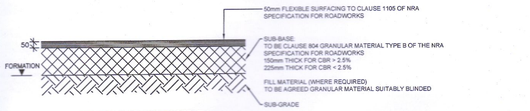
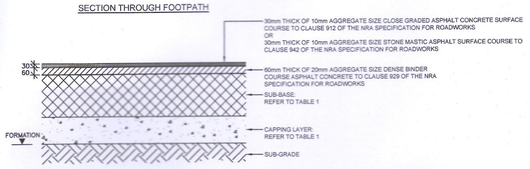
BITUMINOUS SURFACING

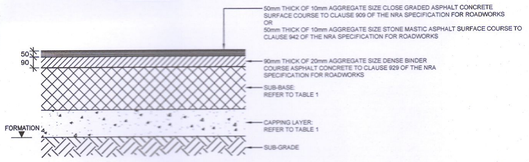
FOR POROUS ASPHALT SURFACING DETAILS REFER TO SPECIFIC SITE DRAWINGS & SPECIFICATIONS



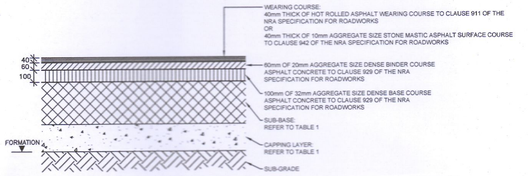
SECTION THROUGH FOOTPATH



SECTION THROUGH CAR-PARKING AND PLAY AREAS

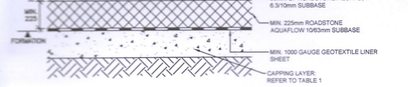


SECTION THROUGH LIGHT TRAFFIC ROADWAY



SECTION THROUGH HEAVY ACCESS ROUTES (INCLUDING COMMERCIAL / RESIDENTIAL ROADS AND HGV / BUS ACCESS

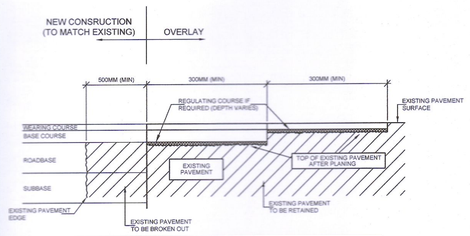
POROUS ASPHALT



SECTION THROUGH POROUS ASPHALT CAR PARKING & PLAY AREAS

TABLE 1		
SUB-GRADE CBR %	SUB-BASE THICKNESS OPTION A	CAPPING LAYER + SUB-BASE THICKNESS OPTION B
<2	-	600 + 225
2 TO 2.9	400	350 + 225
3 TO 4.9	325	350 + 225
5 TO 6.9	250	150 + 225
7 OR MORE	225	- + 225

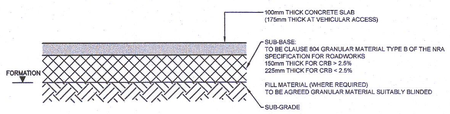
NOTE:
OPTION 'A' = CLAUSE 804 GRANULAR SUB-BASE MATERIAL TYPE B TO THE NRA SPECIFICATION FOR ROADWORKS
OPTION 'B' = 225mm THICK CLAUSE 804 GRANULAR SUB-BASE MATERIAL TYPE B ON CLAUSE 613 CLASS 6F1 OR 6F2 MATERIAL TO THE NRA SPECIFICATION FOR ROADWORKS



NOTES:
1. EDGES OF EXISTING CARRIAGEWAY TO BE CUT BACK BY 0.5m WITH A ROTARY SAW TO FORM A VERTICAL FACE AND PRIMED IN ACCORDANCE WITH CLAUSE 903.
2. WHERE THE BASE IS TO BE LAID IN TWO LAYERS, THE UPPER LAYER OF BASE SHOULD BE STEPPED INTO THE EXISTING PAVEMENT BY 1m MIN WITH THE BASE COURSE AND SURFACE COURSE TO BE EACH STEPPED IN A FURTHER 1m (Min) RESPECTIVELY.

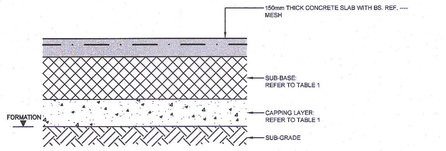
LONGITUDINAL JOINT BETWEEN NEW CONSTRUCTION AND EXISTING ROAD

INSITU-CONCRETE SURFACING

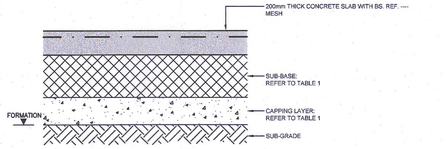


SECTION THROUGH FOOTPATH

NOTE:
AT COMMERCIAL VEHICLE ACCESS CONCRETE SLAB TO BE 200mm THICK WITH SUB-BASE AS PER TABLE 1



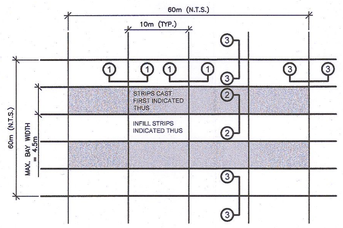
SECTION THROUGH RESIDENTIAL OR COMMERCIAL ROAD (SERVING UP TO 80 HOUSES & 15 COMMERCIAL VEHICLES PER DAY)



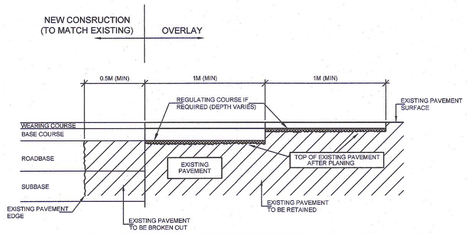
SECTION THROUGH RESIDENTIAL OR COMMERCIAL ROAD (SERVING UP TO 250 HOUSES & 75 COMMERCIAL VEHICLES PER DAY)

TRANSVERSE JOINT SPACING FOR CONCRETE PAVING		
UNREINFORCED CONCRETE	SLAB THICKNESS (mm)	MAXIMUM SPACING (m)
	100 - 150	3
	151 - 175	4
	176 - 200	4.5
	201 - 250	5
REINFORCE CONCRETE	REINFORCEMENT LONG MESH TO BS4483	MAXIMUM SPACING (m) ANY SLAB THICKNESS
	C283	15
	C385	20
	C503	25

NOTE:
THE ABOVE SPACINGS APPLY TO DOWELLED TRANSVERSE CONTRACTION JOINTS. IN LARGE PAVING EVERY FOURTH LONGITUDINAL JOINT SHALL BE CONSTRUCTED AS A CONSTRUCTION JOINT USING SLEEVED DOWEL BARS INSTEAD OF TIE BARS. TO ACCOMMODATE EXPANSION, A DOWELLED EXPANSION JOINT SHOULD REPLACE THE CONTRACTION JOINT AT INTERVALS OF 60-75m.

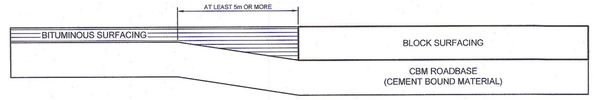


PLAN - LONG STRIP CONSTRUCTION FOR INSITU CONCRETE SURFACING.

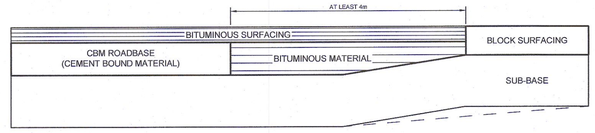


NOTES:
1. EDGES OF EXISTING CARRIAGEWAY TO BE CUT BACK BY 0.5m WITH A ROTARY SAW TO FORM A VERTICAL FACE AND PRIMED IN ACCORDANCE WITH CLAUSE 903.
2. WHERE THE BASE IS TO BE LAID IN TWO LAYERS, THE UPPER LAYER OF BASE SHOULD BE STEPPED INTO THE EXISTING PAVEMENT BY 1m MIN WITH THE BASE COURSE AND SURFACE COURSE TO BE EACH STEPPED IN A FURTHER 1m (Min) RESPECTIVELY.

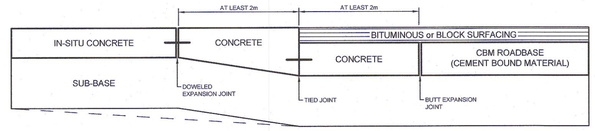
TRANSVERSE JOINT BETWEEN NEW CONSTRUCTION AND EXISTING ROAD



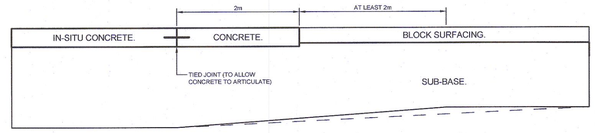
BITUMINOUS TO BLOCK SURFACING (WITH ROAD BASE)



BITUMINOUS SURFACING TO BLOCK SURFACING (NO ROAD BASE)

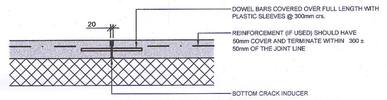


IN-SITU CONCRETE TO BITUMINOUS OR BLOCK SURFACING (WITH ROAD BASE)



IN-SITU CONCRETE TO BLOCK SURFACING (NO ROAD BASE)

TRANSITIONS BETWEEN DIFFERENT SURFACES.

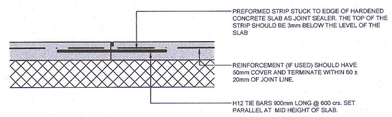


NOTE (1)
GROOVE FORMED BY VIBRATING A NARROW STRIP INTO THE PLASTIC CONCRETE. THIS STRIP IS THEN REMOVED AND REPLACED BY A TEMPORARY FILLER. ALTERNATIVELY A PRE-FORMED SEALING STRIP CAN BE INSERTED INTO THE PLASTIC CONCRETE ACTING AS BOTH TOP CRACK-INDUCER AND TEMPORARY JOINT. THE TOP OF THE GROOVE IS LATER WIDENED BY SAWING TO 20mm AND THEN SEALED.

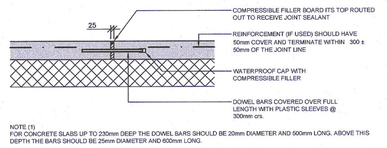
NOTE (2)
THE COMBINED DEPTH OF THE TOP GROOVE AND BOTTOM CRACK-INDUCER SHOULD BE BETWEEN A QUARTER AND A THIRD OF THE SLAB DEPTH. ALTERNATIVELY A DEEP SURFACE GROOVE CAN BE SAWN TO A DEPTH BETWEEN A QUARTER AND A THIRD OF THE SLAB DEPTH AND THE BOTTOM CRACK-INDUCER **OMITTED. THIS IS THE PREFERRED OPTION.**

NOTE (3)
FOR CONCRETE SLABS UP TO 230mm DEEP THE DOWEL BARS SHOULD BE 20mm DIAMETER AND 500mm LONG. ABOVE THIS DEPTH THE BARS SHOULD BE 25mm DIAMETER AND 600mm LONG.

(1-1) CONTRACTION JOINT
(C.J.)



(2-2) LONGITUDINAL CONSTRUCTION WRAPPING JOINT

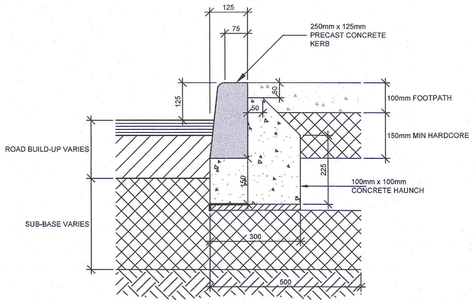


NOTE (1)
FOR CONCRETE SLABS UP TO 230mm DEEP THE DOWEL BARS SHOULD BE 20mm DIAMETER AND 600mm LONG. ABOVE THIS DEPTH THE BARS SHOULD BE 25mm DIAMETER AND 600mm LONG.

(3-3) EXPANSION JOINT
(E.J.)

NOTES

1. THIS DRAWING TO BE READ IN CONJUNCTION WITH RELEVANT ARCHITECTS AND ENGINEERS DRAWINGS.
2. FIGURED DIMENSIONS ONLY TO BE TAKEN FROM THIS DRAWING. ALL DIMENSIONS TO BE CHECKED ON SITE. ENGINEER TO BE INFORMED IMMEDIATELY OF ANY DISCREPANCIES BEFORE WORK PROCEEDS.



TYPICAL PRECAST CONCRETE KERB DETAIL

Rev	Date	Amendments	by	chkd
P01	07.07.22	ISSUED FOR PLANNING	AG	IC

PROJECT
PROPOSED DWELLING AT
No. 46 LIMELIKN ROAD, DUBLIN 12

CLIENT
JOHN McWEENEY

DRAWING TITLE
ROAD SURFACING DETAILS

drawn by: AG | date: 07.07.22 | scale: N.T.S @ A1 | chk: IC

LIME - DOW - 00 - XX - DR - CE
Project | Originator | Volume | Level | Type | Role

21019	4005	P01
DOW Project No.	drg. no.	rev.

S4: SUITABLE FOR PLANNING
Suitability Status: Code - Description

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