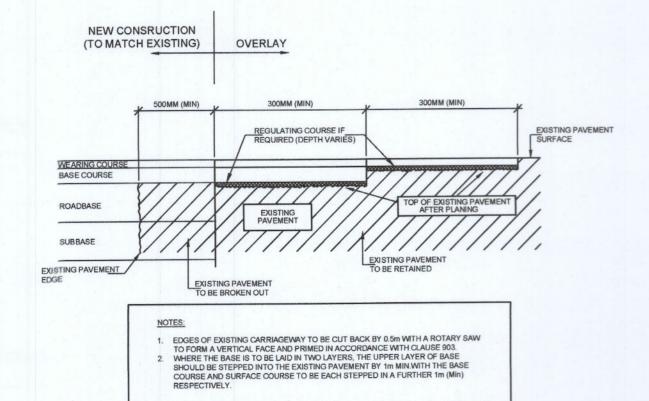


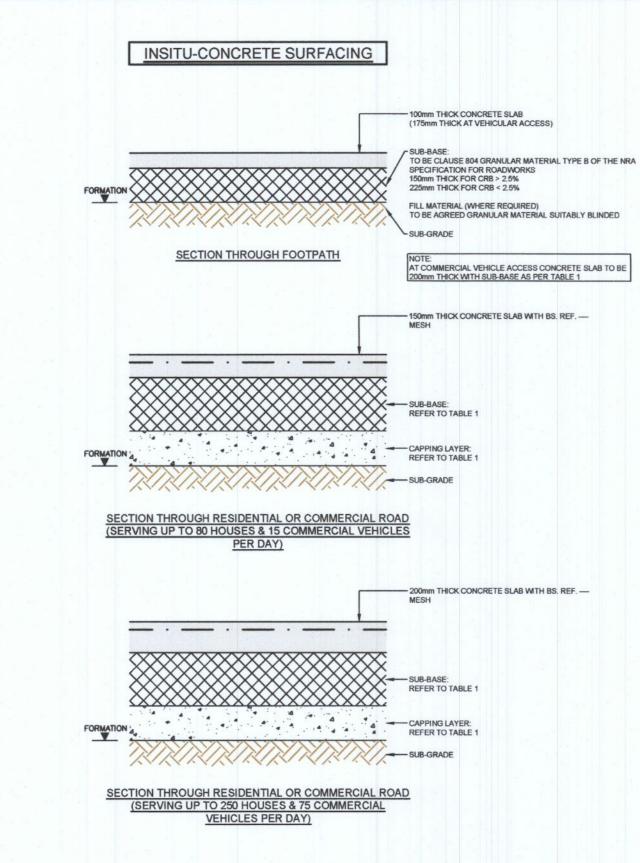
## SECTION THROUGH POROUS ASPHALT CAR PARKING & PLAY AREAS

TABLE 1						
SUB-GRADE CBR %	SUB-BASE THICKNESS OPTION A	CAPPING LAYER + SUB-BASE THICKNESS OPTION B				
<2		600 + 225				
2 TO 2.9	400	350 + 225				
3 TO 4.9	325	350 + 225				
5 TO 6.9	250	150 + 225				
7 OR MORE	225	- +225				

OPTION 'A' = CLAUSE 804 GRANULAR SUB-BASE MATERIAL TYPE B TO THE NRA SPECIFICATION FOR ROADWORKS
OPTION 'B' = 225mm THICK CLAUSE 804 GRANULAR SUB-BASE MATERIAL TYPE B ON CLAUSE 613 CLASS 6F1 OR 6F2 MATERIAL TO
THE NRA SPECIFICATION FOR ROADWORKS

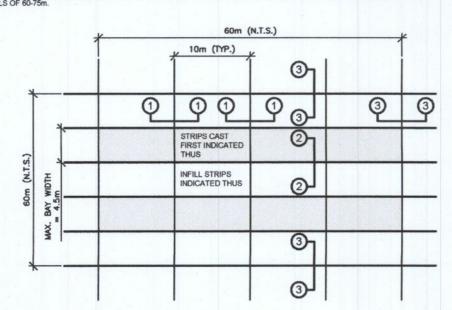


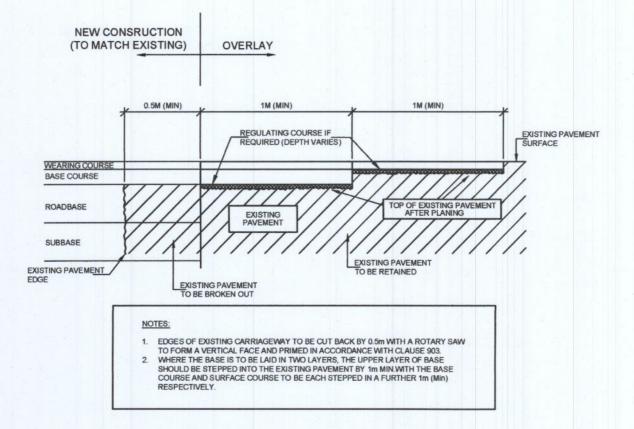
LONGITUDINAL JOINT BETWEEN NEW CONSTRUCTION AND EXISTING ROAD



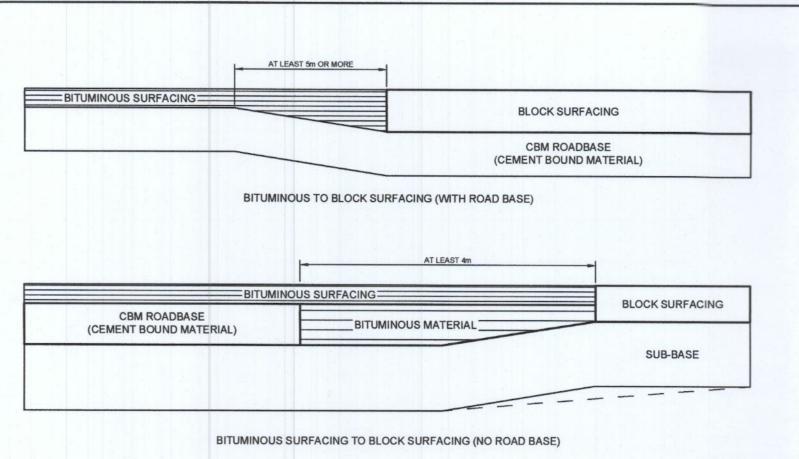
TRANSVERSE JO	OINT SPACING FOR CO	ONCRETE PAVING		
	SLAB THICKNESS (mm)	MAXIMUM SPACING (m)		
UNREINFORCED CONCRETE	100 - 150	3		
	151 - 175	4		
	176 - 200	4.5		
	201 - 250	5		
REINFORCE CONCRETE	REINFORCEMENT LONG MESH TO BS4483	MAXIMUM SPACING (m) AN SLAB THICKNESS		
	C283	15		
	C385	20		
	C503	25		

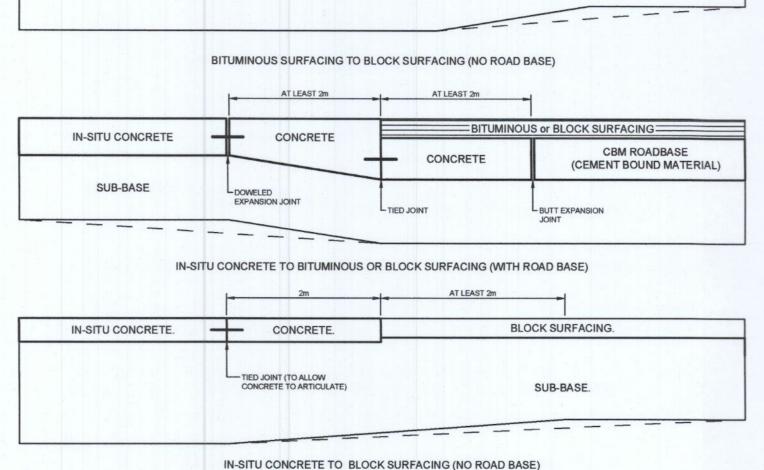
THE ABOVE SPACINGS APPLY TO DOWELLED TRANSVERSE CONTRACTION JOINTS. IN LARGE PAVING EVERY FOURTH LONGITUDINAL JOINT SHALL BE CONSTRUCTED AS A CONSTRUCTION JOINT USING SLEEVED DOWEL BARS INSTEAD OF TIE BARS. TO ACCOMMODATE EXPANSION, A DOWELLED EXPANSION JOINT SHOULD REPLACE THE CONTRACTION JOINT AT



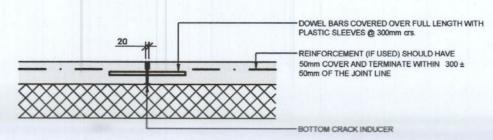


TRANSVERSE JOINT BETWEEN NEW CONSTRUCTION AND EXISTING ROAD





TRANSITIONS BETWEEN DIFFERENT SURFACES

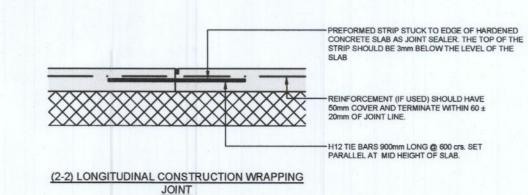


GROOVE FORMED BY VIBRATING A NARROW STRIP INTO THE PLASTIC CONCRETE. THIS STRIP IS THEN REMOVED AND REPLACED BY A TEMPORARY FILLER. ALTERNATIVELY A PRE-FORMED SEALING STRIP CAN BE INSERTED INTO THE PLASTIC CONCRETE ACTING AS BOTH TOP CRACK-INDUCER AND TEMPORARY JOINT. THE TOP OF THE GROOVE IS LATER WIDENED BY SAWING TO 20mm AND THEN SEALED.

THE COMBINED DEPTH OF THE TOP GROOVE AND BOTTOM CRACK-INDUCER SHOULD BE BETWEEN A QUARTER AND A THIRD OF THE SLAB DEPTH. ALTERNATIVELY A DEEP SURFACE GROOVE CAN BE SAWN TO A DEPTH BETWEEN A QUARTER AND A THIRD OF THE SLAB DEPTH AND THE BOTTOM CRACK-INDUCER OMITTED. THIS IS THE PREFERRED OPTION.

NOTE (3)
FOR CONCRETE SLABS UP TO 230mm DEEP THE DOWEL BARS SHOULD BE 20mm DIAMETER AND 500mm LONG. ABOVE THIS DEPTH THE BARS SHOULD BE 25mm DIAMETER AND 600mm LONG

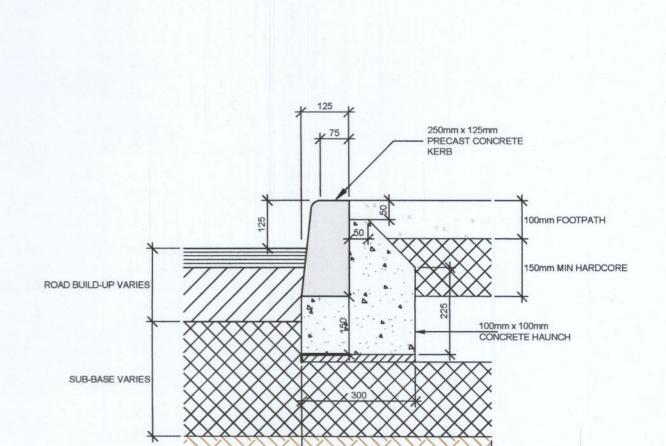
## (1-1) CONTRACTION JOINT



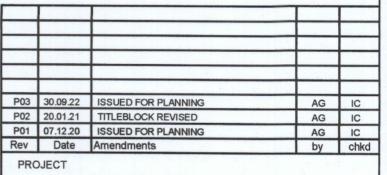
COMPRESSIBLE FILLER BOARD ITS TOP ROUTED OUT TO RECEIVE JOINT SEALANT - REINFORCEMENT (IF USED) SHOULD HAVE 50mm COVER AND TERMINATE WITHIN 300 ±

LENGTH WITH PLASTIC SLEEVES @ 300mm crs. NOTE (1) FOR CONCRETE SLABS UP TO 230mm DEEP THE DOWEL BARS SHOULD BE 20mm DIAMETER AND 500mm LONG. ABOVE THIS DEPTH THE BARS SHOULD BE 25mm DIAMETER AND 600mm LONG.

(3-3) EXPANSION JOINT (E.J.)



TYPICAL PRECAST CONCRETE KERB DETAIL



PROPOSED HOUSING DEVELOPMENT AT CLONBRONE, LUCAN, DUBLIN 20

CLIENT

NACUL DEVELOPMENTS Ltd.

DRAWING TITLE

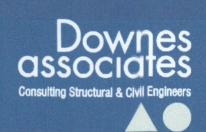
ROAD SURFACING DETAILS

drawn by: AG	date: 30.11.20	scale: N	VA.	@ A1	chk: IC
20047 -	- DOW -	00 -	XX-	-DR	R-CE
20047 DOW Project No.	4C drg. no.	04	rev.	Р	03

S0 - SUITABLE FOR PLANNING

Suitability Status: Code - Description Cashel Business Centre, Cashel Road, Kimmage, Dublin 12 T 01 4901611

E admin@downesassociates.ie www.downesassociates.ie



REFER TO DRAWING 20047-DOW-0000 FOR PROJECT SPECIFICATION.

NOTES

THIS DRAWING IS TO BE READ IN CONJUNCTION WITH

RELEVANT ARCHITECTS AND ENGINEERS DRAWINGS.

FIGURED DIMENSIONS ONLY TO BE TAKEN FROM THIS

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