

Design Statement

for

Warehousing Development

at

**Kingswood Road,
Citywest Business Park, Dublin 24.**

Job No: D1736
Client: Rockface Developments Limited
Date: June 2022
Local Authority: South Dublin County Council
Revision: PL1 (22/06/22)

Calmount Park, Ballymount, Dublin 12.

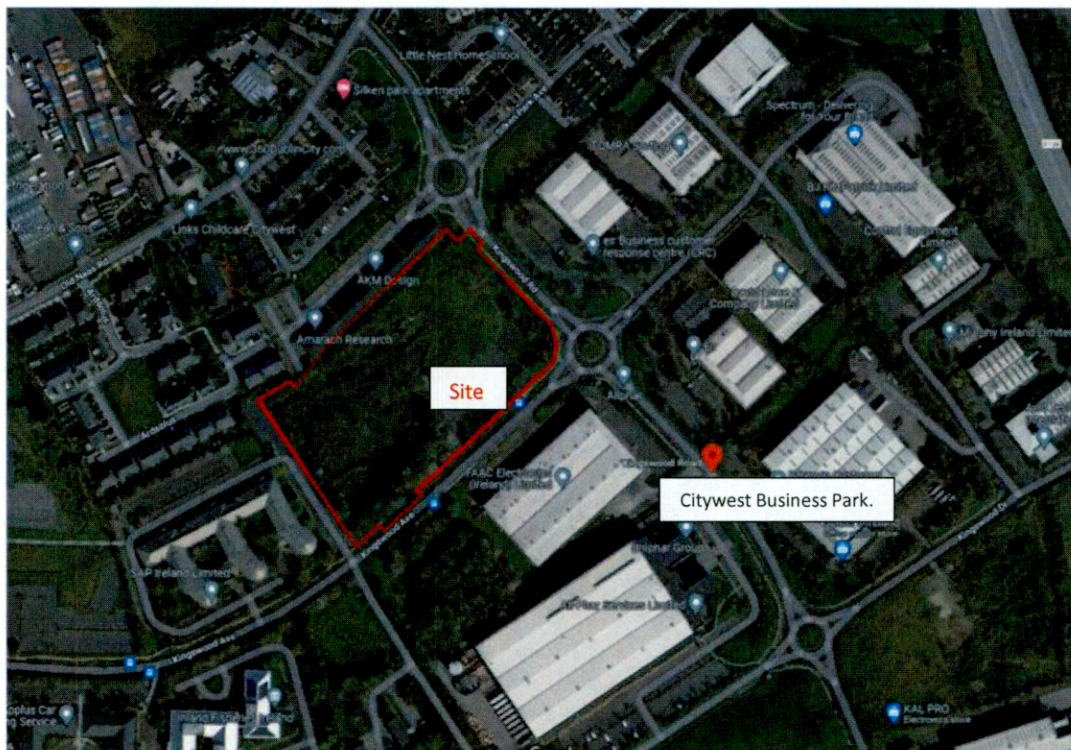
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I. Introduction

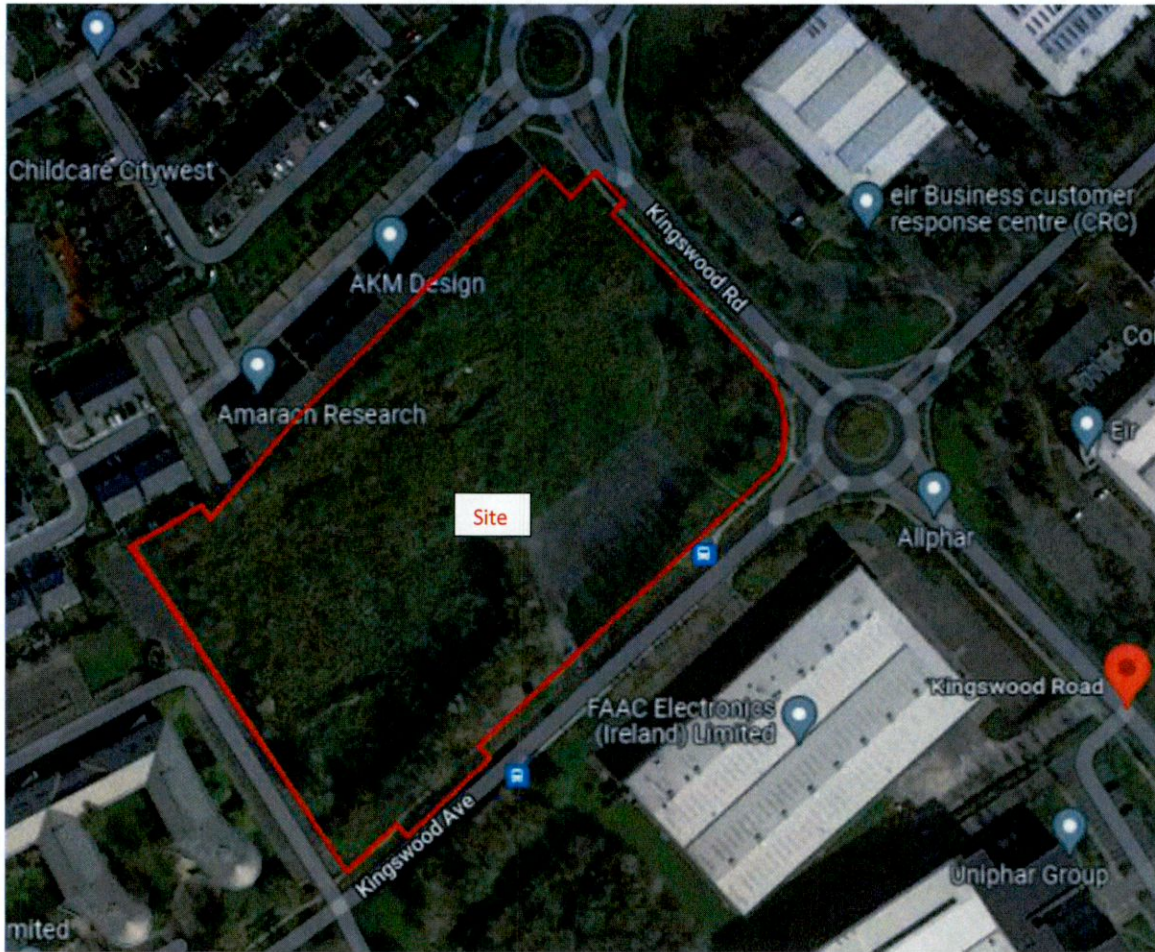
This Design Statement has been prepared by Kavanagh Burke Consulting Engineers in conjunction with Thornton O'Connor Town Planning and Enviroguide Consulting regarding the proposed warehouse development at Kingswood Road, Citywest Business Park, Co. Dublin, for applicants Rockface Development Limited.

The purpose of this document is to demonstrate the design principles and concepts that have been considered for a warehousing development on a suitably zoned site, within Citywest Business Park, located as shown below;



The location of the site can be seen in the above photo in the context of its proximity to neighbouring warehouse units to the south and east, and an established housing development to the north-west side.

An office development exists to the south west with ancillary carpark directly adjacent to the site south western boundary. Also, a linear office development exists directly to the north western boundary as shown above.



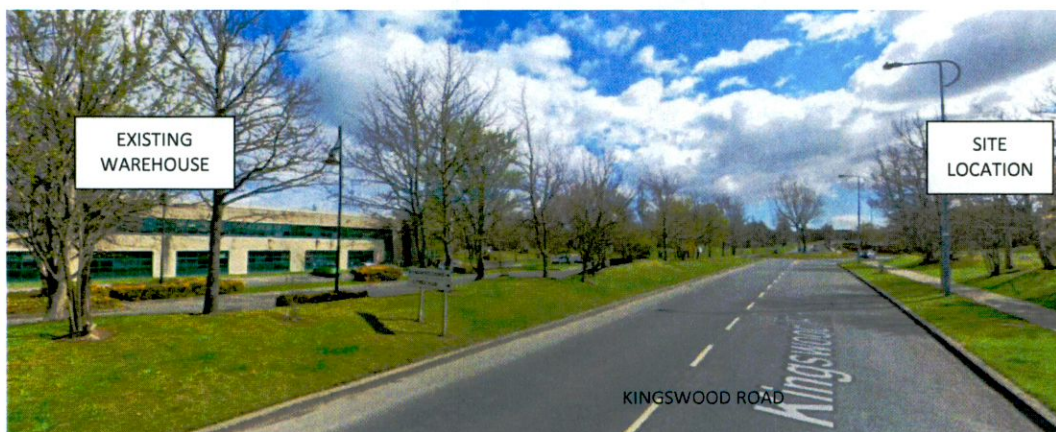
Proposed site within Citywest Business Park.

II. Rationale for Development and Site Arrangement

The rationale for the development is to provide a single occupancy warehouse building with ancillary offices accompanied by safe and spacious HGV access /egress and docking/marshalling yard plus ancillary carparking separated from the HGV activity. The site is appropriately zoned as Enterprise&Employment-EE zoned lands. The proposed development comprises infill warehousing within a mature business park environment which borders existing office and residential developments to the north-west and the existing warehouses to the south-east.



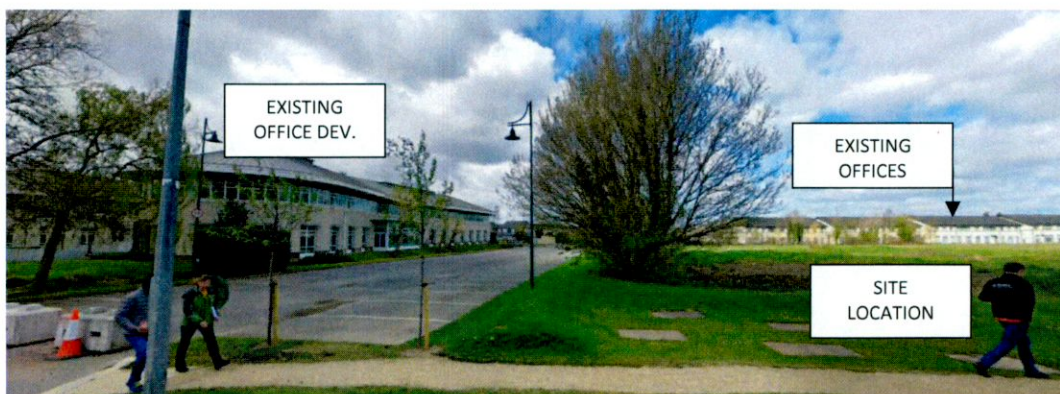
Proposed Warehouse Development on left, existing offices (mid picture) & existing residential area plus Kingswood Ave to the North (on right).



Neighbouring Warehouse development to the east of the site.



Existing Warehouse to the south (left in above photo) of the site.



Existing Office Development to the south-west of the site (on left), existing offices (on back) and proposed Warehouse development on right.

The existing adjacent developments were a strong influence on how this proposal was positioned on site, accessed and screened. The fundamental design consideration was to provide the HGV access and associated yard towards the rear (south west) furthest away for the dwellings. Also, between the boundary with the existing dwellings (north west corner) we propose a landscape screen with acoustic barrier adjacent to the dwelling elevations.

Where the site bounds the existing offices, a landscape screen is proposed with 10m wide minimum throughout the northern boundary. The proposed Ancillary Office is located on the south east corner and has appropriate curtain wall glazing treatment throughout the elevation. The office position on site was influenced also to avoid possible lighting impact to the existing dwellings.

As with many similar sites throughout the Citywest Business Campus, warehousing facilities exist in close proximity to office space, retail buildings and residential developments while being separated by roads, earth mounding and specifically designed soft landscaping.

Included in this application is a detailed landscaping plan which has adopted the Citywest Business Park style of surface mounding and landscaping.

The proposed warehouse development on this undeveloped site will further improve the employment opportunities available in the general area and will bring associated advantages to the local economy and local community.

The design goal to provide modern yet practical and appropriate building appearance at this location, was carried through the design development to the presented proposal which forms this planning application. The building arrangement and design have therefore sought to provide a layout and elevational detail that creates a suitable identity for a development at this major employment node.

III. Site Analysis

The subject site is located in Citywest Business Park. Kingswood Road links the Old Naas Road to the North (Naas Road) and the Citywest Ave to the south while the Kingswood Ave links with Citywest Road (N82) to the west. Citywest Business Park is almost fully developed with the subject site being one of the few remaining undeveloped sites. The Citywest Road, N81 and N7 provides a beneficial logistics corridor for warehousing & industrial developments that rely on a quality road network and an available workforce to meet the career opportunity requirements created.

The site as existing is mostly greenfield with manged grass boundary strips and footpaths in places. This boundary treatment will be enhanced with further earth mounding and landscaping as previously mentioned.

As the site is on the outskirts of the industrial/warehousing development of Citywest Business Park, high quality boundary treatments will be important in providing the transition between the newly developed warehouse facility and existing residential developments.

Proposed building levels have been chosen to provide a functional warehouse facility with practical separate car and HGV access locations. The existing levels of the adjacent roads lends itself to a small level difference to access the south (HGV yard) & east (carpark) of the site. Internally, warehouse and ancillary office have the same finished floor level.

IV. Built Form and Corporate Identity

The design of the warehouse and integrated ancillary offices are simple with a contemporary architectural finish and as such the proposed development can be

easily assimilated into the surrounding context on designated enterprise and employment lands, providing completion to an infill site within Citywest Business Park while being sensitive to existing surroundings.

The use of different cladding profiles, colour shades plus horizontal and vertical placement of the panels have been carefully considered to work together to provide what are large warehouse buildings that are not perceived as intrusive or imposing in scale or height. The proposed wave through the parapet suggests a smooth appearance that helps to minimize the impact of the size of the warehouse. The landscape treatment composes a simple and sophisticated finish to the overall development.



Proposed building.



Proposed building.

The subject proposal has been designed in a similar way to existing industrial/warehousing buildings in the business park.

Samples of some nearby buildings are included below:



Existing Electronic Facility directly to south of subject site.

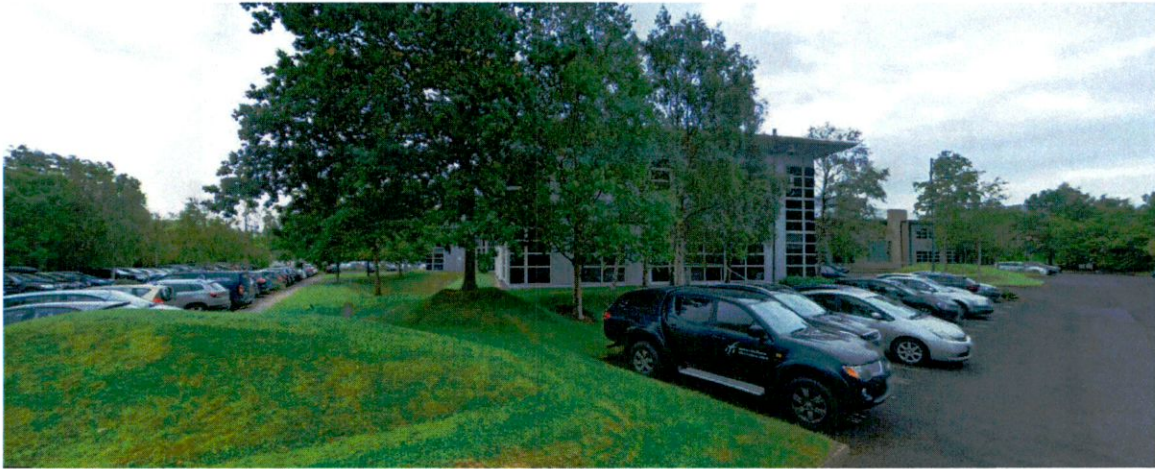


Existing Pharmaceutical Logistic Facility to the south of subject site.



Existing facilities to the east side of the the subject site.

Finishes include metal cladding, strip glazing & curtain wall panells.



Nearby developments in Citywest Business Park – Note industrial/warehousing buildings with site perimeter earth mounds and soft landscaping.



As previously noted.



As previously noted.



As previously noted.

V. Access and Movement

The proposed development comprises a warehouse development with HGV and car access/ egress locations positioned to minimise impact on existing traffic flow so that the greater transport is not negatively impacted on by the inclusion of the facility.

In order to facilitate both car parking and docking/loading bays, car parking access and circulation is separated from HGV activity. The building has been designed by taking into consideration the nature and scale of the facility and providing elevations with appropriate enhancement to a level that avoids both elevational monotony and over detailing. A key consideration in the design was to provide a strong landscaped site perimeter, similar to the existing boundaries abutting estate roads in general in the Business Park.

Pedestrian connectivity is provided outside the site by a network of existing roadside footpaths – from which a pedestrian access will be formed to the proposed ancillary offices/ carparking area.

It is not a design desire to have pedestrians entering the HGV access where heavy goods vehicles will traverse and marshal, therefore a pedestrians entrance and on-site route is not provided to the rear of the facility.

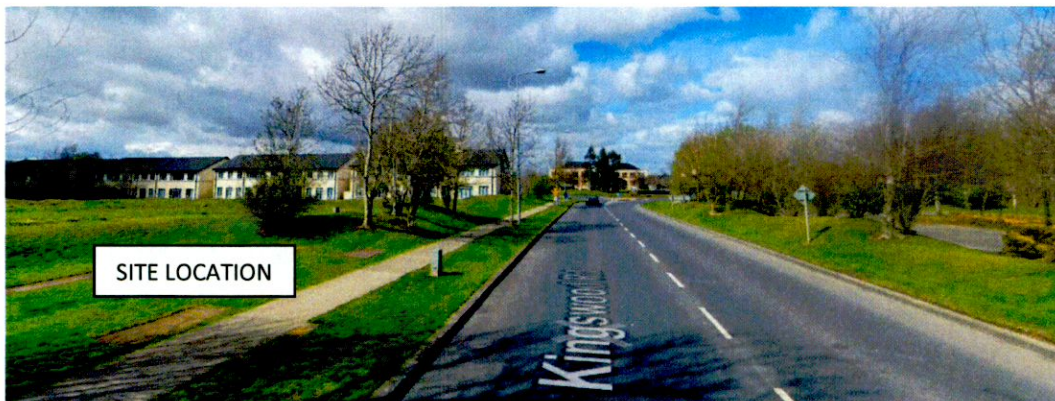
The Traffic & Transportation Consulting Engineers drawings and report that accompanies this submission prepared by Stephen Reid Consulting Engineers should be consulted regarding traffic numbers and related issues.



Kingswood Ave



Kingswood Ave & Kingswood Road.



Kingswood Road.



Kingswood Road & Kingswood Ave.

Existing estate roads, grass margins and footpaths remain in very good condition throughout the Business Park.

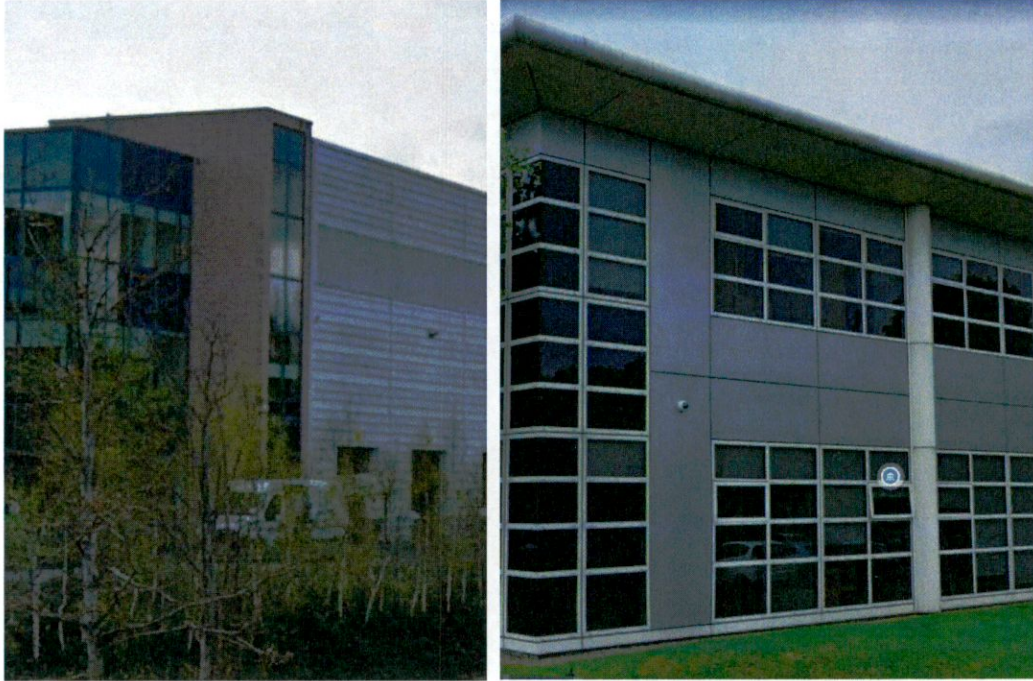
VI. Design to Complement Industrial Design Typologies in the Local Area plus proposed materials and finishes.

As highlighted earlier in this document, the overall design objective for this proposal is to provide a warehousing development that performs its function as a large storage/dispatch building with safe vehicular, pedestrian and cyclist access and egress while conveying an architectural language appropriate to the aspirations of modern warehousing business yet importantly harmonizing with its surroundings through thoughtful integration.

Therefore a limited palette of materials has been provided that will create this modern building finish and provide the required integration.

(The materials used within the proposed development will be similar composite insulated cladding panels of various colour shades, profiles and panel orientation with featured curtain wall panels to avoid large monotonous metal clad surfaces).

Examples of industrial design typologies in the local area are typically as follows:



Examples of the proposed building finishes are as follow:

We therefore note that the architectural language proposed is in our opinion in-keeping with existing adjacent buildings while perimeter earth mounds and specifically designed soft landscaping/planting will soften the overall appearance, continuing the theme of pleasantly landscaped industrial/warehousing sites within the business park.

The materials proposed are intentionally simplistic. Experience demonstrates that clear sharp lines, crisp functional detailing, a limited range of surface materials and a restricted palette of neutral colours combine most effectively to reduce the perceived

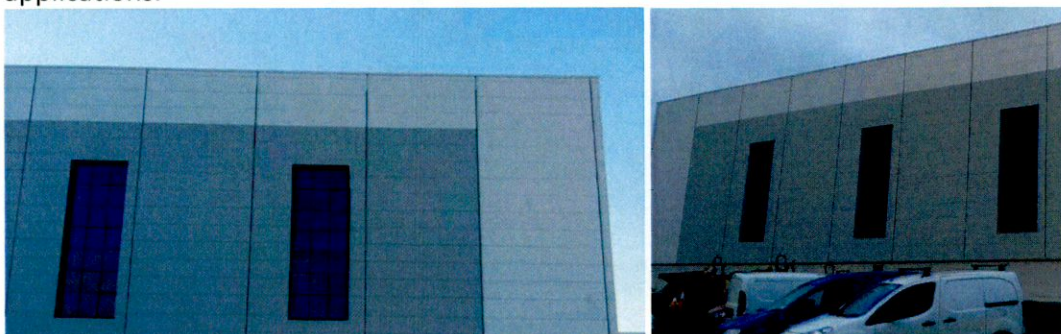
mass of these types of buildings. However, some special features were used to bring dynamism and movement to this development warehouse. The wave shape as aforementioned helps to smooth the linear parapet and a specific profiled finish was used to enhance the aesthetic quality of the building - see below illustrative image:



The range of colours is basic and unobtrusive to promote a combination of details and enhance the lines of the elevations.



Glazing panels integrated into the sleek industrial cladding material are used to break long elevations and make them visually more appealing. The appearance of the integrated ancillary office accommodation is enhanced with curtain wall panels, a green wall adjacent to the main entrance and different details and shades of cladding panel. Proposed colours and materials give smooth polyurethane coating to buildings that delivers a pleasant aesthetic and guaranteed performance in variety of applications.



Keeping creative form, shape, and image as key design factors, it is proposed to use Kingspan's insulated roof and wall system solutions. These roof and wall system solutions are proven for safe construction, structural integrity, fire safety, acoustics,

and environment sustainability, providing both cost effective and high-quality solutions.

The proposed green walls will help to soften the overall appearance coupled with the site's perimeter landscaping.



Examples of green walls proposed from other unrelated buildings.

VII. Landscape and Visual Screening Statement

1 Introduction

This landscape and visual screening statement has been prepared in respect of a proposed logistics warehouse development at the corner of Kingswood Road and Kingswood Avenue within Citywest Business Campus in South County Dublin. The development will comprise the provision of a warehouse with ancillary office and staff facilities and associated development. The warehouse will have a maximum height of 18 metres with a gross floor area of 11,691 sq m including a warehouse area (10,604 sq m), ancillary staff facilities (499 sq m) and ancillary office area (588 sq m).

The development will also include: a vehicular and pedestrian entrance to the site from Kingswood Road, a separate HGV entrance from Kingswood Avenue; 64 No. ancillary car parking spaces; covered bicycle parking; HGV parking and yards; level access goods doors; dock levellers; access gates; hard and soft landscaping; canopy; lighting; boundary treatments; ESB substation; plant; and all associated site development works above and below ground.

2 Statement of Authority

This landscape and visual screening statement has been prepared by Enviroguide Ltd. Enviroguide has 10 years' of experience in Landscape and Visual Impact Assessment of a range of commercial and infrastructure projects including numerous factories and warehouses.

3 Methodology

This LVIA screening assessment will consist of;

- Analysis of the physical land use setting and townscape character of the site an study area.
- Consideration of townscape and visual zonings and associated policies in the South Dublin County Development Plan.
- Preparation and analysis of Zone of Theoretical Visibility (ZTV) maps, which will utilise Digital Surface Model (DSM) data for the study area, which indicates potential visibility in the context of both terrain screening and screening provided by existing vegetation and buildings.
- A summary of potential effects and determination of whether further and more comprehensive Townscape and Visual Assessment is required or can be 'screened-out' of further assessment.

4 Baseline Townscape Setting

Citywest Business Campus in the southwestern outskirts of Dublin City beyond the M50 ring road. It is surrounded by a variety of different commercial land uses and residential housing estates that have enveloped hinterland farmland over the past two to three decades. Although there remains remnant areas of pasture in the northern section of the study area. Other green open spaces in the surrounds are either recreational/amenity areas of the surrounding Citywest business campus, including the Tara Pool and Lake Park to the southwest of the site, or undeveloped lots which are covered in grass and other self-sown species in the interim. The N7 Naas Road runs across the north of the study area, with an interchange with the N82 located to the west of the site and the LUAS light rail line also runs to the south of the business campus. Three of the four sides of the site are bordered by industrial warehouse buildings of a substantial scale. The fourth (north) site boundary is bordered by smaller commercial premises and buildings designed to integrate with the residential use adjacent to them. Along Kingswood Avenue to the north, is the nearby Silken Park Housing estate, which is located to the west between the Citywest industrial areas and those along the N7, such as the Corkagh Nurseries, Ballinlough Refrigeration and Store4U Self Storage, amongst others. To the east however, the N7 veers north, widening the distance covered by the residential and green spaces, to include the Clondalkin Rugby and Football Clubs, the Shamrock Rovers Football Club Academy, and Roadstone Group Sports Club (and associated parking and playfields). Further to the east, separated from the remainder of the study area by the R136, is the Belgard Quarry, which covers the majority of the western extents of the study area, it is contained to the south by the Luas line, which also defines the extent of the large area of residential housing around the periphery of the study area. The two residential areas within the study area are Brookfield and Fettercairn, located on the west and east side of the R136 respectively.

5 South County Development Plan (2016 – 2022)

The proposed site is contained within an EE zoning for 'Enterprise and Employment' within the current Development Plan and will retain their 'EE' zoning as part of the Draft Plan. The established areas to the south and east are unsurprisingly zoned the same, as expressed by the current land use. The southern extents of the study area and immediate north of the site are contained in residential and associated open space zonings. Therefore, the proposed development is in direct accordance with its zoning objective even though it fronts onto established residential zones to the north, which are more sensitive to this scale and form of development than the remainder of the Citywest Business Campus. It should be noted that the row of buildings zoned RES (to protect and/or improve residential amenity) which form the boundary with the site to the north are not typical residences, but currently small commercial premises, creating a land use buffer between the two zones and along with the surrounding industrial park, are likely to substantially contain its visual effects.

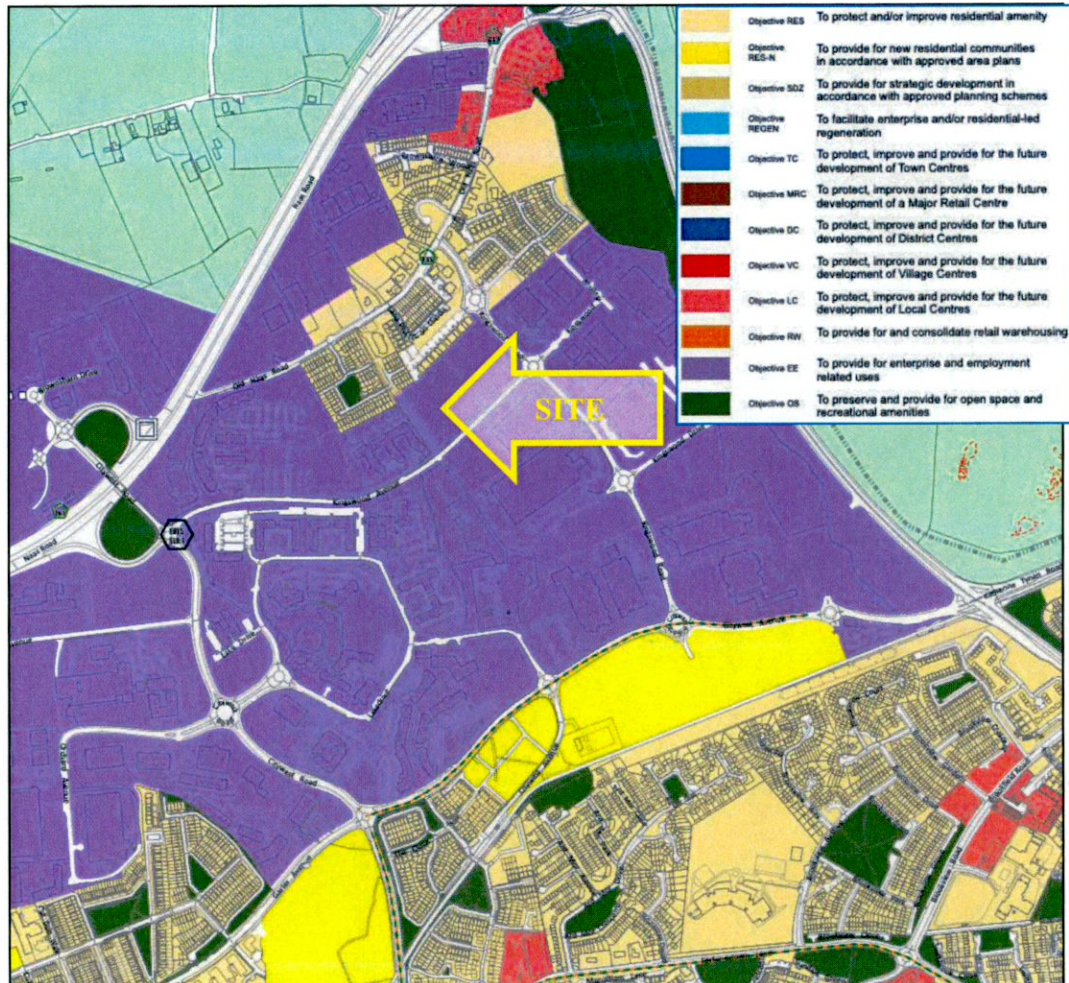


Figure 1 – Land use Zoning Context of the site – South Dublin Development Plan Map 8

6 Potential Townscape Effects

On the basis that the proposed building is contained within an established business park surrounded to the east, south, and southwest by a similar scale and nature of development, the immediate receiving landscape is not considered particularly sensitive from a townscape character perspective. There is a higher degree of sensitivity to this scale and nature of development within the low-rise mid density residential areas to the north, as well as the proposed residential areas on the periphery of the existing residential areas to the south. However, these are by no means incompatible land uses in the peri-urban context of Dublin City where the respective land use zoning objectives about each other. For this reason, it is not considered that the proposed development within its appropriate zoning could give rise to significant and adverse townscape character effects.

7 Potential Visual Effects



Figure 2 - Modified DSM-based Zone of Theoretical Visibility Map showing ground level visibility ignoring proposed vegetation.

The visibility pattern in Figure 2, indicates the area most exposed to potential views of the proposed warehouse building is the carpark associated with the Waterside complex immediately to the west of the site, with sections of the buildings between the site and the Waterside complex also having visibility, albeit less extensive in area. The most important feature between the Waterside complex and the site is the Ardsolus housing cul-de-sac, which is the western-most section of residential area to the north of the site. As shown on the above analysis, this area has consistent visibility and due to the residential usage is of higher sensitivity than the surrounding commercial and industrial use. However, the Ardsolus cul-de-sac is shown within the above analysis at pre-construction stage, with no built form. This absence of structures results in a higher degree of visibility (as shown above) than the now completed built form would have. The completed (existing) built form will screen the interior of the residential area from visibility – resulting in patterning similar to the areas of visibility

through Silken Park, which are confined to the open roadway areas. Additionally, where there are views between buildings, the site will cover a small section of the perimeter of the residential area, which is already surrounded on three sides by the existing industrial / commercial development.

There is also visibility across the adjacent roundabout to the east and surrounding the adjacent buildings, but this is within the Campus and not deemed to be sensitive to views of another new warehouse building. The existing business park buildings and vegetation preclude visibility further to the southwest and northeast and south.

The most potentially impacted area in the immediate vicinity appears to be the Silken Park and Ardsolus residential areas to the north where, even though the dwellings do not front onto the site, there will be an increased intensity and scale of built development in view and also an increased degree of visual enclosure at the end of the streets and between residences.

In the middle distance, c. 300 – 600m away from the site, there are some areas with radial shards of visibility, which indicates visibility between intervening vegetation and buildings. The largest areas of visibility tend to relate to open spaces where fewer intervening buildings occur. It is important to consider that visibility at such distances is likely to relate to a very minor change in the business park backdrop rather than gap filling or the profile of the proposed building rising prominently above the existing built skyline. This is especially the case for those views across pasture to the north and quarry to the east, which will be viewing the proposal as a small addition 'filling in' the wider built-up area.

Within the wider study area (600m+), there is a reasonable degree of visibility from farmland to the north and the N7 interchange to the north, however, this does not represent population-based receptors and is not considered to be a sensitive visual receptor area. The views from the N7 are not along the main corridor, but only the more elevated locations across the overbridges and ramp. Viewers in these specific locations will be experiencing the site fleetingly in the context of the wider landuse (including the quarry). There is one splay of visibility to the south into the residential area, however this will result in views where the site is a small inclusion to the existing built-up area.

8 Conclusion and Potential Mitigation

Although the proposed warehouse building will be visible from some areas beyond the immediate Citywest Business Campus setting, many of these farming and commercial / industrial areas are not considered to be sensitive to such views. The most sensitive areas with potential visibility are the residential estates to the north - Silken Park and Ardsolus and residential zoning for new residential development to the south. In both instances the proposed building will be seen in the same context and backdropped by the existing business park. Potential mitigation to reduce visual impacts includes; a dispersed colour scheme to break-up bulk and massing; buffering the northern and western boundaries of the site insofar as possible (possible car parking areas) to

reduce perceived scale and massing; and, screen planting / street tree planting to soften and screen the building.

In the context of the likely townscape and visual effects, it is not considered that specific mitigation measures are essential to reduce significant impacts. However, incorporation of some or all of the described mitigation measures will ensure that visual impacts will be at the lower end of the spectrum.

Overall, it is not considered that more intensive or detailed townscape and visual assessment is required for the proposed development and can be screened-out of further assessment.

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