrastructure (See Section 4.5)	Local level physical infrastructure	Secondary Phasing Requirement - Local Level Infrastructure Table is a supporting table to Phasing Table

Figure 2.1 – Site Layout SDZ20A/0021 – Roads and Drainage Infrastructure & Phasing



It is noted that the Planning Scheme does not specify a commencement location for the construction of residential units for any phase of residential development stating that “The location for commencing development in the Planning Scheme and within Development Areas is flexible. However, apart from exceptional circumstances, when development commences in a Development Area, subsequent permissions and development in the subject Development Area shall be required to integrate with the previous development in a sequential way.”

The Phasing Table (Table 4.3) for the Planning Scheme details a schedule and programme to plan and deliver strategic infrastructure and amenities on a phased basis, in tandem with the development of residential units in the Planning Scheme. The phasing programme is based on the premise that the number of dwelling units that may be constructed and occupied in each phase of development is dependent on a predetermined amount of works to provide infrastructure, services, facilities and amenities having been completed to serve each phase. As such, in the event that the minimum requirements of the Phasing Table are not delivered for a particular phase, a restriction on the construction and occupation of residential units in the next phase will apply.

With reference to the above, the proposed sequencing of development within Clonburris South West Development Area by Cairn is predicated on an east to west build out, which includes Clonburris Urban Centre (CUC1 – Tile 2) and the development of the parks, (Grand Canal, Urban Square, and open space in Tile 1), as well as the roads and infrastructure works (under SDZ20A/0021), which has permission and has commenced development and will be constructed in 2 phases. The first infrastructure phase 1A will be completed in September 2023, and the remainder Stage 1B will be completed in April 2025 (with Tiles 4 and 5 complete in August 2024).

Table 2.2 – Permitted and Proposed Applications Clonburris

Reg. Ref.	Development Area	No of Units Proposed and Permitted	No of Units Permitted	Planning Status	No. of Units Constructed
SDZ21A/0022 (Tile 1)	Clonburris South West	563 (as permitted)	563 (as permitted)	Granted	0
SDZ22A/0017 (Tile 3)	Clonburris South West	157		Further Information	0
SDZ22A/0018 (Tile 2 and Tile 3)	Clonburris Urban Centre and South West	594		Further Information	0
SDZ22A/0010	Clonburris Urban Centre and South East	294		Clarification of Further Information	0
SDCC Part 8 application	Canal Extension	118	118	Approved	0
Sub Total Clonburris Catchment		1,726	681		0
SDCC Part 8 application	Kishoge South West	263	263	Approved	0
Sub Total Kishoge Catchment		263	263		0
SDZ23A/0004	Adamstown Extension	385		Further Information (6th April 2023)	0
Sub Total Adamstown Catchment		385			0
Overall		2,374	944		0

Table 2.3 – Phasing Table

Phase 0 Prior to commencement of development	
Prior to the commencement of development, strategic district level measures and detailed design shall be prepared by the developer(s) and agreed with South Dublin County Council through a Surface Water Management Plan to implement the prepared Surface Water Strategy.	This has been approved by SDCC.
Prior to the commencement of development, a strategic level Parks and Landscape Strategy (incorporating a Biodiversity Management Plan) shall be prepared by the developer(s) and agreed in writing with South Dublin County Council.	This has been approved by SDCC.
Prior to the commencement of development, detailed Water and Wastewater plans for the Planning Scheme shall be prepared by the developers and agreed with Irish Water and South Dublin County Council.	This has been approved by SDCC.
Phase 1A 0-1,000 Residential Units Constructed and Occupied	
Retail Core development in accordance with the Place Making Requirements.	The applicant is progressing with the preparation of applications for the provision of retail units and is committed to providing retail facilities in accordance with the Clonburris SDZ requirements. It is noted that Tile 2 includes some retail and commercial floorspace which is considered appropriate in the interim.
Development shall accord with the Local Level Infrastructure Requirements in relation to agreeing timeline for the opening of Kishoge Train Station & completion of the realignment of Lock Road (R120)	Not Applicable to CUC and CSW Development Areas It is noted that Irish Rail have confirmed it is their intention to open Kishoge train station by Q1 2024.
Provision of Water and Wastewater infrastructure on a pro rata basis in accordance with detailed plans agreed with Irish Water and SDCC.	Noted.
The planning of works for the provision of community floor space and the availability of childcare spaces	Permitted Tile 1 (SDZ21A/0022) included a creche (534 sq. m) and community multi-purpose floorspace (c. 74 sq. m). Proposed Tile 2 (SDZ22A0018) includes a creche of c. 609 sq. m.
1B 1,001 – 2,000 (Table 4.3 Phasing Table) Residential Units constructed and occupied	
Planning permission for appropriate elements of Clonburris Retail Core, including retail provision, employment, residential, community floorspace and urban hub.	The Clonburris Retail Core is not required at this point in time. Cairn anticipate a planning application for the Clonburris Retail Core in Q3 2024/ Q1 2025.

Planning permission for Kishoge Retail Core, including retail provision, employment, community floorspace and residential	Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced with South Dublin County Council.
Planning and commencement of works at Griffeen Valley Park Extension or Na Cluainte (North and South) in accordance with the prepared and agreed strategic Parks and Landscape Strategy.	It is noted the Strategic Parks and Landscape Strategy has been approved which provides the framework for the Griffen Valley Park Extension and the Na Cluainte North and South) parks. Cairn will provide further detail prior to occupation of 2,000 th unit.
Agree and make available a school site to the Department of Education (in accordance with requirement of the Department of Education and SDCC)	Cairn has progressed a site with the Department of Education and Skills for a school site north of site at Kishoge and planning permission is in place (under SDZ22A/0011).
Provision of a minimum 1,000 sqm net convenience floorspace and 500 sqm net comparison/ retail services floorspace (As part of the Place Making Requirements for delivery of Kishoge)	Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced with South Dublin County Council. The Phasing of the Urban Core has been the subject of extensive discussions with SDCC.
Provision of a minimum of 500 sqm of community services floorspace (e.g. childcare or health centre) (At Kishoge or Clonburris – subject to location of residential units constructed).	Permitted Tile 1 (SDZ21A/0022) included a creche (534 sq. m) and community multi-purpose floorspace (c. 74 sq. m). Proposed Tile 2 (SDZ22A0018) includes a creche of c. 609 sq. m. SDZ22A/0010 (currently at Clarification of Further Information includes a creche c. 520 sq. m). Future Tile 6 Kishoge (KUC-S4) (to be lodged in Q2 will include a creche.
Design and planning permission for District Park Hub at Clonburris Little.	The District Park hub is located in Kishoge South East (KSE-S1 and KSE-S2; Tile 7 and Tile 8) respectively and Cairn are not constructing in that location in the short term. The District Park Hub will be delivered as part of the development of that area in due course and in tandem with Na Cluainte park. It is noted Community space c. 74 sq. m was permitted in Tile 1.
Planning and commencement of Link Streets with associated undergrounded services in Development Areas where construction of residential units commences or commenced in Phase 1 (A or B).	The Clonburris Southern Link Street (CSLS) has planning permission (Roads and Drainage Infrastructure works application - SDZ20A/0021) and stage 1A (between the Fonthill Road and the western edge of Tile 1 and 3) is anticipated to be completed in September 2023 with works currently underway. The remainder of the CSLS is currently out to tender and will be awarded in June 2023 and completed in April 2025. The element serving CSWS1

	(Tile 4) and CSWS2 (Tile 5) is anticipated to be completed in August 2024.
Provide/demonstrate availability of childcare spaces in the catchment in accordance with Section 2.7.3.	As above.
District and catchment level measures from an agreed Surface Water Management Plan to accommodate construction within the subject development areas of construction	Part of CIL works and will be delivered as part of the construction of the Roads and Drainage Infrastructure works (under SDZ20A/0021). Appropriate District surface water attenuation is incorporated and is anticipated to be completed as per timelines above.
Retail Core development in accordance with the Place Making Requirements	Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced.
Development shall accord with Local Infrastructure Requirements in relation to opening of Kishoge Railway Station	Not required for Clonburris Catchment but Cairn are in discussions with Irish Rail and South Dublin County Council. It is anticipated that the relevant elements will be in place by the end of 2023.
Provision of Water and Wastewater infrastructure in accordance with detailed plans agreed with Irish Water and SDCC on a pro rata basis.	Delivery as set out above in relation to Roads and Drainage Infrastructure works (under SDZ20A/0021).
Phase 2 2,001 – 4,000 (Table 4.3 Phasing Table) Residential Units constructed and occupied	Commentary
Provision of District Park Hub at Clonburris Little	The District Park hub is located in Kishoge South East (KSE-S1 and KSE-S2; Tile 7 and Tile 8) respectively and Cairn are not constructing in that location in the short term. The District Park Hub will be delivered as part of the development of that area in due course. It is noted Community space c. 74 sq. m was permitted in Tile 1. The District Park Hub is to be delivered in tandem with the park (to service the park).
Completion of Link Streets and associated junctions with associated undergrounded in development areas where residential units are constructed (includes bridges where relevant).	The Clonburris Southern Link Street (CSLS) has planning permission (Roads and Drainage Infrastructure works application - SDZ20A/0021) and stage 1A (between the Fonthill Road and the western edge of Tile 1 and 3) is anticipated to be completed in September 2023. The remainder of the CSLS is currently out to tender and will be awarded in June 2023 and completed in April 2025. The element serving CSWS1 (Tile 4) and CSWS2 (Tile 5) is anticipated to be completed in August 2024.

Planning and commencement of works to Griffeen or Na Cluainte (alternate option to park commenced in Phase 1B) in accordance with the prepared and agreed strategic Parks and Landscape Strategy	It is noted the Strategic Parks and Landscape Strategy has been approved which provides the framework for the Griffen Valley Park Extension and the Na Cluainte North and South) parks. Cairn will provide further detail prior to occupation.
Completion of works to Park commenced in Phase 1B at Griffeen or Na Cluainte.	As above.
Agree and make available sites to the Department of Education for Schools (in accordance with requirement of the Department of Education and SDCC).	Cairn has progressed a site with the Department of Education and Skills for a school site north of site at Kishoge and planning permission is in place (under SDZ22A/0011).
Provide/demonstrate availability of childcare spaces in the catchment in accordance with Section 2.7.3 including the delivery of a minimum of one purpose built facility in Kishoge or Clonburris Urban Centre.	Permitted Tile 1 (SDZ21A/0022) included a creche (534 sq. m) Proposed Tile 2 in the Clonburris Urban Centre (SDZ22A0018) includes a creche of c. 609 sq. m.
District and catchment level measures from the Surface Water Management Plan to accommodate construction within the subject development areas.	Part of CIL works and will be delivered as part of the construction of the Roads and Drainage Infrastructure works (under SDZ20A/0021). Appropriate District surface water attenuation is incorporated and is anticipated to be completed as per timelines above.
Retail Core development in accordance with the Place Making Requirements.	Tile 6 (KUC-S4) to be lodged in Q2 2023. Preplanning engagement has already commenced.
Provision of Water and Wastewater infrastructure in accordance with detailed plans agreed with Irish Water and SDCC on a pro rata basis.	Delivery as set out above in relation to Roads and Drainage Infrastructure works (under SDZ20A/0021).

2.3 ITEM NO. 1(B) - NET DEVELOPMENT AREA

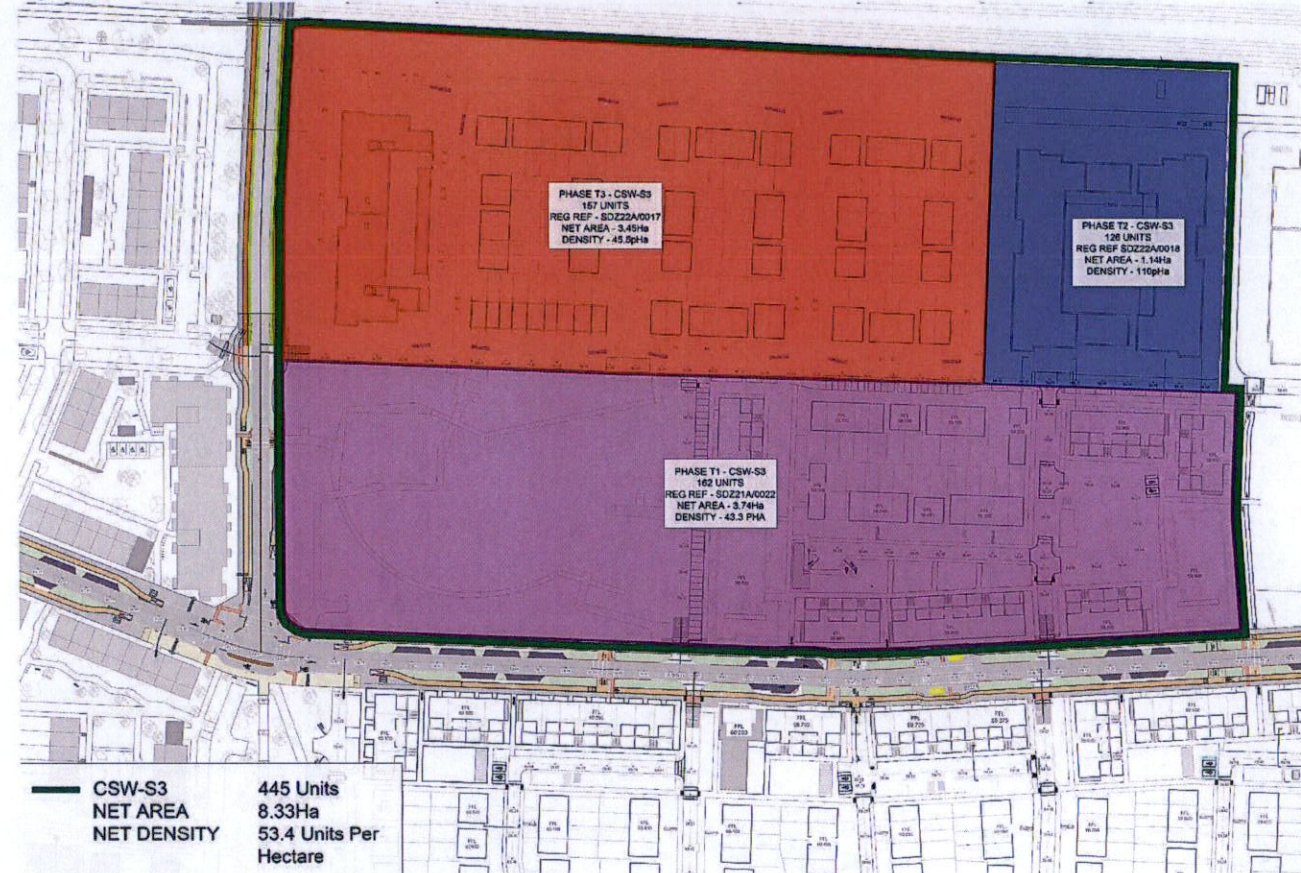
Item no 1(B) states:

“The applicant is requested to detail how the total net development area for CSW-S3 is met through applications SDZ21A/0022, SDZ22A/0017 and SDZ22A/0018.”

2.4 RESPONSE TO ITEM NO. 1(B) - NET DEVELOPMENT AREA

In response to this item, the CSW S3 lands have been subject of 3 no. separate applications comprising Phase Tile 1 (SDZ21A/0022), Phase Tile 2 (SDZ22A/0018) and Phase Tile 3 (SDZ22A/0017) and comprise 8.33 hectares (net) in total.

Figure 2.2 – Net Development Area



Source: MCORM Architect’s Response

Under Phase T1, 3.74 hectares of CSW-S3 were incorporated and 162 units and a local park were proposed. Under Phase T2, 1.14 hectares of CSW-S3 were incorporated and 126 units were proposed. As part of T3, 157 units and the local green corridor were proposed on the remaining 3.45 hectares.

2.5 ITEM NO. 1(C) - DENSITY CALCULATIONS

Item no. 1(c) states:

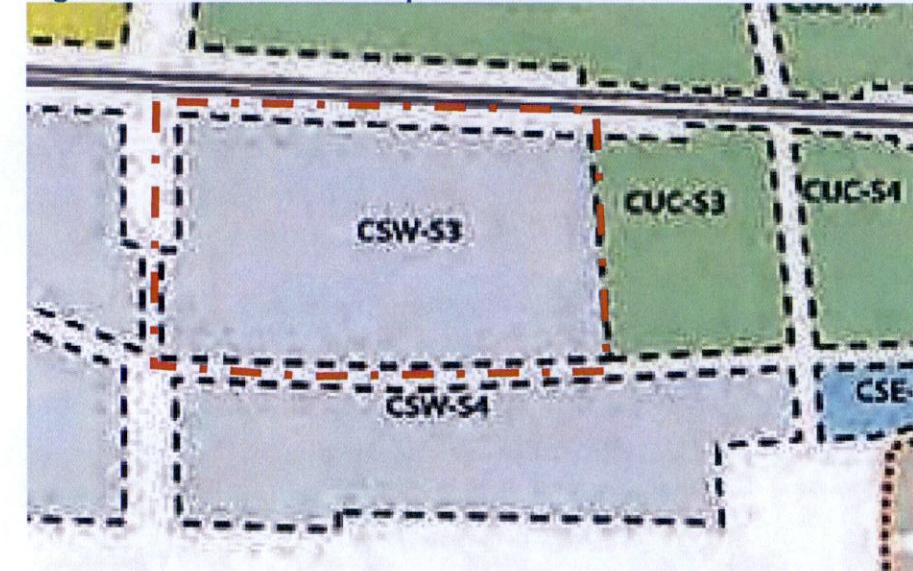
“It is unclear how the applicant has arrived at this density calculation. The applicant is requested to set out the net density for

- the current application
- the current application and previously approved scheme
- the current application, previously approved scheme and units proposed under SDZ22A/0018.”

2.6 RESPONSE TO ITEM NO. 1(C) - DENSITY CALCULATIONS

In response to the above as set out in the MCORM Further Information Response Document, the net density for the current application Tile 3 (SDZ22A/0017) is 45.5 units per hectare. The net density of the current application Tile 3 (SDZ22A/0017) and previously approved scheme CSW-S3 Tile 1 (SDZ21A/0022) and T3 is 44.36 units per hectare.

Figure 2.3 – CSW-S3 Development Area



In relation to the 3 no. applications the overall area is c. 8.33 hectares net and would provide some 445 no. units giving a density of 53.4 units per hectare which is within the density range of 51-61 units per hectare set out for Sub Sector CSW-S3 (which is also set out in Table 3.3.3 of the Planning Scheme).

Figure 2.4 – Planning Scheme Density Margins per Sub Sector

Development Area	Sub Sector	Net Density Margin per Ha.	
		MIN.	MAX.
Clonburriss South West (CSW)	S1	45	55
	S2	45	55
	S3	51	61
	S4	59	69

Source: Table 2.1.8 – Planning Scheme

3.0 ITEM NO. 2 - REQUEST FOR FURTHER INFORMATION – HOUSING QUALITY ASSESSMENT

3.1 ITEM NO. 2(A) – HOUSING QUALITY ASSESSMENT

Item no. 2(a) states:

“The Housing Quality Assessment submitted indicates that the minimum floor areas would be met. In terms of private amenity space, the schedule sets out the minimum required but does not detail the provision made for each house. The applicant is requested to include this detail on the schedule.”

3.2 RESPONSE TO ITEM 2(A) – HOUSING QUALITY ASSESSMENT

In response to this item, an itemised Housing quality assessment detailing the specific areas of all housing and apartment units has been submitted with this further information request and demonstrates compliance with amenity area standards.

3.3 ITEM NO. 2(B) – HOUSING QUALITY ASSESSMENT

Item no. 2(b) states:

“The Housing Quality Assessment fails to indicate the provision of private amenity space for each apartment / type and the applicant is requested to provide this on the schedule of accommodation. It is apparent that there are also a number of discrepancies in the table in relation to some detail, e.g.:

- Type AP_1B: 1bed 2p: agg bed area 41. 8sq.m
- Type AP_1C: 1bed 2p: agg bed area 42.4sq.m
- Type AP_2H: 2bed 4p: total bedspaces 5

The applicant is requested to provide an updates HQA which resolves any discrepancies.”

3.4 RESPONSE TO ITEM 2(B) – HOUSING QUALITY ASSESSMENT

In response to this item, the provision of private amenity space is provided in the updated schedule of accommodation, prepared by MCORM Architects and included in this Further Information response.

3.5 ITEM NO. 2(C) – HOUSING QUALITY ASSESSMENT

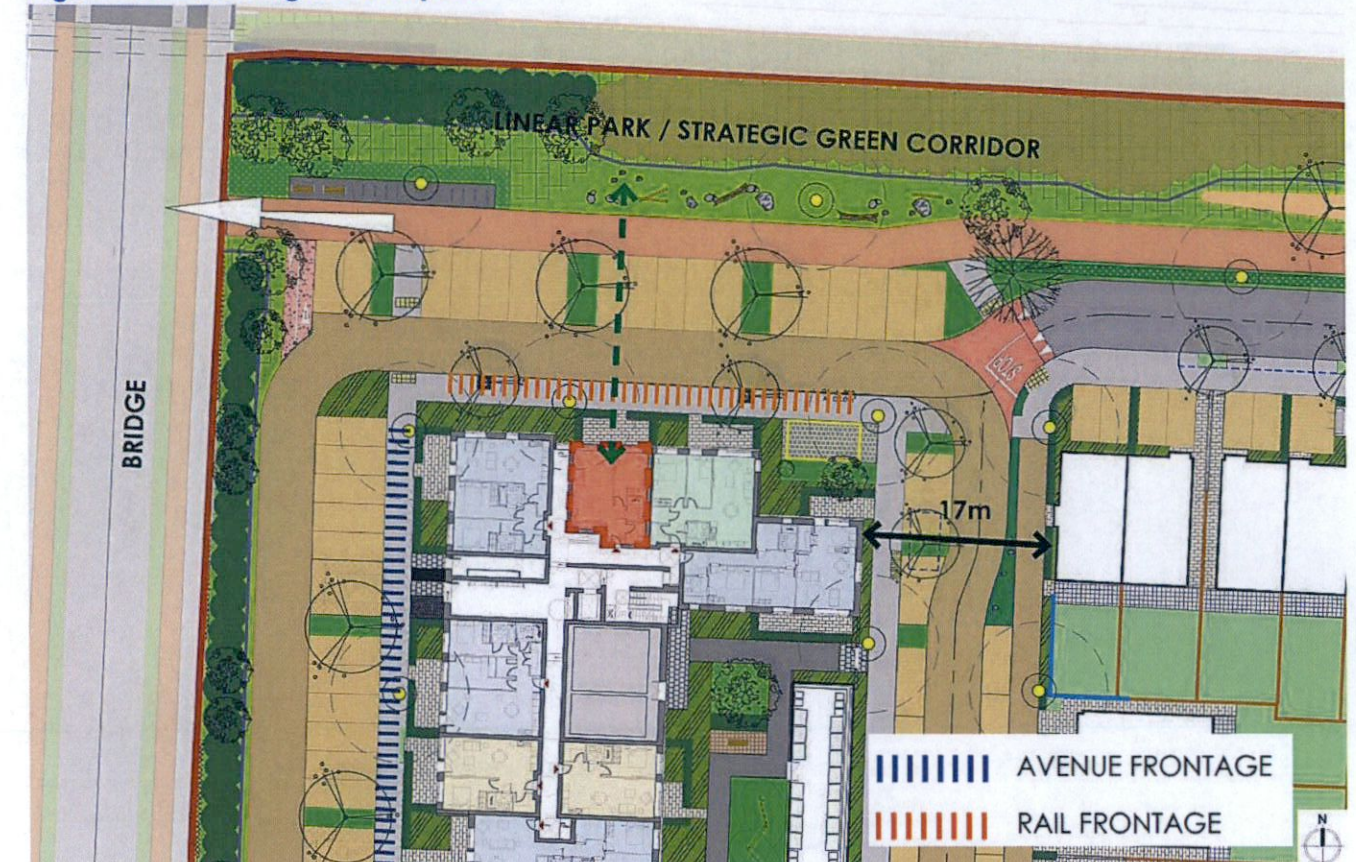
Item no. 2(c) states:

“The applicant is requested to set out how many apartments are single aspect (north). A clear justification and rationale should be provided for any single aspect north facing apartments”

3.6 RESPONSE TO ITEM 2(C) – HOUSING QUALITY ASSESSMENT

In response to this item, as set out in the MCORM Further Information Response document, 4 no. single aspect north facing units have been proposed. These units are 1 bed and have been proposed to fulfil planning scheme requirements which call for Avenue and Rail Frontage in this location.

Figure 3.1 – Frontage and Aspect



This requirement led us to propose a robust and defined building edge to the bridge at 4 storeys, which was extended around to the north elevation, closing out the development cell and narrowing the street to the east of the apartment block into compliance with “intimate local street” widths set out in the planning scheme. Additionally, the Ground floor units is an “Own door access” and enjoys a generous terrace area. The upper floors are overlooking a substantial Green Amenity space as the Local Green Corridor runs parallel to these units. We would view this as a significant visual amenity in the locality. All 4 north facing single aspect units are in compliance with BRE 2nd and 3rd Edition guidelines relating to daylight provision.

4.0 ITEM NO. 3 REQUEST FOR FURTHER INFORMATION - ROADS AND STREET LAYOUT

4.1 ITEM NO. 3 (A) – STREET NO. 18

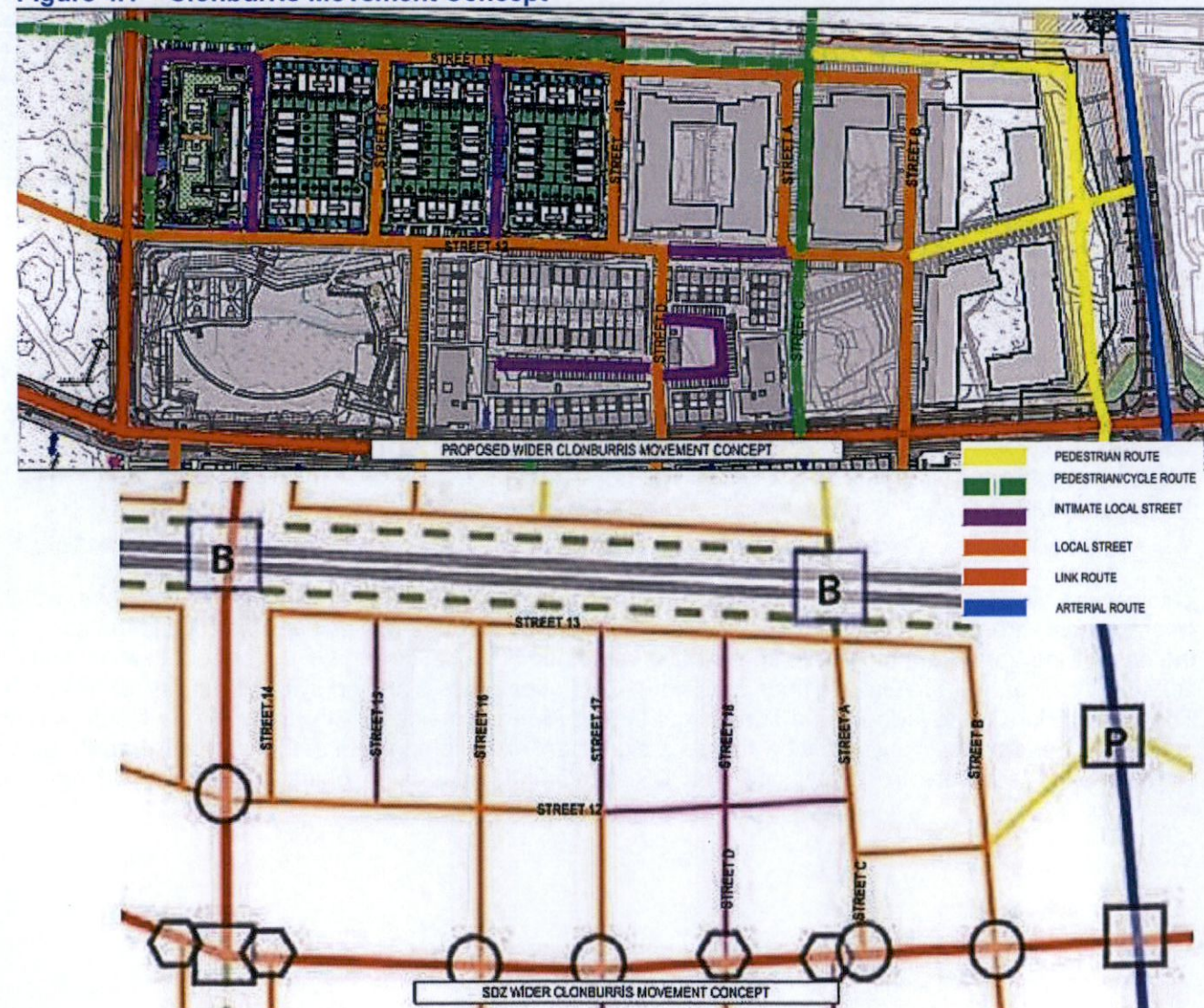
Item no. 3(a) states:

The Roads Section has raised concerns regarding the road hierarchy, stating that Street 18 should be an intimate local street, rather than a local street. It is noted that there are no fixed building frontages on this street, nor are there any green infrastructure or other designations. It is noted that the element of this street within the current application provides parallel parking and could be an intimate local street, however, there is a concurrent application under SDZ22A/0018 which provides perpendicular parking and indicates a street width greater than 20m. The applicant is requested to provide a justification for the road typology in light of the overall road hierarchy within the CSW-S3 subsector.

4.2 RESPONSE TO ITEM 3(A) – STREET NO. 18

In response to this item, we refer the Planning Authority to the DBFL response and movement concept drawing CLB-T3-95-SW-DTM-DR-DBFL-CE-1203 showing a comparison between the wider Clonburris proposed movement concept and the movement concept in the Clonburris SDZ.

Figure 4.1 – Clonburris Movement Concept



The SDZ movement diagram indicates 4 Local Streets north of Street 12 and 3 Intimate Local Streets north of Street 12, matching the number of Street types proposed for the subject development based on Street hierarchy.

The SDZ indicates Street 14 as a Local Street, though it was recommended through a further information request that Street 14 be converted to an Intimate Local Street (Homezone). Furthermore, connecting Street 14 with Street 12 would be impractical due to the close proximity to the Link Route junction to the west of Street 14.

Street 18 is proposed as an Intimate Local Street (Homezone) to maintain the 4 Local Streets north of Street 12 and to maintain the Local Street Link north from Street D (Clonburris T1) since Street C has been converted to a Cycle Link instead of a Local Street.

While recognising the desire to match the exact street hierarchy as shown in the SDZ movement diagram, adjacent development phases, natural topography and tie-in locations to existing infrastructure has informed the proposed movement diagram which satisfies the overall objectives of the SDZ movement concept.

4.3 ITEM NO. 3 (B) – ROAD SAFETY AUDIT

Item no. 3(ii) states:

“The applicant is requested to submit a complete report stating how All Road Safety Audit items were addressed in the submitted application.”

4.4 RESPONSE TO ITEM 3(B) – ROAD SAFETY AUDIT

In response to this item, we refer the Planning Authority to the DBFL Response, and document “Road Safety Audit Responses” which details how each item of the Road Safety Audit has been resolved. The Roads Layout drawing no. CLB-T3-95-SW-DTM-DR-DBFL-CE-1201 has also been revised, showing all Road Safety Audit Items resolved as discussed in the report.

4.5 ITEM NO. 3 (C) – BIN STORAGE LOCATIONS

Item no. 3(c) states:

“The applicant is requested to provide details of bin storage locations and also refuse service arrangements.”

4.6 RESPONSE TO ITEM 3(C) – BIN STORAGE LOCATIONS

In response to this item as set out in the DBFL response, the revised Roads Layout as attached indicates the proposed bin storage locations and AutoTrack analysis of relevant refuse collection vehicles. Refer to drawing CLB-T3-95-SW-DTM-DR-DBFL-CE-1202 for further the location of bin stores and the tracking of relevant refuse vehicles.

4.7 ITEM NO. 3 (D) – AUTOTRACK

Item no. 3(d) states:

*“The applicant is requested to submit a detailed AutoTRAK analysis for the following movements:
I. Emergency vehicle access to all locations within the site (large firefighting tender).
II. Refuse vehicles accessing all bin storage locations.”*

4.8 RESPONSE TO ITEM 3(D) – AUTOTRACK

4.8.1 Response to item no. 3 (d)(i)

As set out in the DBFL response an extensive vehicle tracking analysis has been conducted to ensure an emergency vehicle is able to reach every part of the development. Refer to drawing no. CLB-T3-95-SW-DTM-DR-DBFL-CE-1202.

4.8.2 Response to item no. 3 (d)(ii)

In response to this item, an extensive vehicle tracking analysis has been conducted to ensure a refuse vehicle is able to reach every bin staging area within the development. Refer to drawing no. CLB-T3-95-SW-DTM-DR-DBFL-CE-1202.

4.9 ITEM NO. 3 (E) – BICYCLE PARKING

Item no. 3(e) states:

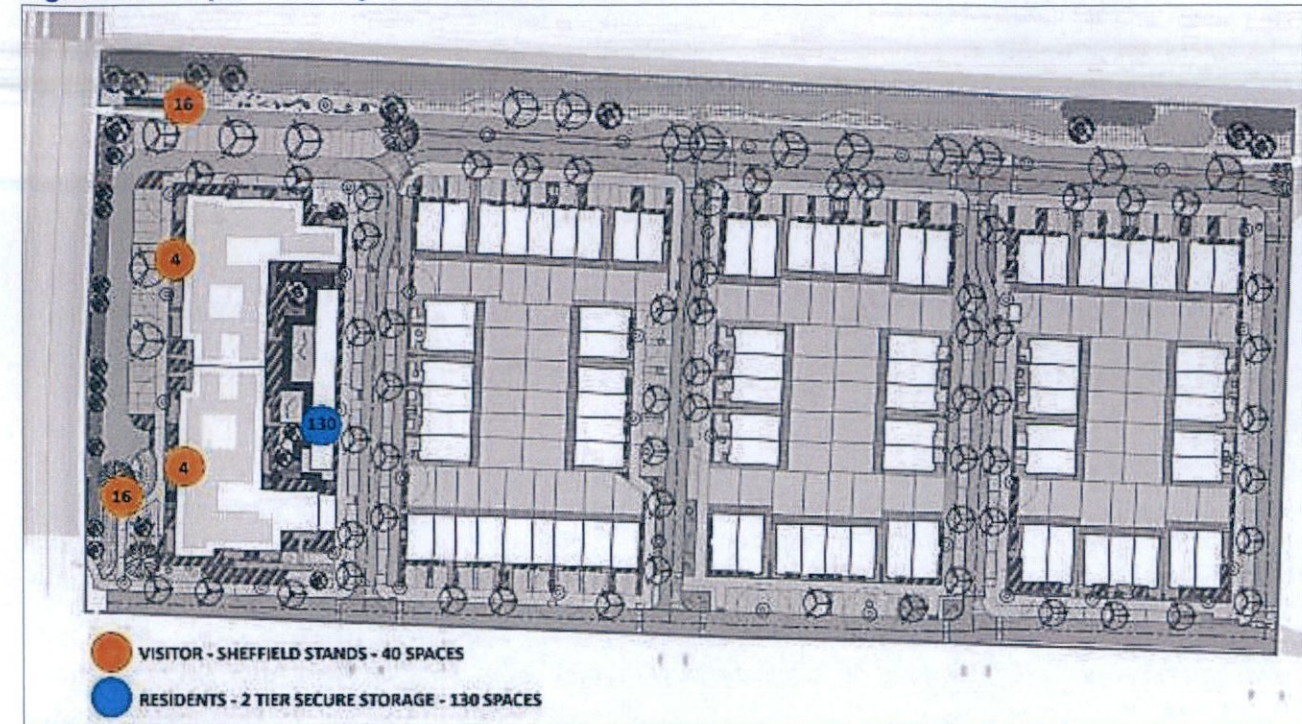
- "The applicant is requested to provide additional information which:*
- I. details all (any additional) bicycle parking locations within the site.*
- II. demonstrates the proposed number of spaces meets the minimum CDP 2022-28 criteria.*
- III. demonstrates there is significant capacity within the proposed bicycle parking store to cater for the proposed number of 170.*
- IV. acknowledges the proposal is in line with the National Cycle manual (2011).*
- V. gives additional detail re the bicycle stand specification (i.e., Sheffield stands) proposed."*

4.10 RESPONSE TO ITEM 3(E) – BICYCLE PARKING

4.10.1 Response to item 3(e) I

Additional visitor bike stands have been added adjacent each apartment block entrance and along the cycle lanes to achieve a total no. of 20 stand for 40 visitor bikes. See drawing 22234-1-100 (LANDSCAPE Masterplan), prepared by CSR which shows the locations. The extract below also shows the long term and visitor parking in the vicinity of the apartment building.

Figure 4.2 – Apartment Cycle Provision



Source: MCORM Architects

4.10.2 Response to item 3(e) II

It is noted that Table 12.23 sets out minimum requirements for apartments comprising 1 per bedroom (long term and 1 cycle space per 2 apartments (short stay). Block 1 comprises 76 no. apartments which equates to a requirement of 126 no. long stay cycle spaces and 38 short stay spaces (164 in total).

In this regard, 170 cycle spaces are proposed with 130 no. in a bicycle store located adjacent to Block 1. A 2 tier secure storage system is proposed to facilitate 130 spaces. Example specification for the 2 tier bike system and additional details on bicycle storage, please refer to drawing no. PL010 Rev A, prepared by MCORM which has been submitted as part of this FI request,

4.10.3 Response to item 3(e) III

The bicycle store includes 130 no. long term cycle spaces which is in excess of the requirements of Table 12.23 of the South Dublin County Development Plan (124 no. spaces required).

4.10.4 Response to item 3(e) IV

As confirmed in the DBFL Response, "All cycle related infrastructure included in the proposals for the subject development conform to the National Cycle Manual (2011)."

4.10.5 Response to item 3(e) V

In response to this item the 170 spaces proposed as part of this application are provided in a mix of typologies and locations. For the apartment residents, the secure storage is located in a dedicated facility in the courtyard of the apartment. The courtyard is secure, can be accessed directly from the street and has direct access points to both circulation cores. This will facilitate the convenient, secure storage area which is easily accessed.

A 2 tier secure storage system is proposed to facilitate 130 spaces. Example specification for the 2 tier bike system and additional details on bicycle storage, please refer to drawing no. PL010 Rev A, prepared by MCORM which has been submitted as part of this FI request. Sheffield stands provide a further 40 spaces for the use of visitors and are integrated in the landscaping proposals. Locations and specifications of the bicycle stands are as set out above.

All visitor bicycle stands in public open space are proposed to be Sheffield stands, for their flexibility and robustness. See drawing no. 22234-2-903 (Bicycle Stand Detail), prepared by CSR Landscape Architects.

5.0 ITEM NO. 4 REQUEST FOR FURTHER INFORMATION – STREET LAYOUT

5.1 ITEM NO. 4(A) – STREET LAYOUT & FOOTPATH WIDTHS

Item no. 4(a) states:

“The applicant is requested to provide the following information / amendments in relation to the streets:

- Street 12: There are concerns regarding the design of the area to the south of the apartment block (Block 1), which does not provide parking nor does it provide a min. 2.5m wide footpath (only 2m is provided at this location).

- Street 13: There are concerns regarding the design of the area to the north of the apartment block (Block 1), which does not provide parking nor does it provide a min. 2.5m wide footpath (only 2m is provided at this location).

- Street 14: Footpath is only 2m wide and should be increased to 2.5m. An area of open space is identified at this location within the Scheme and it is also identified as a strategic green corridor. It is noted that there are significant levels of parking along this street, which are necessary for the apartments. In light of the GI designations in this area, the applicant is requested to provide for a Homezone along Street 14, along the north of the apartments to meet Street 15. There should be a significant increase in the level of planning and GI in this area also.

- Street 15: Appears that carriageway may be 5m. Applicant requested to indicate a width of no greater than 4.8m.

- Street 16: Footpath reduces to 2m in part. 2.5m required.

- Street 17: Noted frontage 21.05m – this should be reduced to 20m. Footpath is 2m, should be 2.5m. Appears that carriageway may be 5m. Applicant requested to indicate a width of no greater than 4.8m.

- Street 18: Generally meets indicative local street A (when considered with information provided on adjacent application). Scheme requires intimate local street at this location and the applicant is requested to provide a rationale for their approach.

Where perpendicular parking is proposed, the applicant is requested to indicate that there is 6m reversing space. The applicant is requested to provide a plan setting out the dimensions of all street widths, in compliance with the scheme, and all foot paths, carriageways and parking bays.”

5.2 RESPONSE TO ITEM 4(A) – STREET LAYOUT & FOOTPATH WIDTHS

In response to this item, all streets have been revised to incorporate the feedback provided by SDCC relating to street and footpath widths. A site layout (CLB-T3-95-SW-DTM-DR-DBFL-CE-1201) prepared by DBFL Consulting Engineers, demonstrates compliance with the parameters set out in the planning scheme has been submitted as part of this further information request.

The following amendments to the layout have been made:

- Updated cycle track alignment connecting Clonburris T3 to the Link Street
- Updated bicycle stand/storage locations as indicated on the Roads Layout
- Updated footpath widths across the entire site to 2.5m minimum
- Street 14 changed to 4.8m wide Homezone instead of a Local Street

- General inclusion of tactile paving at all pedestrian crossing points and desire lines
- Inclusion of regular pedestrian footpath connections from road carriageway to footpaths behind parking spaces for house bin store access
- Rearranged house parking arrangement near all road junctions to avoid clashes with sight lines

Additional planting and Green Infrastructure has been incorporated through the narrowing of the carriageway along Street 14 and moving the apartment block further east. This, along with a reduction in the number of parking spaces on street 14 has allowed for a continuous landscaped corridor from the linear park to the local park.

5.2.1 Street 12

All footpaths along Street 12 have been revised to be minimum 2.5m in width. There is a significant change in level along Street 12 south of Apartment Block 1, which would make parking impractical due to the necessary embankment from the street level down to the ground floor of the building.

5.2.2 Street 13

All footpaths along Street 13 have been revised to be minimum 2.5m in width. There is a significant change in level along Street 12 south of Apartment Block 1, which would make parking impractical due to the necessary embankment from the Street down to the ground floor of the building.

5.2.3 Street 14

Footpaths along Street 14 have been updated to 2.5m in width. In addition, the street has been altered to accommodate a homezone. Additional planning has been included to tie in with the wider north south Strategic Green Corridor along the bridge and road to the west.

5.2.4 Street 15

The Street 15 road carriageway has been updated to 4.8m in width.

5.2.5 Street 16

Footpaths along Street 16 have been updated to 2.5m in width.

5.2.6 Street 17

Footpaths along Street 17 have been updated to 2.5m in width and the road carriageway to a width of 4.8m.

5.2.7 Street 18

Please refer to response to item 3(a).

5.2.8 Perpendicular parking

As indicated and dimensioned on drawing CLB-T3-95-SW-DTM-DR-DBFL-CE-1201, prepared by DBFL Consulting Engineers, all perpendicular parking spaces have been provided with a minimum of 6m reversing space.

5.3 ITEM NO. 4(B) – PARKING PLAN

Item no. 4(B) states:

“Drawing PL008 sets out the following:

- Apartment parking – 50 spaces – 30%
- House parking – on curtilage – 60 spaces – 35%
- House parking – off curtilage 60 spaces – 35%

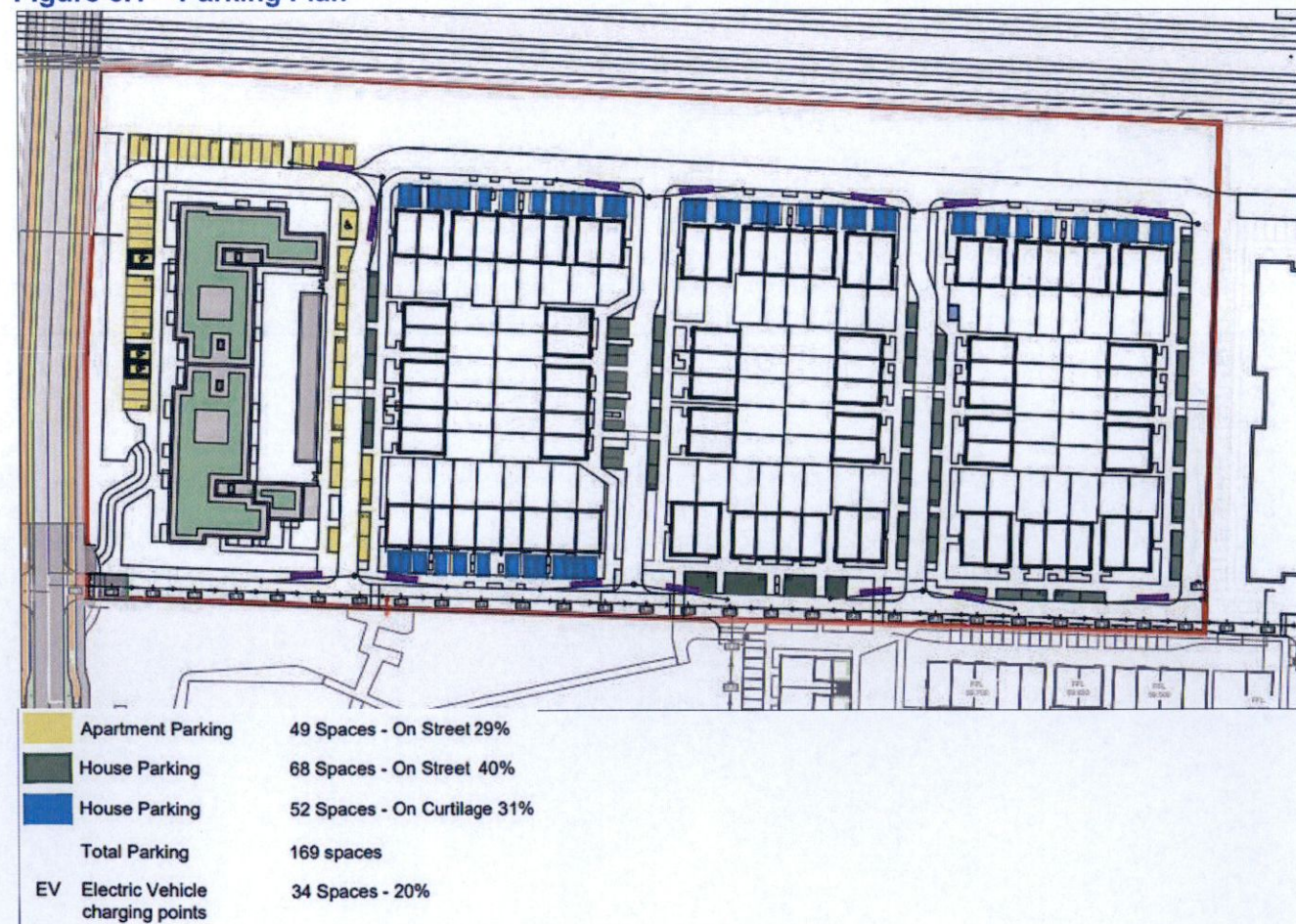
The design statement states that 38% of car parking is on curtilage. The applicant is requested to provide a plan indicating which spaces are on curtilage and which are off curtilage”

5.4 RESPONSE TO ITEM 4(B) – PARKING PLAN

In response to this item, A revised parking plan which incorporates changes to the site layout and identifies on curtilage and on street parking has been submitted as part of the Further Information drawing pack. For further details, please refer to PL008 Parking Strategy Rev A. Parking classification is as follows:

- House parking - on Curtilage - 52 spaces - 31%
- House parking - on Street - 68 spaces - 40%
- Apartment Parking - on Street- 49 spaces - 29%
- Total parking 169 spaces

Figure 5.1 – Parking Plan



Source: MCORM Architects

6.0 ITEM NO. 5 REQUEST FOR FURTHER INFORMATION - INTERFACE WITH FUTURE ROAD

6.1 ITEM NO. 5 (A) - INTERFACE WITH FUTURE ROAD

Item no. 5 (A) states:

“There is concern regarding the interface between the proposed apartment block and the roadway / bridge to the west. Further consideration of this matter is required. The applicant is requested to provide further cross sections to include the bridge and also provide more detail regarding the impact of the bridge on the amenity area to the west of the apartment block. The applicant is requested to provide details of how any retaining features may be incorporated into the adjacent open space.”

6.2 RESPONSE TO ITEM 5 (A) - INTERFACE WITH FUTURE ROAD

As set out in the MCORM FIR Document, the planning scheme identifies this area of the SDZ lands for a vehicular bridge to cross over the existing rail line. It further identifies this as a location for avenue frontage as it recognises that a robust building line must be in place here to address the major piece of connecting infrastructure which will be constructed immediately to the west.

The revised proposal recognises that distance from the bridge, orientation and soft landscaping will have a positive impact on the amenity of future residents of the apartment block. The revisions to the site layout has increased the distance between the bridge and the apartment block and reduced or rearranged parking proposals allowing for increased, uninterrupted Green Infrastructure to be proposed along the bridge. While the bridge design itself will be the subject of a future planning application, the continuous strip of land which runs along the eastern side of the bridge crossing point has been set aside to incorporate any landscape or retaining features which may arise from the design of the bridge. While the amenity area to the west of the bridge is not the subject of this application, a similar approach will be taken there, with a larger area being retained for the amenity area identified in the planning scheme at that location. For further information relating to Bridge interface and separation distances, please refer to PL013 - Bridge and Apartment Block Sections.

Figure 6.1 – CGI Block 1



Figure 6.2 – Architectural Section Block 1 and Future Bridge

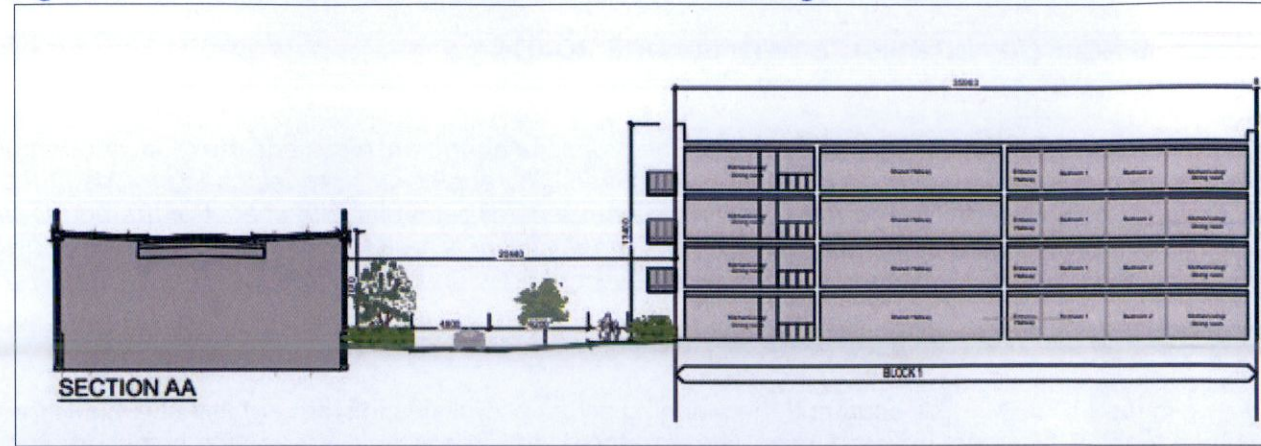
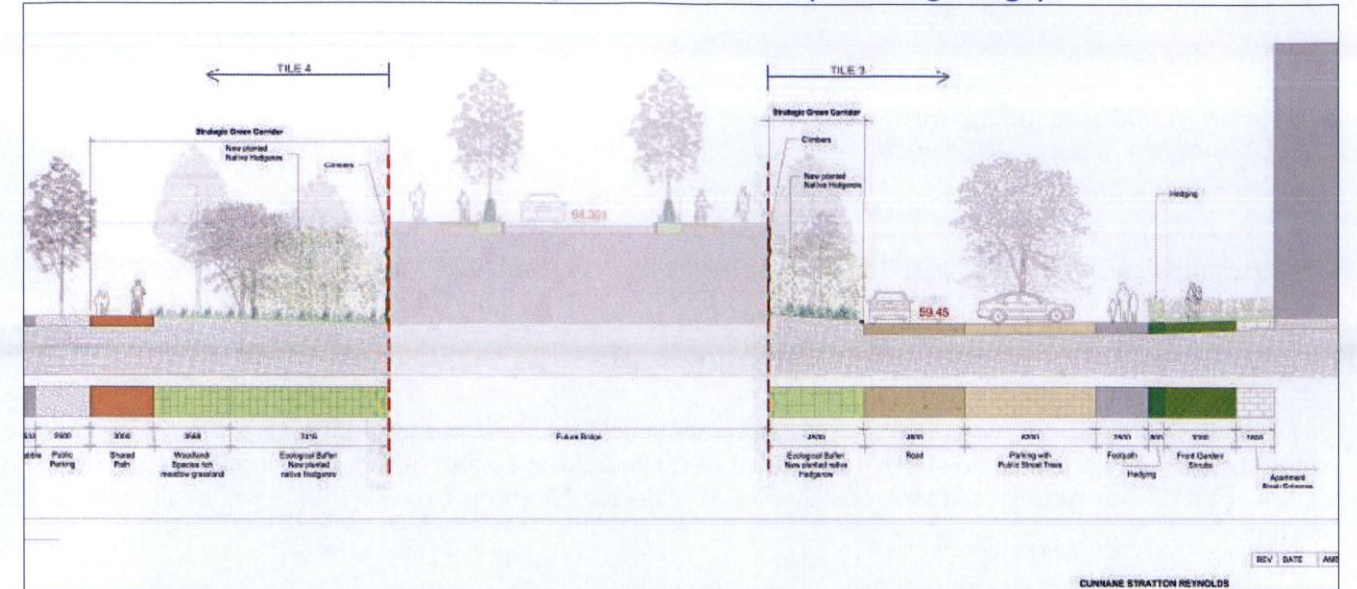


Figure 6.3 – CGI from under future bridge looking towards Block 1



As set out in the CSR Response Document, a native hedgerow is proposed along the bridge, with climbers along the retaining wall of the bridge, to create a densely planted green corridor, in line with the requirements of the strategic green corridor. The turning head has been modified to achieve an enhanced, continuous green corridor along the bridge. Eight additional trees (*Quercus petraea* 'Columna') are proposed in the hedge for further vertical screening and enhancement of the green infrastructure. All ground floor apartments of Block 01 will be protected by 1.1m high clipped hedge to provide separation and an element of privacy from the public footpath. Cross-section drawings have been prepared to illustrate these design amendments.

Figure 6.4 – Section through the Strategic Green Corridor (including bridge)



6.3 ITEM NO. 5 (B) – INTERFACE WITH FUTURE ROAD – SUNLIGHT/DAYLIGHT

Item no. 5(b) states:

“The applicant is requested to set out how the proposed bridge has been taken into consideration in the sunlight and daylight assessment. A ‘worst case’ scenario should be utilised to ensure the impacts on future development are fully assessed.

6.4 RESPONSE TO ITEM 5(B) – INTERFACE WITH FUTURE ROAD – SUNLIGHT/DAYLIGHT

In response to this item, we refer the Planning Authority to the IES Further Information Response document which outlines that the original analysis submitted with the planning application did allow for a representative massing of the bridge. Notwithstanding this, the sunlight/daylight analysis of current design of the bridge has been undertaken to ascertain potential impacts the bridge may have with regard to daylight and sunlight on Block 1.

The report notes that:

“When comparing the initial massing modelled against the updated bridge design, the sunlight results highlight again that the compliance rate is the same for both scenarios and the actual results themselves are very similar if not the same. It can be concluded that the original report had accounted for the bridge massing sufficiently and there would be no material change to sunlight performance of the adjacent proposed Clonburris T3 apartments as a result of the updated bridge design.”

6.5 ITEM NO. 5 (C) – FINE URBAN GRAIN

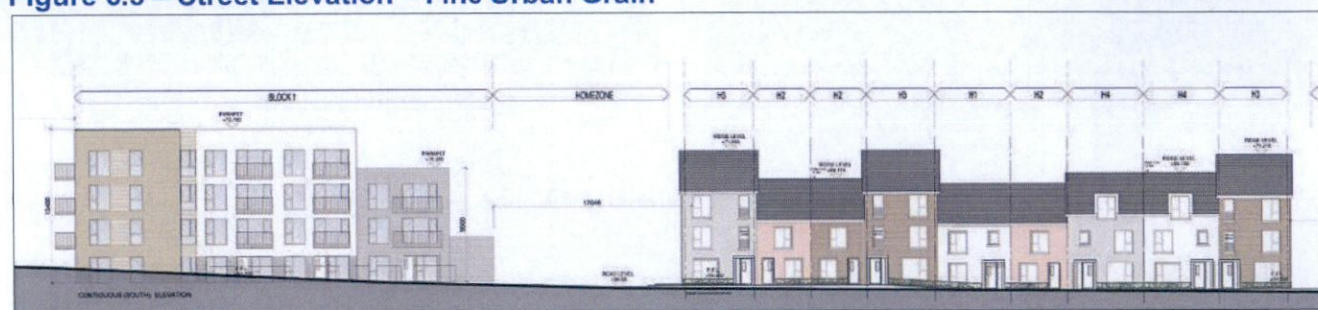
Item no. 5 (c) states:

“The proposals fall short of the scheme's requirement for a fine urban grain in identified locations and it is considered that this requirement provides an opportunity for greater diversity and architectural interest. The applicant is requested to develop further design alternatives and make revisions to meet this provision of the scheme. It should be noted that the Building Height concept envisaged a 3-4 storey height fronting the park.”

6.6 RESPONSE TO ITEM NO. 5 (C) – URBAN GRAIN

In response to this item, as detailed in the MCORM Response document, the design team has undertaken a re-assessment of the urban grain response and has liaised with SDCC to generate an approach which fulfils the aesthetic objective of urban grain and provides a workable and buildable solution for the applicant.

Figure 6.5 – Street Elevation – Fine Urban Grain



The proposal involves 4 house type floor plans which can fit together in a variety of compositions. These floor plans range from 2 bed to 4 bed, are 2-3 storeys and vary in plot width. They can also be adapted to feature alternative roof profiles to generate further visual distinction, should the setting call for it.

Each plot will be vertically articulated using different materials and colours and the internal layouts of the 2 storey units allow for expansion into the roof spaces, should the end user desire additional space. This will lead to further diversification of the units, allowing for independent development of the individual plots as the scheme matures.

These proposals have been presented to and discussed with representatives of SDCC who have agreed that this is an appropriate response to the fine urban grain objective and aesthetic and this approach and its permutations can be applied to future phases.

Figure 6.6 – Fine Urban Grain CGI



We respectfully submit that revisions to the Urban Grain terrace of houses, in juxtaposition with the southern facade of the apartment building, which itself varies in heights and is vertically articulated through changes in the materials.

6.7 ITEM NO. 5 (D) - BOUNDARY TREATMENT

Item no. 5 (d) states:

“The applicant is requested to provide details of boundary treatment.”

6.8 RESPONSE TO ITEM NO. 5(D) - BOUNDARY TREATMENT

In response to this item, we refer the Planning Authority to drawing no. 22234-1-100 LANDSCAPE Masterplan and 22234-1-110 T3 Boundary Treatment Plan and section 22234-1-204 T3 Sections 6 of 6, prepared by CSR.

6.9 ITEM NO. 5(E) – SOLAR PANELS

Item no. 5(E) states:

“The applicant is requested to provide details of any solar panels proposed.”

6.10 RESPONSE TO ITEM NO. 5(E) – SOLAR PANELS

We refer the Planning Authority to PL202 - BLOCK 1 ROOF FLOOR PLAN, submitted with the application which includes areas for PV panels. In relation to the house types, solar locations for solar panels are

shown on the rear elevations. It is noted drawing no. PL105 house types J3 - J3s and drawing nos. PL106 PL107 do not show solar panels.

The applicant would be happy to show locations of solar panels as part of a condition/compliance.

6.11 ITEM NO. 5(F) – RENDER

Item no. 5(v) states:

“The proposed quantum of render on the apartment block is considered to be excessive and would be likely to visually deteriorate over time. The applicant is requested to omit these elements.”

6.12 RESPONSE TO ITEM NO. 5(F) – EXTENT OF RENDER

In response to this item, we refer the Planning Authority to the MCORM Response document which outlines that the extent of render on the main façades of Apartment Block 1 have been significantly reduced on foot of this FI request. While some areas of render remain, they have been retained for specific reasons. Along the western elevation, the stepped back central section is proposed in grey render as it breaks down the linear mass of the building. Along the southern elevation, the variety of materials enhance the urban grain aesthetic, emphasising the variety of tenures evident in this area of the site. In the courtyard, white render has been retained on the upper floors as the reflectance of white render is greater than brick and contributes positively to the daylighting and residential amenity of the apartment units. Please refer to Drawing PL203 Rev A for further details.

Figure 6.7 – Elevations as submitted with planning application



Figure 6.8 – Revised Elevations with additional brick



7.0 ITEM NO. 6 REQUEST FOR FURTHER INFORMATION – ATTENUATION DETAIL

Item no. 6 states:

“It is unclear how surface water attenuation is within requirements of predesigned attenuation limits for Clonburris. The applicant is requested to:

7.1 ITEM NO. 6 (A) S ATTENUATION DETAIL

Item no. 6(a) states:

“Submit a report and drawing to show what attenuation was agreed for the site proposed and what attenuation is provided for the site proposed”

7.2 RESPONSE TO ITEM 6(A) - ATTENUATION DETAIL

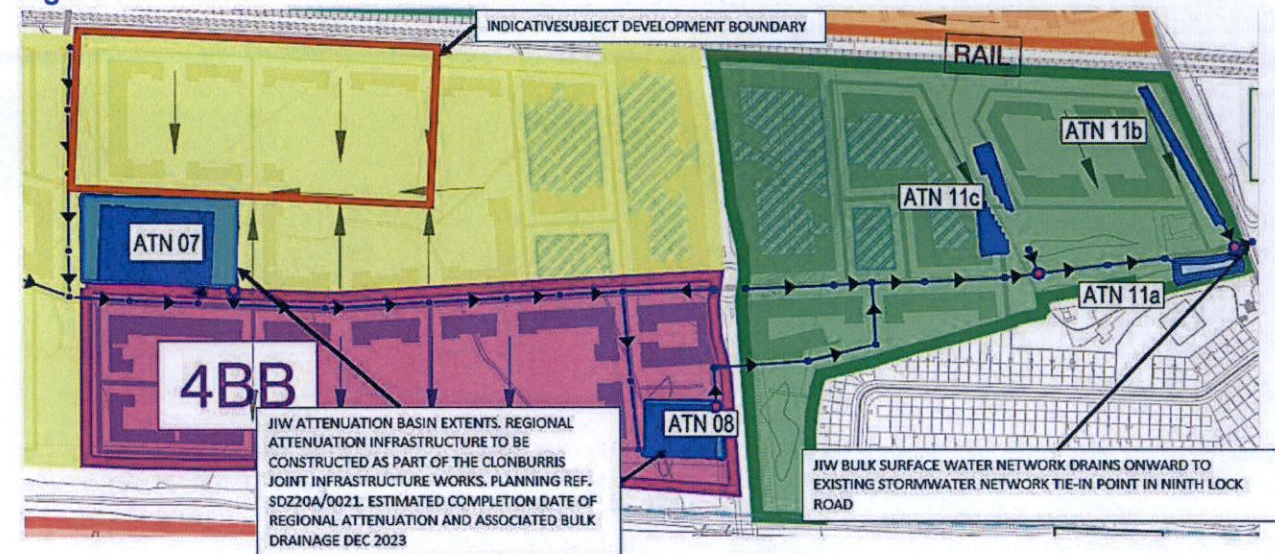
In response we refer the Planning Authority to the DBFL FIR response which outlines that The Infrastructure Design Report and drawings submitted for planning provides this information. The drainage proposals for the site meet and exceed the requirements of the agreed Surface Water Management Plan for Clonburris.

A comparison between the discharge allowed for from the proposed development to be attenuated within the separately permitted regional attenuation basin ATN 07 and the actual discharge is provided in section 3.4.1 of the Infrastructure Design Report.

This shows that 1.87ha of impermeable area contribution from this sub-catchment (4B) has been assumed for regional attenuation sizing and only 1.815ha is proposed from the subject development, which means the development is well within the designed limits.

The figure below shows the overall drainage strategy for the wider Clonburris, which matches the downstream surface water drainage infrastructure provided with the adjacent Clonburris T1 development and the joint infrastructure bulk drainage under construction.

Figure 7.1 – Surface Water Catchments



7.3 ITEM NO. 6(B) – ATTENUATION DETAIL – CALCULATIONS

Item no. 6(ii) states:

“Submit a report to show a comparison of calculations use for proposed site with calculations used in a pre design stage of attenuation for Clonburris Site.”

7.4 RESPONSE TO ITEM 6(B) – ATTENUATION DETAIL – CALCULATIONS

We refer the Planning Authority to the DBFL Further Information response document which outlines that the detail is provided within information submitted for planning. (Refer to item 6 (a) response). Further, refer to drawing CLB-T3-94-SW-DTM-DR-DBFL-CE-1312 which shows the downstream drainage infrastructure within the larger Clonburris SDZ.

7.5 ITEM NO. 6(C) - ATTENUATION DETAIL – SURFACE WATER DRAWINGS

Item no. 6(c) states:

“Submit up to date surface water drawings. Show how surface water layout drawings compare to drawings at pre designed stage of Clonburris Site.”

7.6 RESPONSE TO ITEM 6(C) - ATTENUATION DETAIL – SURFACE WATER DRAWINGS

As set out in the DBFL Further Information response, this detail was provided within information submitted for planning. (Please also refer to item 6 (a) response above).

7.7 ITEM NO. 6(D) -ATTENUATION DETAIL – DISCHARGE RATES

Item no. 6(d) states:

“Submit a report and drawing showing surface water drawings of attenuation and layout both at pe design stage and for proposed site. Outline the discharge rates proposed now and compare to pre design discharge rates for Clonburris.”

7.8 RESPONSE TO ITEM 6(D) - ATTENUATION DETAIL – DISCHARGE RATES

As set out in the DBFL Further Information response, this detail was provided within information submitted for planning. (Please also refer to item 6 (a) response above).

7.9 ITEM NO. 6(E) - ATTENUATION DETAIL – SUDS

Item no. 6(v) states:

“Include SuDS (Sustainable Drainage Systems) in proposed development such as a Green Roof, Swales, permeable paving and other such SuDS. Examples of SuDS can be found in the SDCC SuDS Guide at: [sdcc-sustainable-drainage-explanatory-design-and-evaluation-guide.pdf](#)”

7.10 RESPONSE TO ITEM 6(V) - ATTENUATION DETAIL – SUDS

Green roofs, swales, SuDS tree pits and bio-retention areas/rain gardens are provided as part of a suite of proposed SuDS measures shown on drawings CLB-T3-94-SW-DTM-DR-DBFL-CE-1321 and CLB-T3-94-SWDTM-DR-DBFL-CE-1311. The functioning of these SuDS elements are also described in section 3.3 of the Infrastructure Design Report submitted with the Planning Application.

7.11 ITEM NO. 6(F) - ATTENUATION DETAIL – CONSULTATION

Item no. 6(f) states:

“Contact water services in SDCC to discuss above issues before resubmitting additional information required”

7.12 RESPONSE TO ITEM 6(F) - ATTENUATION DETAIL – CONSULTATION

A meeting with Brian Harkin was held on 3 March 2023 and all drainage related FI items have been discussed and agreed as described in this response document.

8.0 ITEM NO. 7 REQUEST FOR FURTHER INFORMATION – STRATEGIC GREEN CORRIDOR/GREEN INFRASTRUCTURE

8.1 ITEM NO. 7 (A) – STRATEGIC GREEN CORRIDOR

Item no. 7 (a) states:

“The applicant is requested to provide a redesigned proposals that demonstrate:
i. A Strategic Green Corridor along the northern boundary of the site adjacent to the Dublin-Cork Railway. Tree protection measures should include full extent of scrub thicket.
ii. A Strategic Green Corridor approximately N-S along the western boundary incorporating and strengthening the existing hedgerow within the Strategic Open Space.”

8.2 RESPONSE TO ITEM 7(A) (I)

As set out in the CSR response, the revised design of the ecological corridor along the railway retains and integrates the full extent of the existing scrub thicket. A chestnut paling fence is proposed to protect the area from misuse during the landscape establishment period. Additional native hedge planting is also proposed to enhance the existing vegetation mix and create a continuous vegetated corridor and to enhance its ecological value. Hedgerow protection is provided along the scrub thicket during the construction phase. For the layout of tree and hedgerow protection fences see drawing 22234-1-110 T3 Boundary Treatment Plan and arborist drawing Clonburris T3 Tree Protection Plan See drawing 22234-1-106 (LANDSCAPE Planting Strategy Plan) and 22234-1-107 (Green Infrastructure Strategy).

Figure 8.1 –Strategic Green Corridors



8.3 RESPONSE TO ITEM 7(A) (II)

Careful consideration has been given to maintain and protecting existing hedgerows. Whilst maintaining and protecting Hedge No 8 would be a desirable outcome, it is unfortunately not a feasible proposition given that the Planning Scheme requires the inclusion of a future bridge and road/pedestrian/cyclist connection which would impact on a significant portion of Hedge no. 8.

The requirements of the construction of the future bridge (and its required width which is dictated by relevant design standards in respect of width etc.), and in conjunction with the requirement to provide access to the block along the eastern frontage, means that the retention of the hedge is not feasible. It is respectfully submitted that the constraints of the bridge and its future construction were not fully appreciated in the concept for the North South Strategic Green Corridor.

Notwithstanding these localised constraints the remainder of the North South Strategic Green Corridor will be achieved through the permitted Tile 1 (SDZ21A/0022) application (which includes Cappagh Park) and the future Tile 4 (CSW-S1) and Tile 5 (CSW-S2) application which will include the remainder of the Strategic Green Corridor along the western frontage of the future bridge (c. 0.22 hectares) and the southern portion of the Strategic Green Corridor (c. 0.56 hectares) which will link into the East West Grand Canal Strategic Green Corridor.

Furthermore, as outlined in the CSR response, maintaining and protecting fragments would be considered futile and efforts would be better spent replanting a new coherent length of hedgerow with a good mix of native species. It is proposed to plant a new c.90m long mixed species native hedgerow on the verge along the bridge, with additional native climber planting along the retaining walls and 10 new proposed native trees. In addition, the turning head of street no.14 has been inverted to provide improved green infrastructure connectivity along the bridge corridor. These enhancements when considered together, offers considerable green infrastructure enhancement to the Strategic Green Corridor as outlined in the SDZ. See drawing 22234-1-107 (Green Infrastructure Strategy) and section A on drawing 22234-1-200 (T3 Sections 6 of 6).

Overall, it is considered that the design approach includes an appropriate response to the Planning Scheme and also to the constraints of including the future bridge design in the layout.

8.4 ITEM NO. 7 (B) – GREEN INFRASTRUCTURE & SUDS

Item no. 7 (b) states:

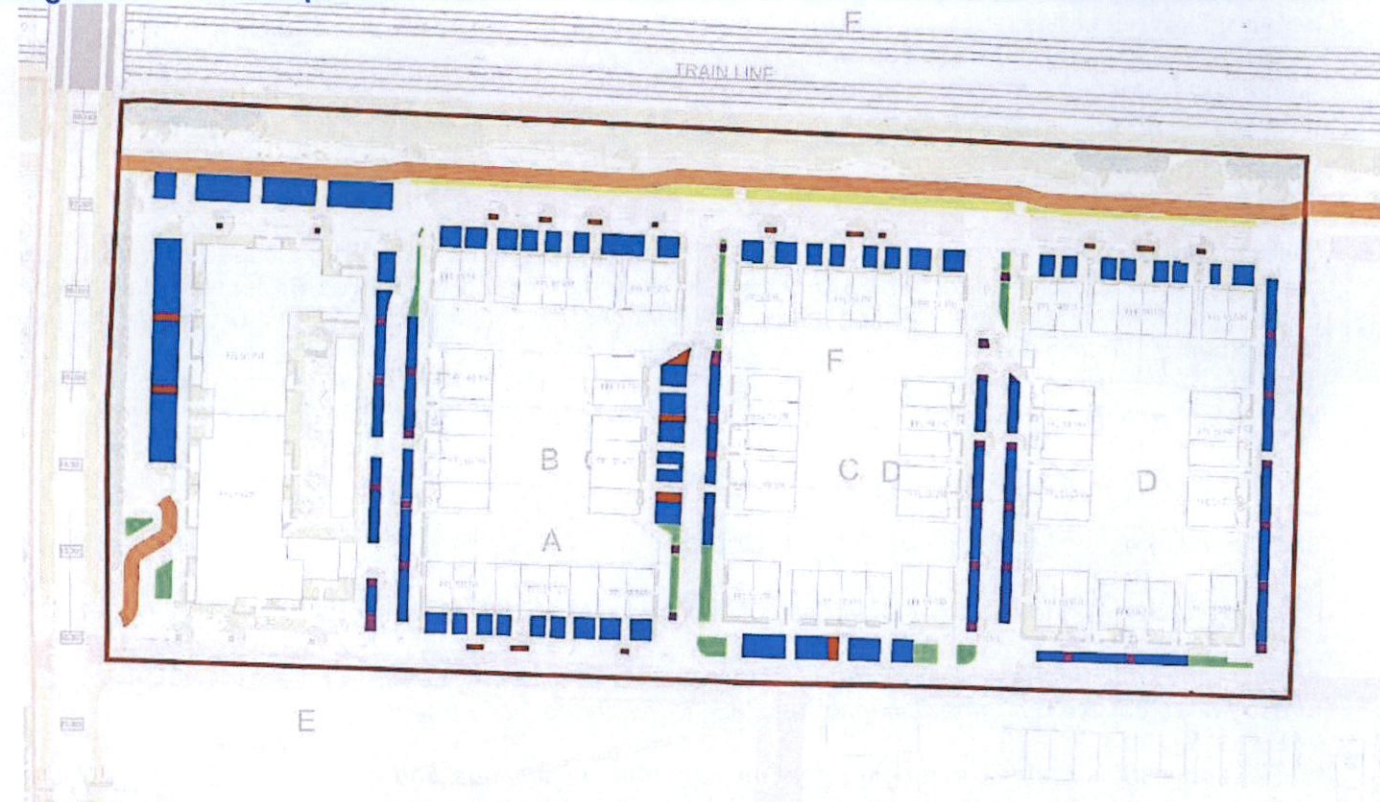
“The Clonburris SDZ strategy requires a green infrastructure-based approach to drainage and stormwater management. SuDS are to be designed as an ecological resource designed into the street, public squares, and open space network. These shall be of a high quality, designed as a series of 'wet' and 'dry' landscape elements to achieve a multifunctional space for amenity, biodiversity, and surface water management.

The applicant is advised SUDs should be designed in accordance with SDCC Sustainable Drainage Explanatory Design and Evaluation Guide 2022. Further use of swales and natural SUDs requested. Additional details are requested including details of filter strips. The applicant is requested to provide road edge detail showing how water accesses the swales/tree pits and bioretention areas. Further information required on Tree pits is requested. The applicant is requested to demonstrate amenity and biodiversity value of Suds measures. Contact Public Realm Section to agree details prior to resubmitting.”

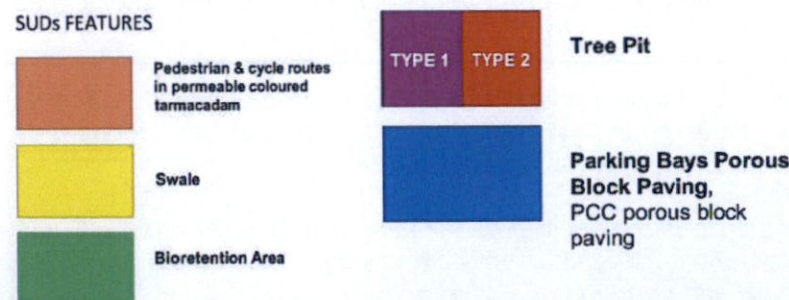
8.5 RESPONSE TO ITEM NO. 7 (B) – GREEN INFRASTRUCTURE & SUDS

In response to this item, it is noted that extensive permeable paving, swales, bio-retention areas/rain gardens and tree pits have been proposed as SuDS features, providing an extensive treatment train.

Figure 8.2 – Landscape SuDS Plan



CSR Drawing 22234-1-108



As set out in the CSR response, wherever possible, additional SuDS measures have been incorporated throughout the site and these are shown on both the drainage layouts and landscape layouts. Proposed enhancements include two additional attenuation areas at the south-west corner of the apartment block and pedestrian / cycle routes along the railway and the bridge are now proposed to be permeable asphalt. SuDS elements form an integral part of the overall drainage design provided by DBFL Consulting Engineers, with inputs and co-ordination from CSR as landscape design consultants. The details of SuDS tree pits, road edge details showing connections to the various SuDS elements and the stormwater system, are to engineer's design and details (refer to DBFL drawings). See drawing 22234-1-108 (LANDSCAPE SUDS Plan). If more detail is required, an updated detailed landscape plan could be submitted to SDCC for agreement prior to commencement on site.

8.6 ITEM NO. 7 (C) – REVISED LANDSCAPE MASTERPLAN

Item no. 7 (c) states:

“The applicant is requested to provide a revised detailed landscape plan, to be agreed with Public Realm, with full works specification, that accords with the specifications and requirements of Council’s Public Realm Section. The revised landscape plan shall integrate retained and enhanced hedgerows and trees, tree planting and SuDS drainage to provide strong green infrastructure links throughout the development in accordance with Clonburris SDZ Planning Scheme, Parks and Landscape Strategy, Biodiversity Management Plan and SDCC County Development Plan (2022-2028). The applicant is requested to incorporate the following in the revised Landscape Proposals:”

8.7 ITEM NO. 7 (C) (I) – REVISED LANDSCAPE MASTERPLAN

Item no. 7 (c) (i) states:

“Details of all natural SuDS features including swales, rain gardens, bioretention areas, tree pits, channel rills, kerbing, filter strips, ponds, detention basins, wetlands etc.”

8.8 RESPONSE TO ITEM NO. 7 (C) (I) – SUDS DETAILS

We refer the Planning Authority to response to 7b above and to the Landscape SuDS Plan where the DBFL SuDS design has been reflected and co-ordinated with landscape elements. An indicative planting list for SuDS features can be found in the planting schedule: See drawings 22234-1-108 (LANDSCAPE SUDS Plan); 22234-1-109 LANDSCAPE Planting Schedule.

If more detail is required, the applicant would be happy to include a condition to provide an updated detailed landscape plan which could be submitted to SDCC for agreement prior to commencement on site.

Details of natural SuDS features have been included in the information submitted for planning. Details of the bio-retention area and swales are shown on DBFL drawing CLB-T3-94-SW-XXX-DR-DBFL-CE-5303. Details of tree pits and the proposed green roof build-up are shown on drawing CLB-T3-94-SW-XXX-DR-DBFL-CE-5304. Details of the proposed permeable paving build-up are shown on drawing CLB-T3-95-SW-XXX-DR-DBFL-CE5202.

8.9 ITEM NO. 7 (C) (II) – REVISED TREE AND HEDGEROW PROTECTION DETAILS

Item no. 7 (c) (ii) states:

“Revised tree and hedgerow protection measures: show extent and position of tree and hedgerow protection fences including retention of Hedge 8.”

8.10 RESPONSE TO ITEM NO. 7 (C) (II) – REVISED TREE AND HEDGEROW PROTECTION DETAILS

As set out in the CSR response, careful consideration has been given to maintaining and protecting existing hedgerows. Existing hedgerows and areas of scrub thicket have been retained and integrated within the revised landscape scheme wherever possible. Due to the compact nature of the scheme and the required layout of the street network and residential areas as specified within the Clonburris SDZ planning document, the retention of existing hedgerows within the body of the site is not possible. New native and biodiverse hedgerow planting is proposed along the western boundary, along the future bridge and along the railway, to enhance the existing vegetation and provide a continuously vegetated green corridor. The proposed native hedging mix consists of a staggered double row of bare-root planting and will be protected by a chestnut paling fence to prevent potential access and misuse.

In relation to Hedge No. 8, please refer also to response to 7a above. Whilst maintaining and protecting Hedge No 8 would be a desirable outcome, it is unfortunately not a feasible proposition, noting the requirements of the construction of the future bridge (and its required width which is dictated by relevant design standards in respect of width etc.), and in conjunction with the requirement to provide access to the block along the eastern frontage, means that the retention of the hedge is not feasible.

The CSR response notes that maintaining and protecting fragments would be considered futile and efforts would be better spent replanting a new coherent length of hedgerow with a good mix of native species.

See drawing 22234-1-106 LANDSCAPE Planting Strategy Plan

For the layout of tree and hedgerow protection fences see drawing 22234-1-110 T3 Boundary Treatment Plan and arborist drawing Clonburris T3 Tree Protection Plan.

8.11 ITEM NO. 7 (C) (III) – TREE PLANTING DETAIL

Item no. 7 (c) (iii) states:

“Woodland planting should use the Miyawaki technique (3-5/m2) in order to establish quickly and reduce maintenance costs. This will require an establishment period and maintenance of 5 years for newly planted woodland areas prior to taking in charge.”

8.12 RESPONSE TO ITEM NO. 7 (C) (III) – TREE PLANTING DETAIL

Woodland planting is not proposed on Tile 3. Along the northern ecological corridor most vegetation is existing, protected scrub thicket with enhancements. Additional native hedgerows, parkland trees and feature trees are proposed to complete the ecological corridor and create a north-south green corridor along the future bridge. See drawing 22234-1-100 T3 Landscape Masterplan.

8.13 ITEM NO. 7 (C) (IV) – PLAYSPACE DETAIL

Item no. 7 (c) (iv) states:

“All playspace surfacing to be engineered woodchip surfacing”

8.14 RESPONSE TO ITEM NO. 7 (C) (IV) – PLAYSPACE DETAIL

Formal play spaces have not been proposed within TIC areas on Tile 3. Informal play opportunities are integrated along the shared path on the northern east-west green corridor. These spaces consist of large wooden logs, large boulders, play steps and sensory planting and would not require engineered surfacing. Details of the natural play will be submitted to SDCC for approval following a grant of planning once play elements are being sourced and will be in accordance with the relevant safety standards and SDCC play requirements.

8.15 ITEM NO. 7 (C) (V) – PLAYSPACE DETAIL

Item no. 7 (c) (v) states:

“Details of all play items and natural play features to be provided and agreed with SDCC Public Realm Section. Play areas must be universally accessible.”

8.16 RESPONSE TO ITEM NO. 7 (C) (V) – PLAYSPACE DETAIL

We refer the Planning Authority to response to 7(c) (iv) above.

8.17 ITEM NO. 7 (C) (VI) – LANDSCAPE DETAIL

Item no. 7 (c) (vi) states:

“Landscape Proposals to reflect the requirements of additional information items 1 and 2 above to deliver the required green infrastructure.”

8.18 RESPONSE TO ITEM NO. 7 (C) (VI) – LANDSCAPE DETAIL

Refer to response to 7 (a) (b) & (c)(i) above. SuDS elements are reflected on Landscape Proposals. Tree and hedgerow protection measures are shown on drawing Clonburris T3 Tree Protection Plan. If more detail is required, an updated detailed landscape plan could be submitted to SDCC for agreement prior to commencement on site.

9.0 ITEM NO. 8 - REQUEST FOR FURTHER INFORMATION – ARCHAEOLOGY

9.1 ITEM NO. 8 – ARCHAEOLOGY

Item no. 8 states:

“The applicant is requested to provide an Archaeological Assessment as follows:”

a. The applicant’s archaeologist is required to submit a Preliminary Report on the excavations to have already been carried out under Licence No. 22E0719 with the Department.

b. The applicant is required to engage the services of a suitably qualified Archaeologist to carry out the Archaeological Impact Assessment (AIA) which should include a programme of Archaeological Test Excavation (as an extension to Licence No. 22E0719) to respond to this request for Further Information. No sub-surface work shall be undertaken in the absence of the archaeologist without his/her express consent.

c. The archaeologist shall inspect the proposed development site (PDS) and detail the historical and archaeological background of the site and the boundary walls (consulting appropriate documentary sources) and review all cartographic sources and aerial photographs for the area.

d. The Archaeological Test Excavation must be carried out under licence from this Department and in accordance with an approved method statement; note a period of 5-6 weeks should be allowed to facilitate processing and approval of the licence application and method statement.

e. Test trenches shall be excavated at locations chosen by the archaeologist, having consulted the site drawings. Excavation is to take place to the uppermost archaeological horizons only, where they survive. Where archaeological material is shown to be present, the archaeologist shall stop works pending further advice from this Department. Please note that all features/archaeological surfaces within the test trenches are to be hand-cleaned and clearly visible for photographic purposes.

f. Having completed the work, the archaeologist shall submit a written report to this Department and the Local Authority describing the findings of the AIA and the results of the Archaeological Test Excavation. The report shall comment on the degree to which the extent, location and levels of all proposed foundations, service trenches and other sub-surface works required for the development will affect the archaeological remains. This should be illustrated with appropriate plans, sections, etc

g. Where archaeological material is shown to be present, further mitigation measures will be required; these may include refusal, redesign to allow for preservation in situ, excavation and/or monitoring as deemed appropriate. This Department will advise the Local Authority with regard to these matters. No decision should be made on this application until this Department and the Local Authority have had the opportunity to fully evaluate the findings of the AIA.

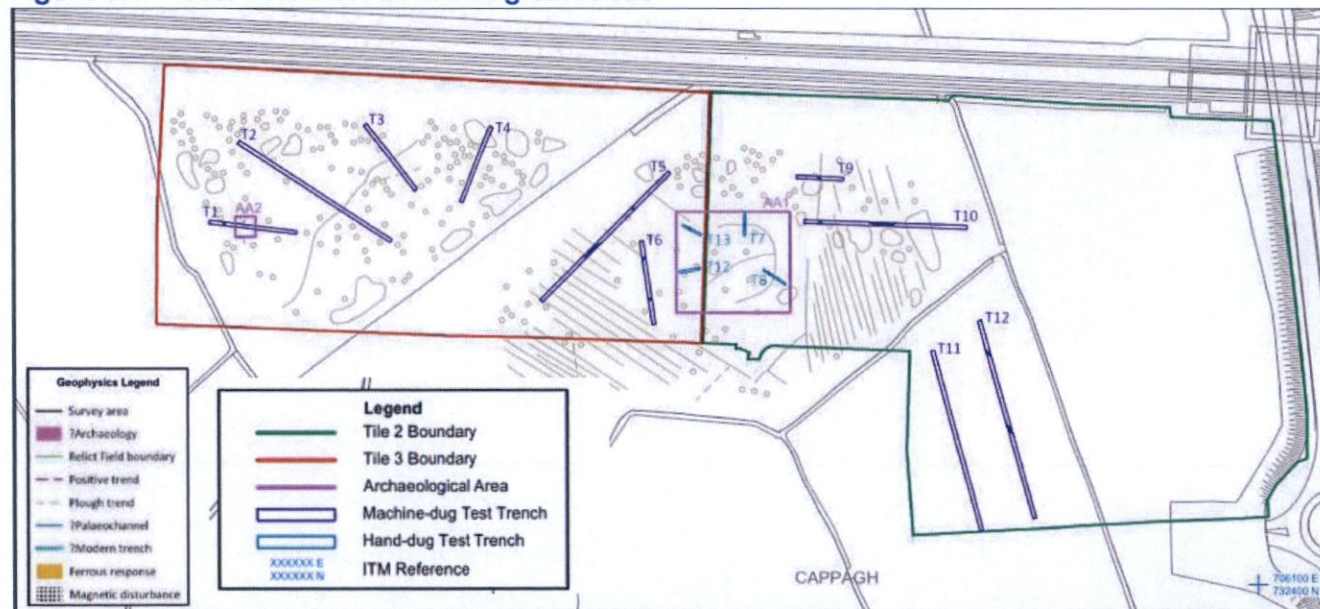
9.2 RESPONSE TO ITEM 8 (A-F)

We refer the Planning Authority to the enclosed Archaeology Assessment prepared by IAC, included with the Further Information Response.

The IAC Report addresses both Tiles 2 and 3 as the testing area is located across the boundary of the 2 no. applications.

The assessment was carried out by Fergal Murtagh under excavation licence 22E0719 and metal detection licence 22R0359 during November 2022 and March 2023. It follows a previous geophysical survey report carried out in June 2022 (Dowling 2022). Two additional hand dug trenches were excavated within Tile 3, during March 2023

Figure 9.1 – Detail view of Archaeological Areas



In March 2023, following the issuing of an RFI, two additional hand dug trenches were excavated across the projected extent of the possible enclosure DU017-036, partially located within Tile 3.

With reference to Tile 3 the IAC report outlines the following:

“Tile 3

Ground disturbances associated with the proposed development may have an adverse impact on the potential archaeological remains identified in Archaeological Area 1, which may relate to c. 20% of the recorded enclosure site DU017-036.

It is recommended that the small section of AA1 within Tile 3 be preserved by record as part of the Tile 2 works (as per the above mitigation measures). This will enable the whole of the site to be preserved by record in one phase of works.

Ground disturbances associated with the proposed development will have an adverse impact on the archaeological remains identified in Archaeological Area 2.

It is recommended that an area measuring c. 10m x 10m be opened around the metalled surface found in Trench 1 in order to facilitate its preservation by record (Archaeological Area 2) with further monitoring to establish the total length. All works should be carried out by an archaeologist under licence to the National Monuments Service of the DoHGLH. Ground disturbances may have an adverse impact on previously unrecorded small or isolated archaeological features or deposits that have the potential to survive beneath the current ground level and outside the footprint of the excavated test trenches.

It is recommended that all topsoil stripping in Tile 3 be monitored by a suitably qualified archaeologist. If any features of archaeological potential are discovered during the course of the works further archaeological mitigation may be required, such as preservation in situ or by record. Any further mitigation will require approval from the National Monuments Service of the DoHGLH.”

10.0 CONCLUSIONS

It is considered that the above, read in combination with the accompanying inputs by MCORM Architects, DBFL Consulting Engineers, and CSR Landscape Architects positively addresses the points raised by the Planning Authority in the Further Information Request on the 7th of February 2023.

The proposed development is considered to constitute proper and sustainable development and will contribute to a high-quality residential environment.

On this basis, it is respectfully submitted that the proposals are granted planning permission.