

- NOTES**
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH RELEVANT ARCHITECTS AND ENGINEERS DRAWINGS.
 - FIGURED DIMENSIONS ONLY TO BE TAKEN FROM THIS DRAWING. ALL DIMENSIONS TO BE CHECKED ON SITE. ENGINEER TO BE INFORMED IMMEDIATELY OF ANY DISCREPANCIES BEFORE ANY WORK PROCEEDS.
 - REFER TO DRAWING 20047-DOW-0000 FOR PROJECT SPECIFICATION.

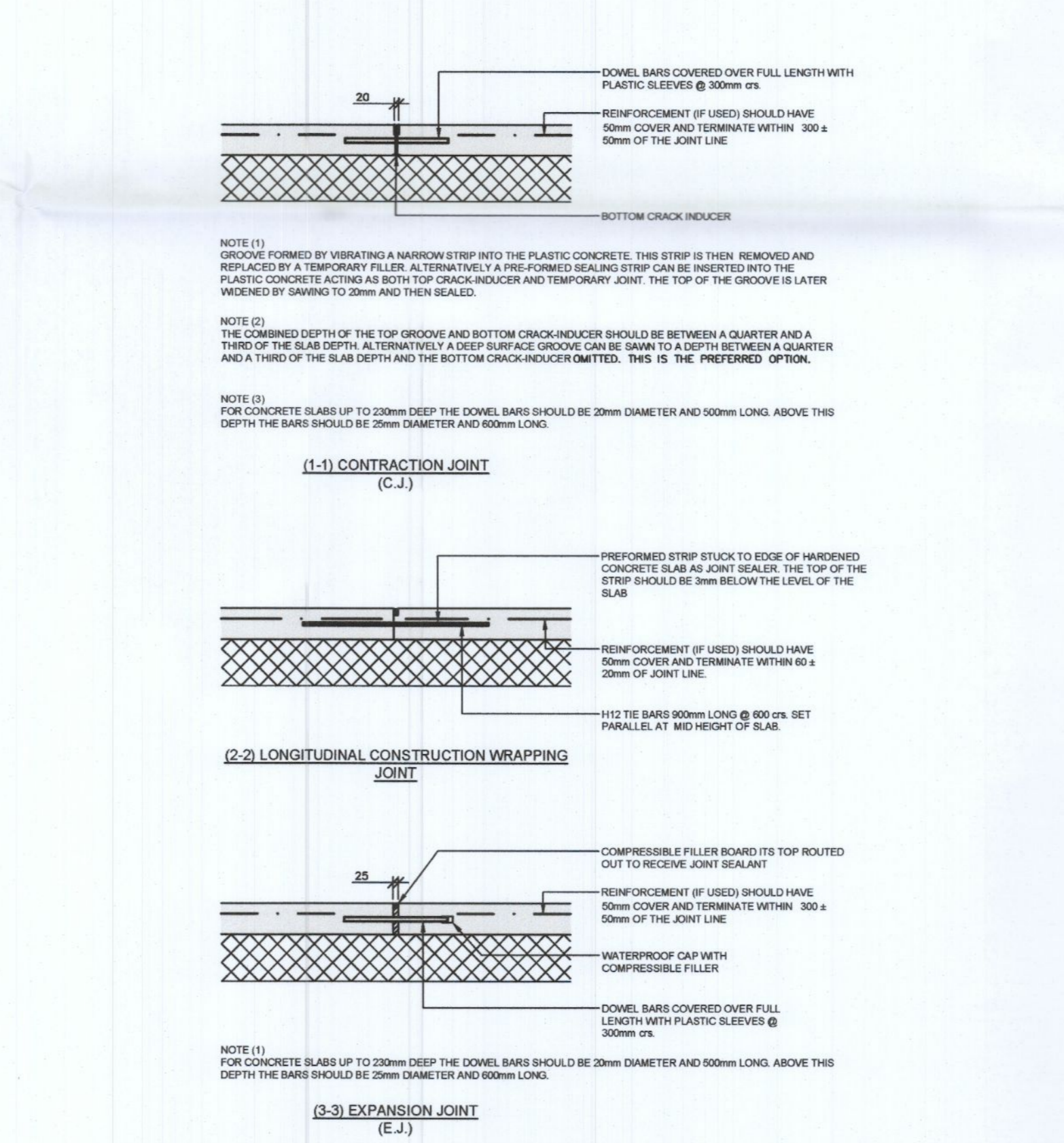
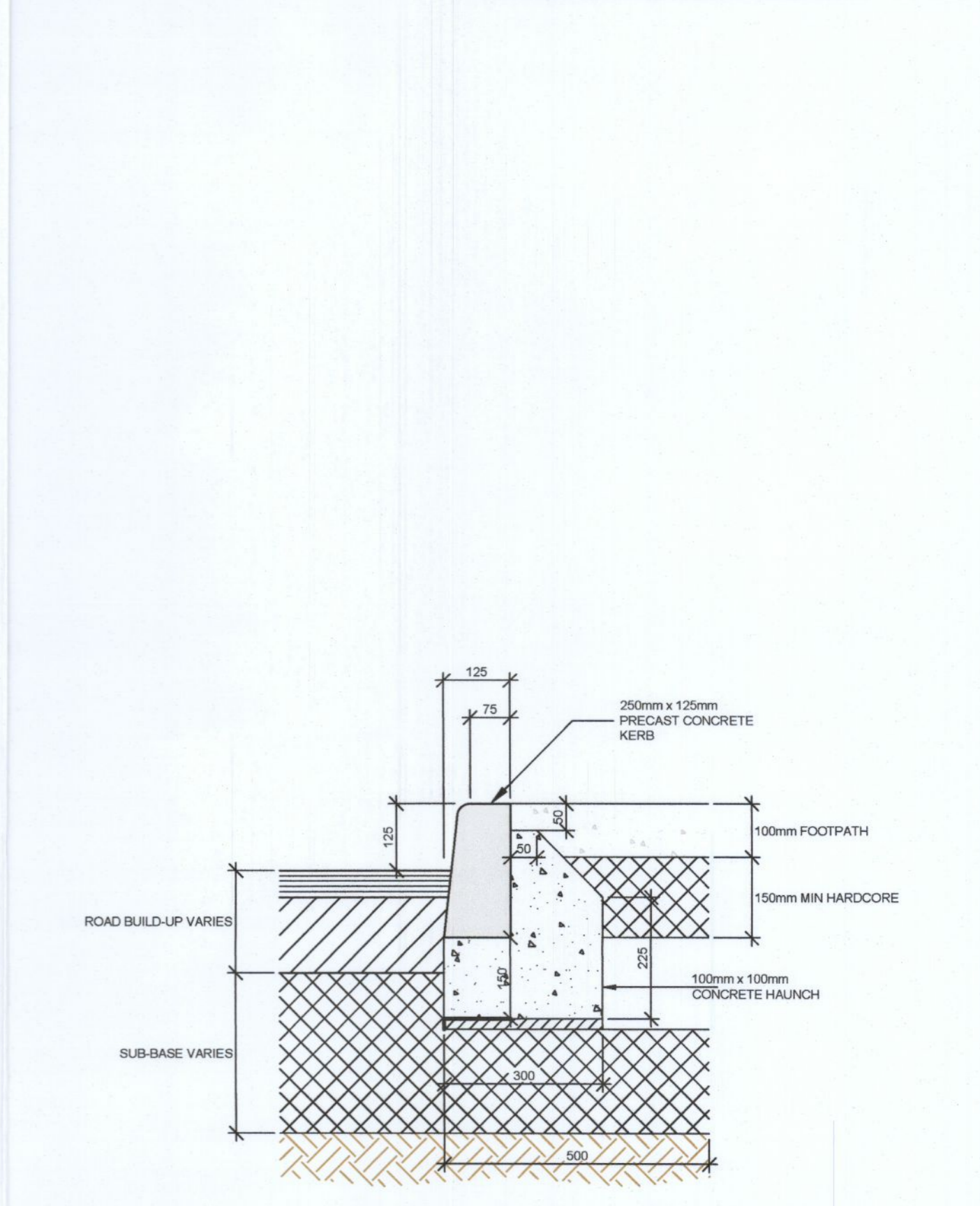
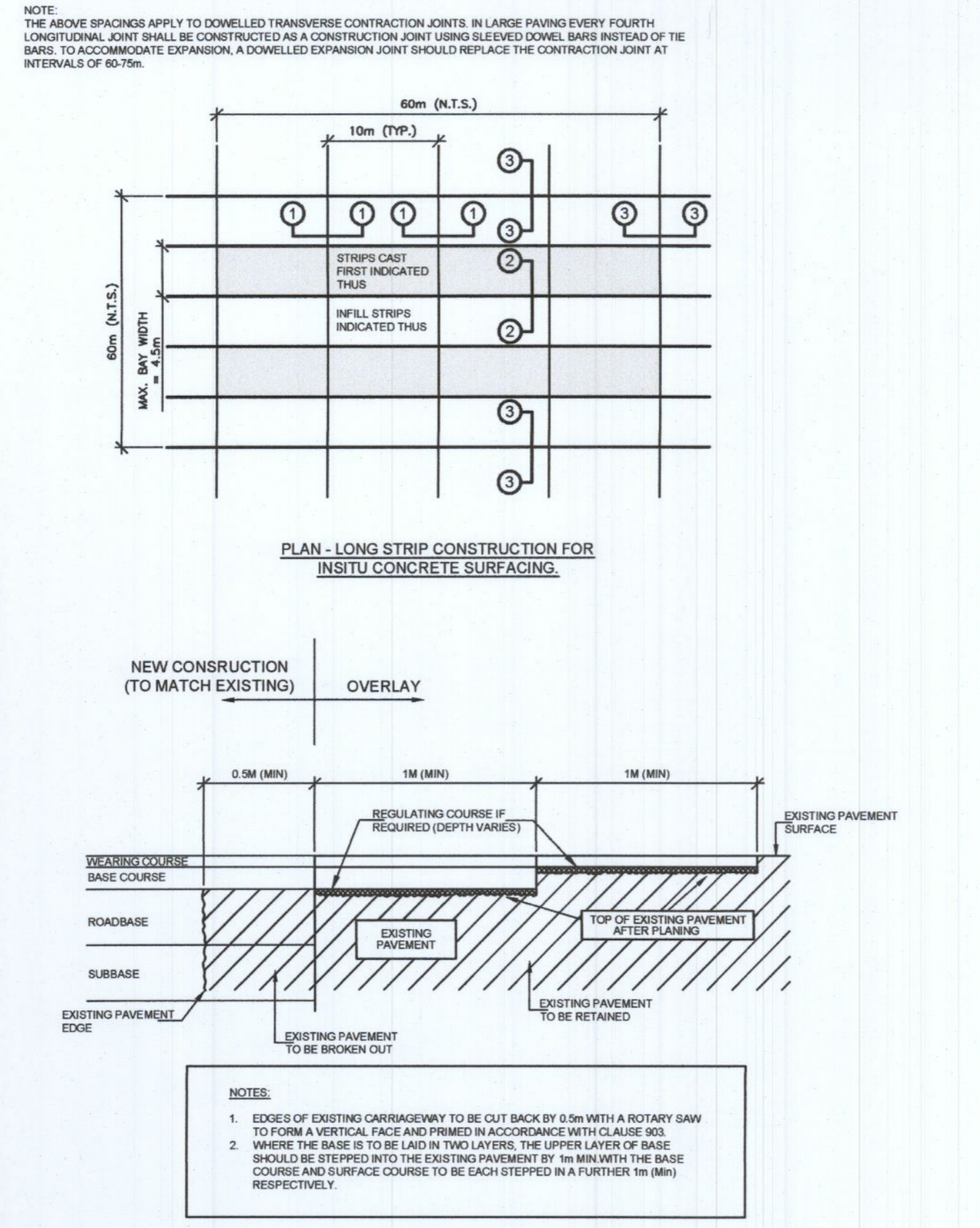
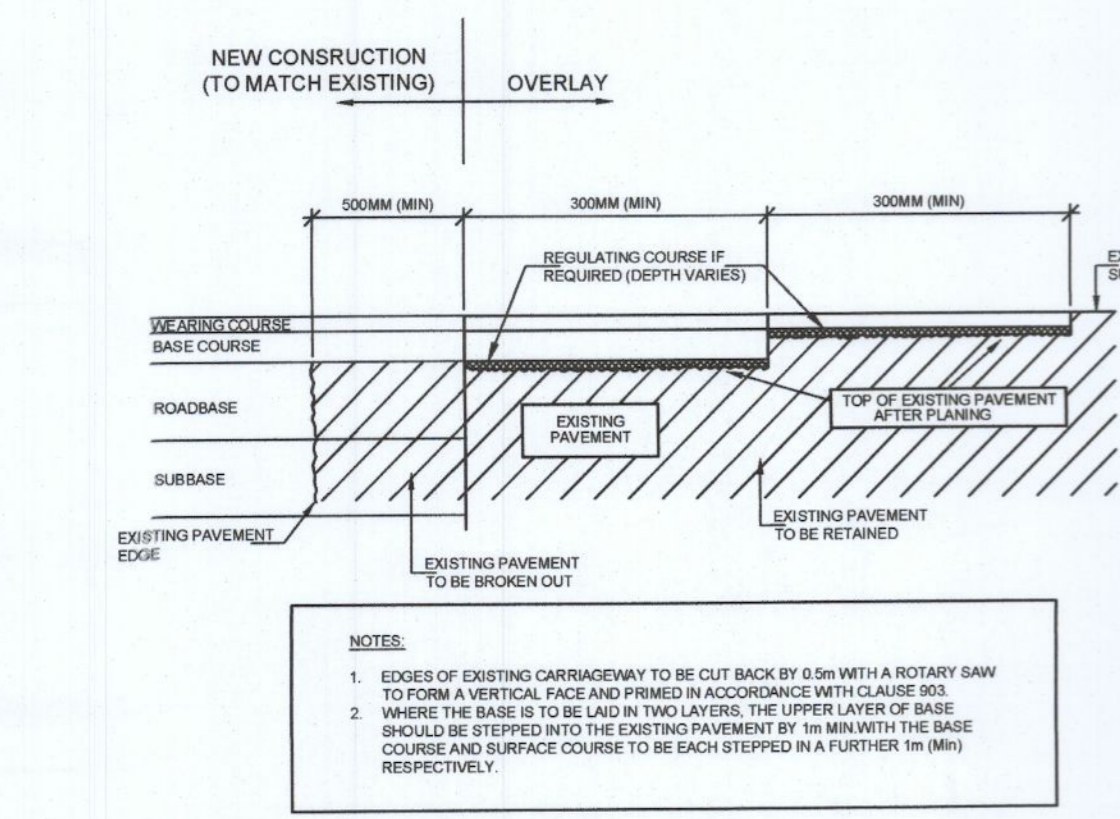
TABLE 1

SUB-GRADE CBR %	SUB-BASE THICKNESS OPTION A	CAPPING LAYER + SUB-BASE THICKNESS OPTION B
<2	-	600 + 225
2 TO 2.9	400	350 + 225
3 TO 4.9	325	350 + 225
5 TO 6.9	250	150 + 225
7 OR MORE	225	- + 225

NOTE: OPTION 'A' = CLAUSE 804 GRANULAR SUB-BASE MATERIAL TYPE B TO THE NRA SPECIFICATION FOR ROADWORKS. OPTION 'B' = 225mm THICK CLAUSE 804 GRANULAR SUB-BASE MATERIAL TYPE B OR CLASS 913 CLASS OF 11 OR 6P2 MATERIAL TO THE NRA SPECIFICATION FOR ROADWORKS.

TRANSVERSE JOINT SPACING FOR CONCRETE PAVING

	SLAB THICKNESS (mm)	MAXIMUM SPACING (m)
UNREINFORCED CONCRETE	100 - 150	3
	151 - 175	4
	176 - 200	4.5
	201 - 250	5
REINFORCE CONCRETE	REINFORCEMENT LONG MESH TO BS4463	MAXIMUM SPACING (m) ANY SLAB THICKNESS
	C283	15
	C385	20
	C503	25



Project: PROPOSED HOUSING DEVELOPMENT AT CLONBRONE, LUCAN, DUBLIN 20.

Client: NACUL DEVELOPMENTS Ltd.

Drawing Title: ROAD SURFACING DETAILS

drawn by:	AG	date:	30.11.20	scale:	N/A	@:	A1	chk:	IC
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20047 - DOW - 00 - XX - DR - CE

Project	Originator	Volume	Level	Type	Role
20047	4004				P03

DOW Project No. dfg. no. rev.

S0 - SUITABLE FOR PLANNING

Suitability Status: Code - Description

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