



Planning  
Application  
Report


Development of  
436no. Dwellings

At Adamstown Station  
Development Area -  
Blocks A, C and D  
Adamstown SDZ

For Quintain  
Developments Ireland  
Limited

APRIL 2022

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## 1 INTRODUCTION

We are instructed by Quintain Developments Ireland Ltd, 6<sup>th</sup> Floor, Fitzwilliam Court, Leeson Close, Dublin 2, D02 YW24 to lodge this planning application for the development of 436no. new homes within the Adamstown Station Development Area of the Adamstown Strategic Development Zone (SDZ).

The lands which are the subject of this application are outlined in red on the enclosed Site Location Map prepared by Henry J Lyons Architects and also on the individual Site Layout Plan drawings. The other lands in the vicinity of the application site in the control of the applicant are outlined in blue on the enclosed Site Location Map. The locations where the 4no. site notices have been erected are shown on the enclosed Site Location Plan, prepared by Henry J Lyons Architects.

We confirm that prior to making this planning application, extensive pre-planning discussions were entered into with various Departments of South Dublin County Council, as Planning Authority.

### 1.1 Compliance with Planning & Development Act 2000, as amended

We confirm that prior to making this planning application the drawings enclosed herewith have been screened by this office with reference to the Planning & Development Act 2000 (as amended) and are consistent with the spirit and intent of the same.

It should be noted, at the outset, that Adamstown is designated as a Strategic Development Zone in accordance with a Government Order on 1<sup>st</sup> July 2001. This Order was made in accordance with Part IX of the Planning & Development Act 2000 (Statutory Instrument no 272 of 2001 refers). South Dublin County Council is recognised as the named 'Development Agency' under this Order and have prepared a Planning Scheme for the lands in this case.

Section 170 of the Planning & Development Act 2000 sets out the procedure for planning applications within a Strategic Development Zone where they are essentially the same as any other application for planning permission for development. However, three Sections of the Act should be kept in mind, these are quoted as follows: -

*"(2) A planning authority shall grant permission in respect of an application for a development in a strategic development zone where it is satisfied that the development, where carried out in accordance with the application or subject to any conditions which the planning authority may attach to a permission, would be consistent with any planning scheme in force for the land in question, and no permission shall be granted for any development which would not be consistent with such a planning scheme,*

*(3) Notwithstanding Section 37, no appeal shall lie to the Board against a decision of a planning authority on an application for permission in respect of a development in a strategic development zone*

*(4) Where a planning authority decides to grant permission for a development in a strategic development zone, the grant shall be deemed to be given on the date of the decision."*

Considering this information, where an application is submitted to the Planning Authority, it shall be granted planning permission, so long as it complies with the Planning Regulations and the appropriate Planning Scheme. In addition, there is no right to appeal the Council's decision to An Bord Pleanála.

This application for development is made subject to the above provisions. Compliance with both the Planning and Development Regulations 2001, as amended, and the Adamstown Planning Scheme 2014, as amended (hereafter referred to as the Adamstown Planning Scheme) is demonstrated in this report. A confirmation of the EFT payment already made for the appropriate planning fee is also included with this application.

3no. letters of consent accompany this application. These have been provided by Adamstown Infrastructure Designated Activity Company, Adamstown Station 1 Limited and Adamstown Station & Boulevard Limited.

## 1.2 Site Description

The development lands are located within the Adamstown Strategic Development Zone (SDZ) and comprise a third phase of development at Adamstown Station (Development Area 11). The site is generally bounded by Adamstown Avenue to the north, to the east by undeveloped lands which benefit from planning permission Reg. Ref. SDZ20A/0008 (as amended by SDZ20A/0016, SDZ20A/0018 and SDZ21A/0017), to the south by Station Road, the railway line and train station and to the west by the railway overpass and its approach road, known locally as ‘the farmer’s bridge’.

The overall Adamstown Station Development Area is the key district centre in the Adamstown SDZ Planning Scheme and is bounded by primary roads, including Station Road Adamstown Park, Adamstown Avenue and Adamstown Boulevard, which are complete.

The proposed development comprises the construction of 3no. apartment Blocks (Blocks A, C & D) on a site of c. 2.43 ha (outlined in red in Figure 1). The development consists of 436 no. residential units, with 9no. studio units, 204no. 1-bedroom units, 213no. 2-bedroom units and 10no. 3-bedroom units, along with an internal resident’s amenity area and public and communal open space.

## 1.3 Summary Description of Development

The proposal in this case comprises of:

- 436 no. apartments (c. 37,402 sq.m GFA including car parking and storage) in a mixture 3 bed units (10no.), 2 bed units (213no.), 1 bed units (204no.) and studio units (9no.).
- Block A ranges from 4 storeys to 5 storeys in height.
- Block C ranges from 5 storeys to 9 storeys in height.
- Block D ranges from 2 storeys to 5 storeys in height.
- Ancillary residents Pavilion Amenity Building (c. 480 sq.m GFA)
- Communal open space provided at podium and ground levels (3,281 sq.m).
- Public open space (c .3.138 sq.m).
- The provision of 220no. car parking spaces in a mixture of on-street parking, beneath podium and within the Block F multi-storey car park.
- The provision of 526no. bicycle parking spaces beneath podium level and at street level.
- All ancillary and associated site development and landscape works.

A full description of development is included in the Site and Newspaper Notices as well as Section 5 of this Report.

## 2 SITE CONTEXT

### 2.1 Adamstown

Adamstown is located approximately 16 kilometres to the west of Dublin City Centre, and 1.1 kilometres south of Lucan village. The site is situated on lands that have been designated as a Strategic Development Zone (SDZ), on lands known as Gollierstown and Adamstown, Lucan, Co. Dublin. The lands are bound by the existing 'Alderlie' development and Adamstown Avenue to the north, to the west by underdeveloped lands, to the south by Station Road, the railway line and train station and to the east by Adamstown Park and Adamstown Community Centre.

'Adamstown Station – Development Area 11', is situated to the southern side of the SDZ. Adamstown Station is bounded by Adamstown Boulevard, Adamstown Square, Adamstown Castle and Adamstown train station.'

The Adamstown Planning Scheme promotes the Adamstown Station Development Area as a high development density character area.

The following infrastructure has already been delivered within the Adamstown SDZ:

- Adamstown Link Road – linking Adamstown with wider external strategic road network
- Adamstown Rail Station & Temporary Park & Ride
- 2 no. Primary Schools
- A Post-Primary School
- A crèche (Giraffe)
- Local Shops
- Neighbourhood Park and Playground
- Trunk Mains Water Supply, Foul and Surface Water drainage including Tobermaclugg Pumping Station
- New ESB 110kV station and full range of utilities
- Adamstown Community Centre
- Adamstown All-Weather Pitch
- Adamstown Park (Loop Road #1)
- Shackleton Drive (Loop Road #2)
- Tandy's Lane Park (complete and open to the public)
- Tobermaclugg Village Centre, anchored by a Lidl

Permission has also been granted for the following strategic elements of development at Adamstown:

- Adamstown Boulevard (including QBC) (substantially complete and will be open in summer 2022)
- Celbridge Link Road (under construction for completion in Q3 2022)
- Airlie Park (under construction and is due for completion in Q3 2022)
- A third Primary School at Tandy's Lane Village

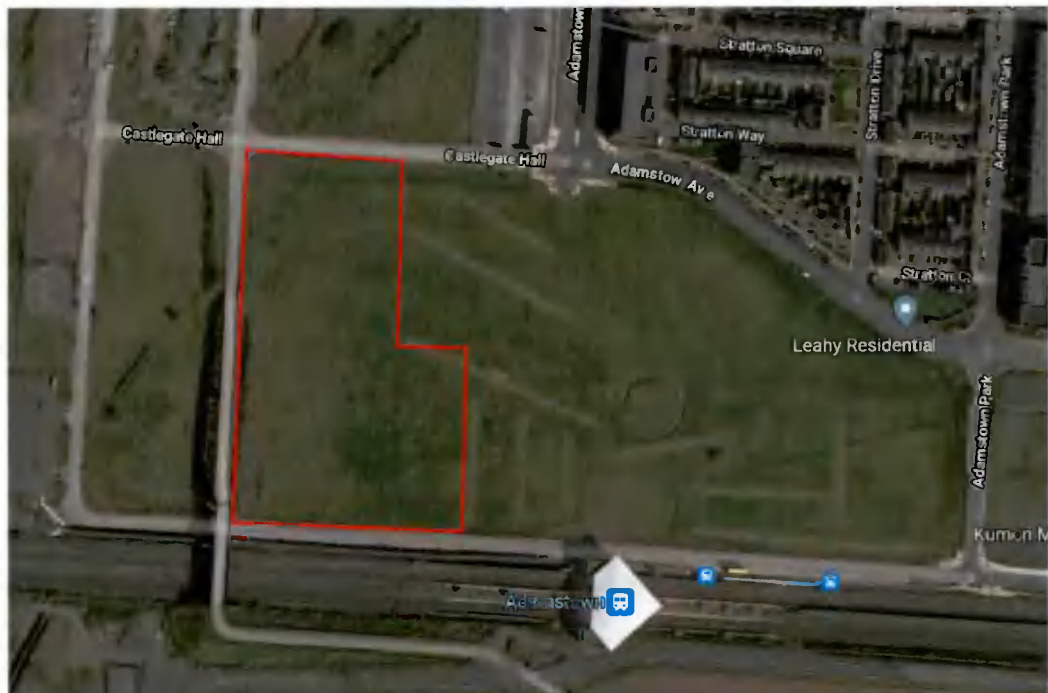


The provision of Tandy's Lane Park, Airlie Park and the Celbridge Link Road rest with South Dublin County Council to construct under the LIHAF arrangements.

## 2.2 Application Site

The Adamstown Station development area (Development Area 11) extends to 8.6 hectares and is located to the south of the Adamstown SDZ.

The site is generally bounded by Adamstown Avenue to the north, to the east by undeveloped lands which benefit from planning permission Reg. Ref. SDZ20A/0008 (as amended by SDZ20A/0016, SDZ20A/0018 and SDZ21A/0017), to the south by Station Road, the railway line and train station and to the west by the railway overpass and its approach road, known locally as 'the farmer's bridge'.



**Figure 1:** Subject site at Adamstown Station, Adamstown, Co. Lucan, with approximate site boundary this application outlined in red. Please refer to the enclosed Site Location Map prepared by Henry J Lyons Architects for definitive red line boundary.



**Figure 2:** Aerial Photograph of the Adamstown Station tile and its immediate environs on 4<sup>th</sup> February 2022 showing progress of the construction of Blocks B, E and F (Phase 1 at The Crossings). Outline of application site shown approximately in red. (source: Barrow Coakley Photo & Video)

### 3 PLANNING HISTORY

There are a number of planning applications of particular note in relation to the application site and its immediate vicinity which hold particular interest in relation to this development proposal.

#### 3.1 Application Site

##### 3.1.1 SDZ07A/0003 (Adamstown District Centre Enabling Works)

The Council granted permission on 5<sup>th</sup> June 2007 for permission for a package of enabling works to facilitate the construction of the Adamstown District Centre. The proposed works included the construction of a basement structure together with podium slab. This permission was not acted upon and has since withered.

##### 3.1.2 SDZ08A/0002 (Adamstown District Centre Phase 1)

The Council granted permission on 18 July 2008 for the construction of Phase 1 of the Adamstown District Centre comprising large scale mixed use commercial and residential development (c. 109,044m<sup>2</sup>) to be constructed in buildings ranging in height from 2 -10 storeys. This permission was not acted upon and has since withered.

#### 3.2 Adjacent Developments

##### 3.2.1 SDZ20A/0008 (Adamstown District Centre Phase 1)

Quintain Developments Ireland Limited applied for permission on 28 April 2020 for a new Phase 1 of the Adamstown District Centre development, comprising a mixed use commercial and residential development of c.36, 621sq.m (gross floor area) in total (excluding the multi-storey car park) to be constructed in buildings ranging in height from 4-9 storeys. The non-residential element of the development consists of a total of c.9, 653sq.m (net sales floor space), as follows: 16 retail units comprising c.8, 693sq.m including 1 supermarket (c.6, 880sq.m in total) and 2 retail service units (c.290sq.m); 5 retail/restaurant/café units comprising a total of c.959sq.m; projecting signage and awnings on retail and retail/café/restaurant units. The residential element consists of a total of 278 residential units. Permission was granted on 17 September 2020.

##### 3.2.2 SDZ20A/0016 (Revisions to Blocks B and E of SDZ20A/0008)

Quintain Developments Ireland Limited applied for permission on 6 October 2020 for minor amendments to the development permitted under SDCC Reg. Ref. SDZ20A/0008. The amendments included repositioning of landscaped courtyards of Blocks B and E, re-arrangement of car parking, minor adjustments to the footprint of the permitted retail and retail/ café/ restaurant units, and one additional apartment. Permission was granted on 30 November 2020.

##### 3.2.3 SDZ20A/0018 (Revisions to Block F of SDZ20A/0008)

Quintain Developments Ireland Limited applied for permission on 11 December 2020 for amendments to the development permitted under Planning Permission Reg. Ref. SDZ20A/0008, as amended by SDZ20A/0016 which comprised of adjustments to the configuration of the footprint of Block F. This also included removal of a level of the multi-storey car park, revised façade treatment, reconfiguration of the ground floor layout to provide 2 supermarket units, relocation of entrance to the multi storey car park and access and egress to ground floor internal vehicular goods delivery area. Permission was granted on 15 February 2021.

**3.2.4 SDZ21A/0007 (Adamstown District Centre Phase 2: Block G)**

Quintain Developments Ireland applied for permission on 21 April 2021 for Phase 2 of Adamstown District Centre (known as 'Block G'). This comprised of 185no. apartments in 2no. buildings, ranging in height from 4 to 9 storeys. 93no. car parking spaces were also proposed. Permission was granted on 13 September 2021.

**3.2.5 SDZ21A/0020 (Block G Amendment)**

A planning application for revisions to SDZ21A/0007 was lodged with South Dublin County Council on 21 October 2021. The amendments include repositioning of landscaped courtyards of Block G, re-arrangement of car parking, modification of 174no. apartments, minor adjustments to the footprint of the permitted Block G2, the reduction in height to the northern half of Block G2, and adjustments to the location of the bicycle, plant, and waste stores serving Block G, including the provision of bicycle storage structures in the courtyard. Permission was granted on 07 March 2022.

**3.2.6 SDZ21A/0017 (Block F Amendment –Extra Floor)**

A planning application for revisions to SDZ20A/0008 (as amended by SDZ20A/0016 and SDZ20A/0018) was lodged with South Dublin County Council on 10 September 2021. The proposed revision includes the provision of an additional level of car parking to be inserted into the recently permitted multi-story car park (Block F) under Reg. Ref. SDZ20A/0018. With these additions, Block F will be 4 storeys in height, with an additional setback plantroom on the roof.

Car parking is provided at first, second and third floors. A total of 673 car parking spaces to now be provided within Block F, with 230 spaces allocated to residential use and 443 spaces associated with the supermarket units and other non-residential uses in the district centre.

Permission was granted on 4 November 2021.

**3.2.7 SDZ08A/0001 (Sports & Leisure Building)**

The Council granted permission on 14 March 2008 for construction of a Community Centre and All-Weather pitch. This application was not acted upon and has since withered.

**3.2.8 SDZ10A/0001 – SDZ10A/0001/EP (Adamstown Square)**

This application comprised 177no. residential dwellings (150no. houses and 27no. apartments). Permission was first granted on 15 February 2011, and an extension of duration of the permission was granted on 20 March 2013 for a period of 5 years. This development is known as Alderlie and is located immediately to the south of Adamstown Way.

**3.3 Infrastructure in Adamstown**

The proposed development will benefit from close proximity to a wide range of amenities, including: - primary and secondary schools, a community building, local parks, the train station and a Quality Bus Corridor. The following table is a record of all grants of permission for Adamstown that involve infrastructure to benefit development in the SDZ.

Reg Ref	Description of Infrastructure	Date of Grant
SDZ04A/2	Adamstown Station	25-Feb-2005
SD04A/0964	Adamstown Link Road and R120 Upgrades	29-Jul-2005
SDZ05A/3	Adamstown Castle Park	13-Dec-2005

SDZ06A/1	Adamstown Castle Bus Shelters	10-Mar-2006
SDZ06A/2	Adamstown Castle Primary Schools	2-Jun-2006
SDZ06A/4	Tobermaclugg Pumping Station	18-Jul-2006
SD06A/0497	ESB Substation Western SDZ Boundary	4-Aug-2006
SDZ06A/5	Adamstown SDZ Strategic Roads	15-Aug-2006
SDZ06A/8	Temporary Park and Ride Relocation	22-Sep-2006
SDZ06A/9	Crowley Overbridge	17-Oct-2006
SDZ06A/11	District Centre Strategic Roads	22-Nov-2006
SDZ07A/0002	Adamstown Post Primary School	12-Dec-2007
SDZ07A/0003	District Centre Enabling Works	5-Jun-2007
SDZ10A/0335	Adamstown Primary School No.2	5-Jun-2007
SDZ11A/0001	Adamstown Primary School (Temporary)	28-Jul-2011
SDZ14A/0001	Adamstown Secondary School (Retention)	4-Jul-2014
SDZ16A/0001	Community Building with Sports Hall	25-Nov-2016
SDZ16A/0003	Northern part of Loop Road 2	23-Jan-2017
SDZ17A/0004	All Weather Pitch	18-Jul-2017
SDZ17A/0007	Loop Road 1	05-Sep-2017
SDZ17A/0009	Celbridge Link Road	26-Feb-2018
SDZ18A/0002	Tobermaclugg 2 (including Northern section of Adamstown Boulevard)	15-Mar-2018
SDZ18A/0003	Tandy's Lane Park	4-Apr-2018
SDZ18A/0009	Adamstown Boulevard Road (southern section) and Realigned Tandy's Lane	25-Sept-2018
SDZ18A/0014	Airlie Park	12-Feb-2019
SDZ18A/0015	Tobermaclugg Village Centre	25-Feb-2019
SDZ20A/0005	Tobermaclugg Park	17-Apr-2020
SDZ20A/0008	Adamstown District Centre Phase 1	17-Sept-2020
SDZ21A/0001	Adamstown Primary School at Tandy's Lane	8-June-2021
SDZ21A/0007	Adamstown District Centre Phase 2	13-September-2021

**Table 1: Infrastructure Permissions in Adamstown**

### 3.3.1 SDZ06A/5 (Roads Infrastructure)

The Council granted permission on 15 August 2006 for approximately 2,780 metres of roadway throughout the Adamstown SDZ area. These consisted of major avenue roads, including Adamstown Way.

### 3.3.2 SDZ16A/0001 (Community Building with Sports Hall)

The Council granted permission on 25 November 2016 to Adamstown Infrastructure DAC for a Community Building with Sports Hall on Station Road, Adamstown, Lucan, Co. Dublin. The delivery of a community centre is a key requirement for Phase 2 of the Adamstown Planning Scheme. The proposed development fulfils the requirements in terms of size and function for the community centre.

### 3.3.3 SDZ16A/0003 (Northern part of Loop Road 2)

The Council granted permission on 23 January 2017 for a development which consisted of 267 no. dwelling units and the construction of part of the main road and busway network for the Adamstown Planning Scheme. The proposed main road will make provision for vehicular access to serve the

residential development, online bus services, car parking as required, pedestrian footpaths and cycle paths.

#### **3.3.4 SDZ17A/0004 (All-Weather Pitch)**

An application was submitted for the development of an all-weather pitch on the 30 May 2017. The proposed pitch is to be located on a site measuring 0.56ha and is to be located adjacent to the above permitted Community Centre. The pitch is to be accessed via separate entry and exit point onto Station Road. South Dublin County Council granted permission for this development on 18 July 2017. Construction works are complete, and the facility has been transferred to SDCC.

#### **3.3.5 SDZ17A/0007 (Loop Road 1/Adamstown Park)**

An application was submitted on 13 July 2017 for the development of Loop Road 1, and a number of additional junctions to the south. The proposed road also included a realignment of an existing section of Tandy's Lane. South Dublin County Council granted permission for this development on 5 September 2017. Construction works are complete, and the road (Adamstown Park) is now open to traffic.

#### **3.3.6 SDZ17A/0009 (Celbridge Link Road)**

An application was submitted on 22 December 2017 by Adamstown Infrastructure DAC for a section of the Celbridge Link Road. The proposed development contained 1220m of 7m carriageway with parallel parking bays, planting/grass verges, buffer/lighting strips, off-road cycle tracks and footpaths to link a section of the proposed Celbridge Link Road (subject of a Part 8 application) to the proposed Adamstown Drive and Adamstown Way, 13 uncontrolled junctions for future access to the Adamstown SDZ development areas. The proposed development included all associated infrastructure works and 178no. car parking spaces. South Dublin County Council granted permission on 26 February 2018.

#### **3.3.7 SDZ18A/0003 (Tandy's Lane Park)**

An application was submitted on 8 February 2018 by Adamstown Infrastructure DAC for the development of Tandy's Lane Park on a site of 7.7 Ha., in Tandy's Lane Park amenity area. South Dublin County Council granted permission for this development on 4 April 2018.

#### **3.3.8 SDZ18A/0009 (Adamstown Boulevard Road and Realigned Tandy's Lane)**

Adamstown Infrastructure DAC applied for permission on 3 August 2018 for c.480m of road known as the Adamstown Boulevard connecting the permitted section of the Boulevard to the north (Reg. Ref. SDZ18A/0002) to its junction at Adamstown Way. The relocation and realignment of 280m of Tandy's Lane was also proposed within the application. Permission for this road was granted on the 25 September 2018.

#### **3.3.9 SDZ18A/0014 (Airlie Park)**

Adamstown Infrastructure DAC applied for permission on 10 December 2018 for development of Airlie Park on a site of c.10.95Ha. Permission was granted on the 12 February 2019.

#### **3.3.10 SDZ18A/0015 (Tobermaclugg Village Centre)**

Cairn Homes Properties applied for permission on 21 December 2018 for a supermarket with ancillary off-licence sales area comprising 3,619.7sq.m GFA (of which 1,693sq.m net retail sales area) with

ground floor entrance and circulation area providing access to first floor supermarket and ancillary areas and 1 ground floor retail unit/coffee shop (134sq.m GFA). Permission was granted on 25 February 2019.

There has been a subsequent permission secured to make minor amendments to this development. Works have commenced and construction is on-going.

### **3.3.11 SDZ20A/0005 Tobermaclugg Park**

Cairn Homes Properties Limited, Tierra Limited and Hugh McGreevy and Sons Limited applied for permission on 10 February 2020 for the provision of a new public park, called 'Tobermaclugg Park' of approximately 3.4 Ha comprising of site development and landscape works. Permission was granted on 17 April 2020.

### **3.3.12 SDZ21A/0001 Adamstown Primary School**

Quintain Developments Ireland Limited applied for permission on 12 January 2021 for the construction of a predominantly part two storey, part three storey primary school comprising 32 classrooms, within the Tandy's Lane Development Area. Permission was granted on 8 June 2021.

### **3.3.13 SDZ21A/0007 (Adamstown District Center Phase 2)**

Quintain Developments Ireland applied for permission on 21 April 2021 for Phase 2 of Adamstown District Centre (known as 'Block G'). This comprised of 185no. apartments in 2no. buildings, ranging in height from 4 to 9 storeys. 93no. car parking spaces were also proposed. Permission was granted on 13 September 2021.

### 3.4 Record of Residential Development in Adamstown

The below table outlines a list of residential development throughout Adamstown.

Reg. Ref.	Applicant	Description	Date of Grant	Status	No. of Units	Cumulative Total Units
SDZ10A/0001/EP	Castlethorn	Square III	15-Feb-11	Complete	177	1,587
SDZ16A/0003	Cairn Homes Ltd.	Tobermaclugg I	23-Jan-17	Complete	267	1,854
SDZ16A/0005	Castlethorn Developments (Adamstown), UC	Somerton I	26-Apr-17	Complete	246	2,100
SDZ17A/0002	Castlethorn Developments (Adamstown), UC and Adamstown Developments DAC	St. Helen's I	19-Jun-17	Complete	135	2,235
SDZ17A/0003	Margaret Doyle	Dodsboro Road	06-Jul-17	Permission Granted	1	2,236
SDZ17A/0006	Hugh McGreevy & Sons and Tierra Ltd.	Tubber Lane I	22-Aug-17	Under Construction	169	2,405
SDZ18A/0002	Cairn Homes Ltd.	Tobermaclugg II	15-Mar-18	Complete	268	2,673
SDZ18A/0004	Cairn Homes Ltd.	Airlie Stud	22-May-18	Complete	237	2,910
SDZ18A/0011	DRES Properties (Holdings) plc	St Helen's II	19-Dec-18	Under Construction	343	3,253
SDZ19A/0004	DRES Properties	Somerton II	15-Oct-19	Permission Granted	237	3,490
SDZ19A/0003	Cairn Homes Ltd	Tobermaclugg III	17-Jul-19	Complete	135	3,625
SDZ19A/0008	Hugh McGreevy & Sons and Tierra Ltd.	Tubber Lane II	06-Feb-20	Permission Granted	148	3,773
SDZ19A/0011	Quintain Developments Ireland Limited	Tandy's Lane Village	22-Jun-20	Permission Granted	245	4,018



SDZ20A/0008	Quintain Developments Ireland Limited	Adamstown Station Blocks B, E & F	17-Sep-20	Permission Granted	266	4,284
SDZ20A/0011	Cairn Homes	Tobermaclugg IV	20-Jul-20	Permission Granted	40	4,324
SDZ20A/0017	Quintain Developments Ireland	Aderrig Phase 1	02-Feb-21	Permission Granted	235	4,559
SDZ21A/0007	Quintain Developments Ireland	Adamstown Station Block G	13-Sept-21	Permission Granted	185	4,774
SDZ20A/0020	Timothy & Henry Crowley	St Helens	12-Aug-21	Permission Granted	113	4,857

Table 2: Sequence of Residential Development in Adamstown

As of 31 December 2021, there were 3,115 occupations in Adamstown.

#### 4 PRE-PLANNING CONSULTATION

Pre-Planning meetings regarding the proposed development were held with South Dublin County Council on 24 March 2021, 19 May 2021, 23 July 2021, 05 October 2021 and 17 February 2022. The following representatives from South Dublin County Council were present for one or all of the meetings: Eoin Burke, Aws Taki, Tracey McGibbon, Therese Pender, John Joe Hegarty, Graham Murphy, Fionnuala Colins, Yasir Khan, Ronan Toft, Ronan O'Brien, Colm Harte, Sarah Watson, Padraig Collins, and Laurence Colleran.

Overall, the proposed development was positively received from the outset. Following discussions with SDCC, the development proposals were refined and improved, in order to ensure a high standard of design and residential amenity. The application pack was subsequently finalised to incorporate the Planning Authority's key comments.

#### 5 DESCRIPTION OF PROPOSED DEVELOPMENT

The proposed development comprises Phase Three of the Adamstown District Centre and consists of 37,402 sq. m (gross floor area including car parking and storage) of residential development to be constructed in 3no. blocks ranging in height from 2 to 9 storeys. There is a total of 436no. apartments, comprising 9no. studio apartments, 204no. 1-bedroom apartments, 213no. 2-bedroom apartments and 10no. 3-bedroom apartment. An ancillary residents Pavilion Amenity Building (2 storey) is linked to Block D, ancillary resident's amenity rooms and facilities are also provided at ground floor level of Block A. All apartments are provided with private open space in the form of balconies or gardens. The proposed block description is as follows:

**Block A** (c. 11,821sq. m gross floor area): 4 - 5 storeys in height, with a total of 143no. apartments (70no. 1-bedroom apartments, 67no. 2-bedroom 4 person apartments and 6no. 3-bedroom apartments). A resident's only podium level, courtyard is provided within Block A. Ancillary plant, storage, waste, and internal bicycle parking rooms provided at ground floor level.

**Block C** (c. 9,680sq. m gross floor area): 4 - 9 storeys in height, with a total of 124no. apartments (60no. 1-bedroom apartments, 5no. 2-bedroom 3person apartments 55no. 2-bedroom 4person apartments and 4no. 3-bedroom apartments). Communal open space is provided within 2no. parcels of land to the east of Block C between Block C and Block D. Ancillary plant, waste and internal bicycle parking rooms provided at ground floor level.

**Block D** (c.12,757sq. m gross floor area): 2 - 5 storeys in height, together with linked ancillary resident's pavilion building (2 storey). Block D comprises a total of 169no. apartments (9no. studio apartments, 74no. 1-bedroom apartments, 11no. 2-bedroom 3person apartments, and 75no. 2-bedroom 4person apartments). Communal open space is provided within 2no. parcels of land within Block D. Ancillary plant, amenity rooms and internal bicycle parking rooms provided at ground floor level. Ancillary plant, waste and internal bicycle parking rooms provided at ground floor level.

The development provides a total of c. 3,138 sq. m landscaped public open space. A total of c. 3,281 sq. m resident's communal open space is provided at ground floor level and at first floor level on a podium above the car parking area. A total of 220no. car parking spaces are provided for this development, with 60no. at street level, 88no. beneath the podium and a further 72no. within the Block F multi-storey car park. 526no. bicycle parking spaces are provided, including 416no. covered, stacked bicycle parking spaces and 110no. Sheffield Stands in the public realm. Photovoltaic panels are provided on the roof of Blocks A, C and D, as well as lift over runs and plant at roof level. The development also includes the provision of ancillary site development, boundary treatments and landscape works.

## 5.1 Residential Development

### 5.1.1 Apartment Types

Figure 3 below sets out the proposed unit mix:

UNIT MIX					
	Blk A	Blk C	Blk D	Total	%
Studio	-	-	9	9	2%
1B	70	60	74	204	47%
2B (3P)	-	5	11	16	4%
2B (4P)	67	55	75	197	45%
3B	6	4	-	10	2%
<b>Total</b>	<b>143</b>	<b>124</b>	<b>169</b>	<b>436</b>	

**Figure 3:** Extract from Schedule of Accommodation, prepared by Henry J Lyons Architects showing breakdown of proposed apartment units.

As can be seen from the drawings accompanying this planning application there are a variety of different apartment sizes and typologies within this development, including 71no. own door units. This assists in ensuring a varied choice of apartment type and size throughout the scheme.

The proposed apartment units will be accommodated in 3no. Blocks (Block A, Block C and Block D). The apartment units are not divided into clear 'types' as with the house units. The apartments are provided in a range of studio units, 1-, 2-, and 3-bedroom units. The studio units' range in size from 39 - 39.8 m<sup>2</sup>; 1 bedroom units' range in size from 48 - 54.3 m<sup>2</sup>; the 2-bedroom, 3 person units' range in size from 65.7 - 71.9 m<sup>2</sup>; the 2-bedroom, 4 person units' range in size from 73.2 - 85.2 m<sup>2</sup> and the 3 bedroom unit range in size from 91.6 - 96.6 m<sup>2</sup>

The objective of the design for the residential development is the creation of a distinct neighbourhood where each dwelling has a high level of amenity and sense of place within the overall development.

We refer the Planning Authority to the enclosed Schedule of Accommodation and Housing Quality Assessment, prepared by Henry J Lyons Architects, which confirms that all proposed apartment units comply with the 2020 Apartment Guidelines.

## 5.2 Architect's Design Statement

An Architect's Design Statement has been prepared by Henry J Lyons Architects and provides a detailed site analysis and urban design rationale. This document is enclosed with this Planning Application. The enclosed Site Layout Plan also prepared Henry J Lyons Architects should be read in conjunction with the Design Statement. The proposed development layout has been informed by consideration of the surrounding context, as evidenced by the factors outlined below.

### 5.2.1 Layout

The layout of Blocks A, C and D have been informed by the overall masterplan for the Adamstown Station Development Area, which was developed in consultation with the Planning Authority, and submitted as part of planning application Reg Ref SDZ20A/0008. A south facing public pocket park is provided along Great Western Way to the south of Block A.

Similarly, a footway is provided to the west of Block A and Block C to provide a buffer from the neighbouring cattle bridge. A communal open space is provided at podium level within Block A and at ground floor level of Block C and D. Each benefiting from excellent levels of sunlight and providing high quality amenity for the future residents.

Car parking is provided in a mixture of on-street parking, beneath podium at Block A and within the Block F multi-storey car park.

We refer the Planning Authority to Section 2 of the enclosed Design Statement prepared by Henry J Lyons for further information.

### 5.2.2 Connectivity, Inclusiveness & Permeability

An integrated design approach has been employed for the design of the internal street layout across the Adamstown Station masterplan area. This has resulted in a highly permeable, connected street layout that provides a safe, convenient, comfortable, and place-based environment for all users. The layout is based on a clear hierarchy of streets as set out in the Planning Scheme and the specific street and junction layouts as set out in the Adamstown Street Design Guide and reflected in the Design Manual for Urban Roads and Streets.

The Block A, C & D proposal sits within this overall context and provide for excellent permeability and connectivity through the site. Diagrams showing pedestrian, and cycle movement are provided within the Outline Residential Mobility Management Plan prepared by Atkins Consulting Engineers. These are replicated below:

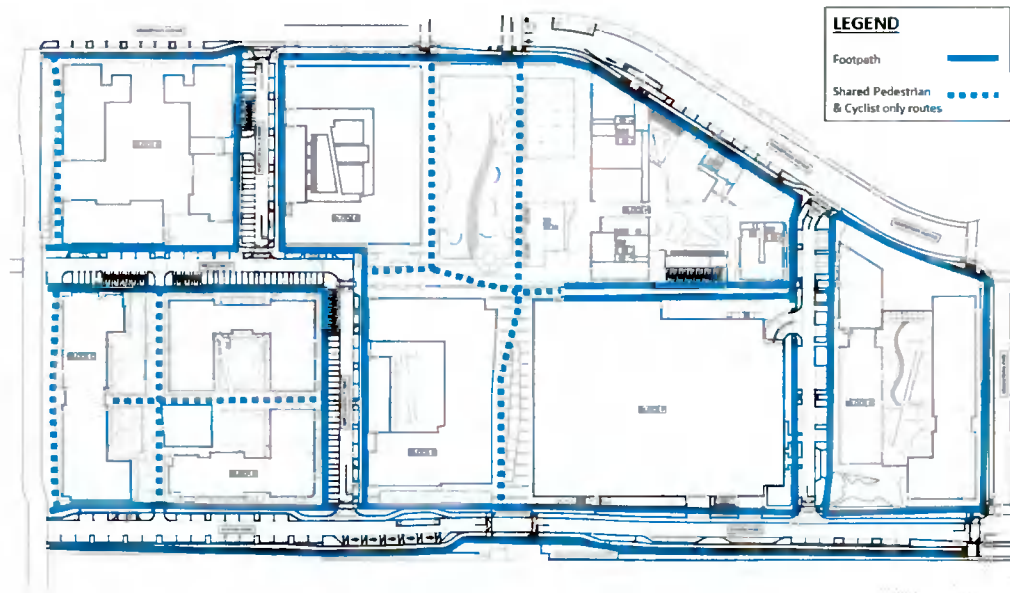


Figure 4: Extract from the Outline Residential Mobility Management Plan, prepared by Atkins Consulting Engineers showing Pedestrian Movement within the proposed development.

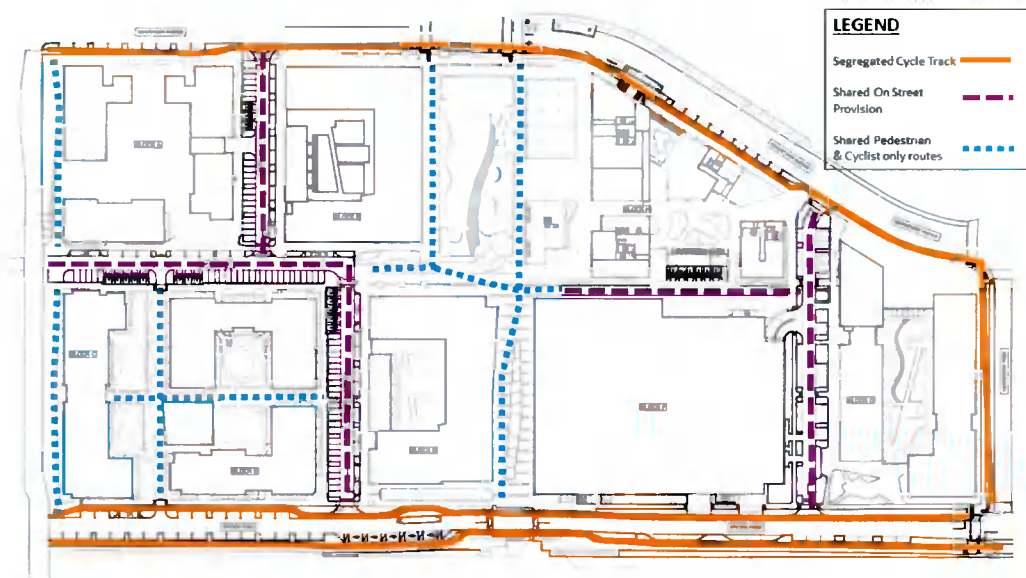


Figure 5: Extract from the Outline Residential Mobility Management Plan, prepared by Atkins consulting Engineers showing Bike Movement within the proposed development.

A Universal Access Statement has also been prepared by O’ Herlihy Access Company and is enclosed as part of this application.

### 5.2.3 Roads Hierarchy and Design

The site is bounded by Avenues to the north, a back street to the East, Station Road to the south, and by the railway overpass and its approach road, known locally as ‘the farmer’s bridge’ to the west as highlighted in the diagram below.

The road hierarchy follows the principles set out in the Planning Scheme.

We confirm further that Atkins Consulting Engineers have assessed the sight lines of the road junctions proposed in this case. Further details of this can be found in the enclosed roads drawings, Road Engineering Report and Road Safety Audit prepared by Atkins, Consulting Engineers.

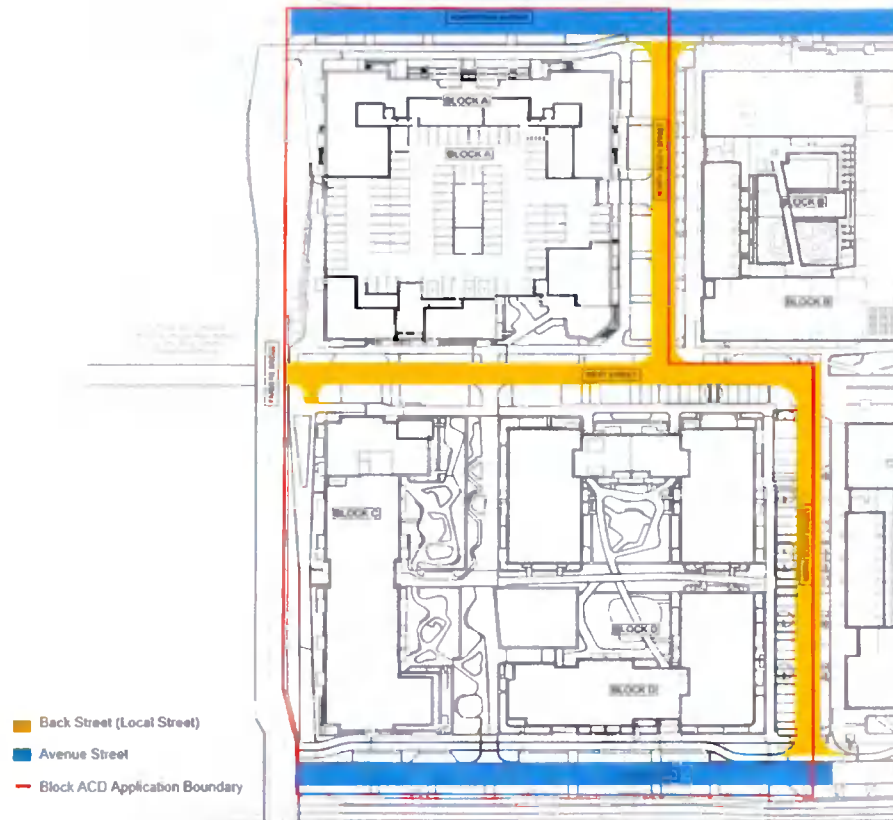


Figure 6: Extract from Roads Design Report, prepared by Atkins Consulting Engineers illustrating the roads hierarchy within the proposed development.

It is worth noting that the North South Street shown on the above Figures, positioned between the proposed Blocks A and D and permitted Blocks B and E (now under construction) was permitted under SDZ18A/0008 as a Back Street/Local Street. The Site Layout Plans prepared by Henry J Lyons Architects have sought to identify that for the benefit of the Planning Authority by the inclusion of a green line depicting the outer edge of that development permitted under SDZ18A/0008 as it relates to the application site in this instance. The parking proposals along this Back Street arising from this development have resulted in the position of this Back Street on Plan by a few metres, but the design of same remains as already permitted.

#### 5.2.4 Car Parking

The proposed development will be served by 220no. car parking spaces. 60no. of these are provided on-street, immediately adjacent to the proposed Blocks A, C and D. 8no. of these spaces are universally accessible. A further 88no. spaces are provided beneath a podium at Block A. The remaining 72no. car parking spaces are provided within the Block F multi-story car park that is already permitted and under construction.

We refer the Planning Authority to Section 8.8.3 of this report, which confirms that the proposed car parking provision complies with the Planning Scheme.

Table 6-7 - Adamstown District Centre Block ACD Residential Car Parking Allocation

Parking Type	Adamstown District Centre Block ACD
On street parking	58
On Street parking (visitor)	2
Podium parking Block A	88
Block F 3 <sup>rd</sup> Floor MSCP	72
<b>Total Allocation</b>	<b>220</b>

Figure 7: Extract from Schedule, prepared by Atkins Consulting Engineers showing breakdown of car parking.

Atkins Consulting Engineers have prepared a parking drawing that clearly identifies the arrangement of car parking spaces per Block, including those for Block B and E and also shows where in Block F the 72 no. car parking spaces in Block F are located.

### 5.2.5 Public OpenSpace

Table 2.15 of the Planning Scheme defines the required public open space for the entire Adamstown Station Development Area to be 0.3 hectares. A total of 16,000 sqm of public open space/ public realm is permitted under SDZ20A/0008 (Phase 1 of the District Centre, called *The Crossings*). This significantly exceeds the minimum requirement for public open space in this entire Development Area. Nevertheless, a further 3,138 sqm of public open space is provided as part of the proposed development.

This is provided in 3no. parcels of the proposed development, through the provision of a pocket park in the southeast corner of Block A, and through 2 no. further parcels running north south and east west through Block C and Block D. These spaces will be suitable for incidental play and will be provided with play elements. We refer the Planning Authority to the Landscape Design Statement and landscape plans prepared by Camlins Landscape Architects for further information.

### 5.3 Landscaping & Boundary Treatment

The landscape proposals in this case have been designed by Camlins, Landscape Architects. These designs have been directly informed by the following studies that have been undertaken in respect of the site and the development being proposed: -

- Appropriate Assessment Screening Report, prepared by Brady Shipman Martin
- Ecological Appraisal, prepared by Brady Shipman Martin
- Daylight & Sunlight Report, prepared by O'Connor Sutton Cronin Consultant Engineers,
- Pedestrian Wind Comfort Study, prepared by O'Connor Sutton Cronin Consultant Engineers,
- Energy & Sustainability Report, prepared by O'Connor Sutton Cronin Consultant Engineers,
- Public Lighting Plan and Report, prepared by O'Connor Sutton Cronin Consultant Engineers,
- Building Lifecycle Report, prepared by O'Connor Sutton Cronin Consultant Engineers,
- Drainage Drawings, prepared by CS Consulting Engineers

There has been a high level of co-ordination between Camlins Landscape Architects, Brady Shipman Martin Ecologists O'Connor Sutton Cronin Consultant Engineers and CS Consulting Engineers to ensure that the requirements of biodiversity, Irish Water, the provision of the required services and the provision of a good quality of landscape is appropriately provided in this case.

## **5.4 Water Services**

The enclosed Engineering Assessment Report, prepared by CS Consulting Group, details the proposals for water services to and from the development.

Much of the strategic water services infrastructure has already been put in place to serve Adamstown. This proposal seeks to connect to that existing infrastructure.

### **5.4.1 Water Supply**

The proposed watermain infrastructure and routing plan is shown on the enclosed drawing D101-002 prepared by CS Consulting. Irish Water has provided a Confirmation of Feasibility for the proposed development (enclosed in Appendix B of Engineering Services Report).

We refer the Planning Authority to the enclosed Watermains Drawings and Engineering Services Report, prepared by CS Consulting Group, for full details of the proposed water supply to the development.

### **5.4.2 Surface Water Drainage**

It is proposed that this development will be primarily drained to the existing culvert infrastructure that was constructed in 2011 to manage surface water flow in the area. SuDS infrastructure has also been incorporated into the design of the development. This includes bio retention tree pits, permeable paving, and downstream defenders to contribute to improving water quality in the runoff from the site.

We refer the Planning Authority to the enclosed Engineering Services Report and Engineering Drawings, prepared by CS Consulting Group, for full details of the Surface Water Drainage proposed through this planning application.

### **5.4.3 Foul Water Drainage**

It is proposed to discharge foul water from the proposed development directly into the existing foul water system which was constructed under the Adamstown SDZ Strategic Drainage Scheme. Foul effluent from the proposed development will be collected in pipes of 225mm diameter and flow under gravity to the existing 225mm diameter foul sewer on Station Road.

We refer the Planning Authority to the enclosed Engineering Services Report and Drawings prepared by CS Consulting Group for full information regarding these proposals.

### **5.4.4 Flood Risk Assessment**

A Flood Risk Assessment Report has been prepared by CS Consulting Group and is enclosed as part of this application. This document assesses the risk and outlines required mitigation measures associated with flooding on the site, concluding in all instances that the residual risk of flooding to the proposed development to be minor and within acceptable levels.

### **5.4.5 Taking in Charge**

It is proposed that various roads, footpaths and open space will be taken in charge by South Dublin County Council. The Applicant's proposals for Taking in Charge are represented on Drawing No.LL591-200-0005 prepared by Camlins Landscape Architects.

## 5.5 Transportation

This planning application is accompanied by the following reports, prepared by Atkins Consulting Engineers:

- Transport Statement
- Outline Residential Mobility Management Plan
- Roads Design Report
- Design Stage Traffic Management Plan
- Stage 1 Road Safety Audit

These reports demonstrate that the impact of traffic from the proposed development will be mitigated, and that the development will result in a low dependence on car travel. The road and junction layouts have been designed to meet relevant road safety standards. We refer the Planning Authority to these reports and the accompanying engineering drawings for further information.

## 5.6 Farmer's Bridge

Immediately to the west of Adamstown Train Station there is an overpass bridge which allows agricultural vehicles to safely cross the railway line. This was constructed under planning permission Reg. Ref. SDZ06A/9, at the time that the Dublin to Kildare rail line was upgraded. The bridge is solely used for agricultural vehicles and is in private use by a local farmer. The approach road to this bridge is the western boundary of the application site. The approach road begins on an embankment which gradually rises onto an elevated structure supported by struts.

The long-term future of 'the farmer's bridge' - as it is known - is uncertain. As development continues in Adamstown and the area becomes less agricultural in nature, the need for a solely agricultural overpass may diminish. Furthermore, the Planning Scheme is not explicit in how the farmer's bridge should be treated in the development of lands adjoining it.

In the absence of certainty, this development proposal has carefully considered the interface between the proposed apartment buildings and the farmer's bridge approach road. It ensures that the proposed apartment buildings adjacent to the structure comfortably interface with the bridge, so that in the event that the bridge remains into the long term, the future residents will not experience any adverse amenity from proximity to the bridge.

A landscaped pedestrian walkway is provided between the bridge and the proposed apartments buildings. The apartments are designed so that the walkway is always subject to passive surveillance and will remain an attractive and safe place to walk or cycle. The enclosed Sunlight and Daylight Report demonstrates that the units closest to the bridge will not suffer from poor levels of internal daylight or any adverse amenity impacts. Cross sections have also been provided to show the relationship between the proposed buildings and the farmer's bridge approach road.





**Figure 8:** Extract the Design Statement prepared by Henry J Lyons Architects illustrating the bridge walk along Block C.

We refer the Planning Authority to the Design Statements of both Henry J Lyons Architects and also Camlins, Landscape Architects for further detail on how the development proposed engages with the existing temporary bridge structure and provides an amenity for future residents to enjoy.

A vehicular hammerhead is provided to enable turning of vehicles at the western boundary of the site. As the future of the bridge becomes clearer, and development commences in the 'Boulevard' Development Area immediately to the west of the bridge where this road connects, this road can potentially be extended to continue westwards if necessary. The lands in the Boulevard Development Area are also in the control of the Applicant.

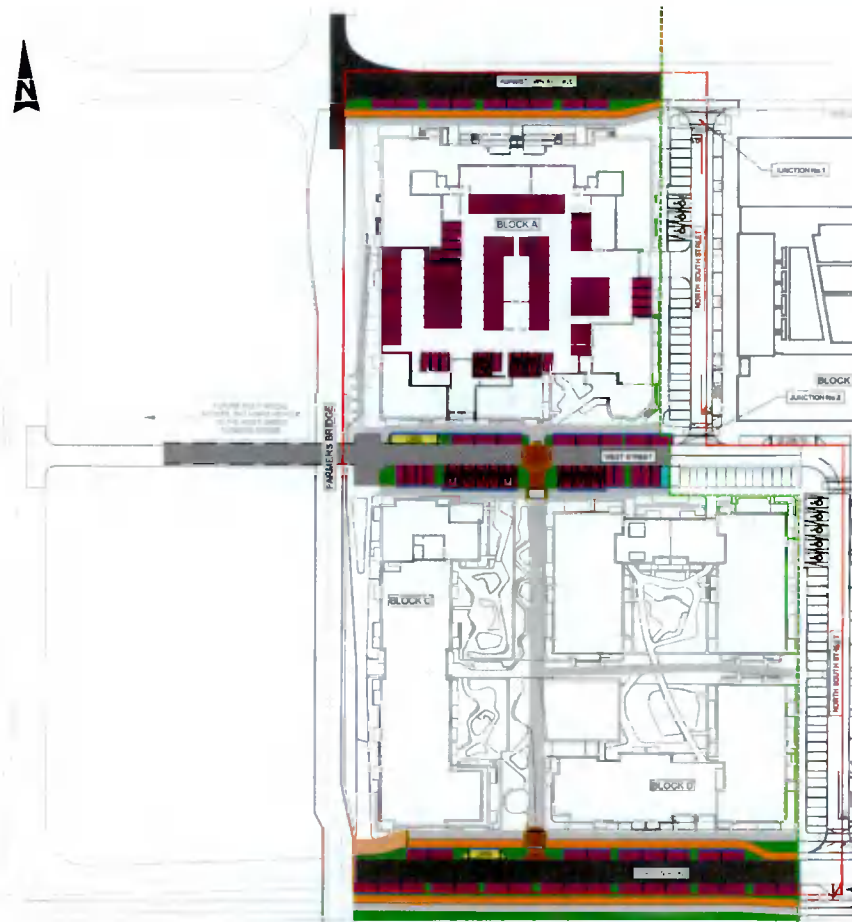


Figure 9: Extract from Drawing No: 5150924 / HTR /08 / DR /0102 Junction Layout Plan and car parking distribution, prepared by Atkins Consulting Engineers.

## 6 STRATEGIC PLANNING CONTEXT

The application site lies within the boundaries of the area covered by the Adamstown Planning Scheme 2014, as amended (Planning Scheme). The Planning Scheme has been prepared by South Dublin County Council, in accordance with the following strategic planning guidance:

- *Regional Planning Guidelines for the Greater Dublin Area, 2010 - 2022*
- *Retail Strategy for the Greater Dublin Area, 2008 - 2016*
- *Greater Dublin Area Transport Strategy, 2011 - 2020*
- *Design Manual for Urban Roads and Streets, March 2013*
- *Guidelines for Planning Authorities on Sustainable Residential Development in Urban Areas (Cities, Towns and Villages) 2009*
- *Sustainable Urban Housing: Design Standards for New Apartments – Guidelines for Planning Authorities, 2007*
- *Planning and Development of Large Scale, Rail Focused Areas in Dublin, 2013*
- *School Travel Toolkit Smarter Travel Workplaces: A Guide for Implementers*
- *National Cycle Manual Greater Dublin Area Cycle Network Plan 2013*

The Planning Scheme as adopted is in accordance with these strategic planning documents, and therefore any planning application, which is in compliance with the Planning Scheme, can also be said to be in compliance with those documents.

The Design Standards for New Apartments (2020) came into effect after the adoption of the current Planning Scheme. Under the Planning and Development Act 2000, as amended, these guidelines identify *specific planning policy requirements* that take precedence over local statutory plans, including SDZ Planning Schemes.

Section 8 of this report details the compliance of the proposed development with the Adamstown Planning Scheme. Section 9 of this report details the compliance of the proposed apartment units with the Apartment Standards Guidelines (March 2018).

## 7 SOUTH DUBLIN COUNTY DEVELOPMENT PLAN, 2016 - 2022

The application site is designated to be part of the Adamstown Strategic Development Zone. Section 169(9) of the Planning and Development Act 2000 (as amended) reads as follows:

*“(9) A Planning scheme made under this section shall be deemed to form part of any development plan in force in the area of the scheme until the scheme is revoked, and any contrary provisions of the development plan shall be superseded.”*

As such, the Adamstown Planning Scheme 2014 is the principal planning document in relation to the site and should be deemed to form part of the Development Plan. This report thus examines the proposed development’s compliance with the Adamstown Planning Scheme primarily. In accordance with the above section of the Act, the Planning Scheme is deemed to form part of the Development Plan. Any complementary measures in the Development Plan will still apply within the SDZ, and these are also discussed in this section.

### 7.1 Land Use Zoning

The application site is zoned ‘SDZ’ in the South Dublin County Development Plan 2016 – 2022 and is designated to be part of the Adamstown Strategic Development Zone. The land-use objective for the SDZ zoning reads:

*“To provide for strategic development in accordance with approved planning schemes.”*

## 8 COMPLIANCE WITH THE ADAMSTOWN PLANNING SCHEME

### 8.1 Extent of Development

The proposed development would be the third phase of development to take place in the Adamstown Station Development Area (Development Area 11).

The min-max extent of total residential development allowed within the Adamstown Station Development Area under the Planning Scheme (as amended) is 49,000 – 97,500 sq. m, or 490-975 dwellings.

The current proposal is for 436no. dwellings (34,259 m<sup>2</sup>) (37,402 m<sup>2</sup> gross floor area including car park and storage) on an area of approximately 2.43 hectares. This will serve as Phase 3 of development in the Adamstown Station Development Area.

### 8.2 Overall Compliance

The following Tables identify compliance of the proposals with the primary criteria applying to each of the Development Areas.

Criteria (Adamstown Station – Development Area)	Planning Scheme	Permitted / Constructed	Proposed in this Application	Remaining Development Area
Min – Max Dwellings per Ha	75-150 ±20% (60-180)	96	180	75-150±20% (60-180)
Min-Max Total Dwelling Units	490-975	463	436	0-76
Min – Max Total Residential Development	49,000 – 97,500 sqm	41,224 sqm	34,259	0-22,017sqm
Min -Max Non-Residential Development	11,700 – 29,250 sqm	9,888 sqm	0 sqm	1,812 – 19,362 sqm
Min Local Public Open Space	0.3 ha	1.7ha	3,138 sqm	No additional requirement

Table 3: Table demonstrating area breakdown.

The Masterplan accompanying Planning Application Reg Ref SDZ20A/0008, as updated in the Design Statement accompanying this planning application, provides evidence for the Planning Authority that the balance of the Development Area has been designed so as to be compliant with the above provisions.

### 8.3 Density

The Adamstown Station Development Area is identified in the Planning Scheme as a high-density character area in the context of Adamstown. Table 2.17 of the Adamstown Planning Scheme identifies the permissible range of dwelling density allowed within each net Development Area in Adamstown. The relevant standards for the proposed development are as follows (as per Table 2.7):

- Adamstown Station net Development Area: 75-90 Dwellings per Hectare

The 2020 Amendment to the Planning Scheme however amended the allowable density range to 75 – 150 dwellings per hectare in the Adamstown Station Development Area.

Section 2.3.17 of the Planning Scheme reads:

*“Development on any individual development site may fall above or below the specified density ranges for that Development Area by a maximum of 20% subject to the specified min-max density ranges being achieved for the area as a whole. The applicant will be required to demonstrate at planning application stage that the provisions of the Scheme with respect to density can feasibly be met through future development proposals within the applicant’s landholding or on adjoining landholdings within that Development Area subject to the written agreement of adjoining landowner(s).”*

This allows for the residential density within an individual planning application in this Development Area to be as low as 60 dwellings per hectare, or as high as 180 dwellings per hectare. The proposed development achieves a density of c. 180 dwellings per hectare, which is within this  $\pm 20\%$  range of the maximum density of 150 dph and therefore complies with the Planning Scheme.

We would note also that the Applicant is the owner of the entirety of the Development Area and is therefore in adequate control to ensure residential densities across the Development Area as a whole are also compliant with the Planning Scheme.

A masterplan for the entire Development Area was submitted with application Reg. Ref SDZ20A/0008, which was first phase of development at the Adamstown District Centre. This has been subject to refinements, as outlined in the enclosed Design Statement prepared by Henry J Lyons Architects. Consideration has therefore already been given to the layout of the future stages of development within the subject Development Area. This has taken account of the anticipated densities across the Development Area as a whole to ensure compliance with the Planning Scheme.

It is submitted that, overall, the Adamstown Development Area will be entirely consistent with the density parameters of the Planning Scheme.

#### **8.4 Non-Residential Development**

No non-residential development is proposed as part of this application. As the Planning Scheme includes Flexible Use Perimeter Buildings, the ground floor level of the proposed buildings has been designed to have greater floor to ceiling heights than the minimum standards specified by the Design Standards for New Apartments (2020). In addition to enhancing the residential amenity provided, this also allows for the possibility of future conversion to commercial uses, should the demand arise.

#### **8.5 Building Type and Height**

Figure 2.4 of the Planning Scheme indicates that the application site should contain courtyard buildings and perimeter buildings. The Planning Scheme defines the buildings heights for those types of building in this part of the Adamstown Station Development Area as:

- For Courtyard buildings, 2 to 4 storeys, with up to 5 storey at corners or feature buildings
- For Perimeter buildings, 3 storeys plus setbacks to 5 storeys plus setback

Furthermore, landmark buildings may be developed up to a maximum height of 10 storeys in this area. There is further discussion regarding the landmark building at Section 8.6 of this Report and also in the Design Statement prepared by Henry J Lyons, Architects.

Courtyard Blocks are placed along Great Western Way, centrally located in the group of buildings. The southern wings along blocks C & D are also kept low at 5 storeys to assist with daylight / Sunlight

permeability into the central gardens. A 2 storey resident's pavilion structure is linked to Block D which is primarily 5 storeys in height.

The proposed building heights are therefore considered to comply with the Planning Scheme.

We refer the Planning Authority to the enclosed architectural drawings and Design Statement, prepared by Henry J Lyons Architects, for further details relating to the building heights proposed.

## 8.6 Landmark Buildings

The indicative layout of the Adamstown Station Development Area as shown in the Planning Scheme shows 1no. landmark building location within the application site. The maximum height for a landmark building in this Development Area is 30 metres (approximately 10 storeys).

Apartment Block C is designated as the landmark perimeter building as part of this application. It rises to 9 storeys at the junction of Great Western Way and the road which runs between the Adamstown Station Tile and the Adamstown Boulevard Tile, known locally as 'the farmer's bridge'. This road is on the main vista to and from the District Centre and the proposals for a landmark building here marks this important nodal point appropriately. In addition to the increased building height, this landmark building has been thoughtfully designed. It is noted that Section 2.3.34 of the Planning Scheme states:

*"It is important to note that the significance of these buildings need not be limited to their height and that their presence may be enhanced by changes in building form, colour and construction materials."*

The proposed landmark building a Block C complies with the spirit and intent of the Planning Scheme in this regard.



Figure 10: Extract from Design Statement prepared by Henry J Lyons Architects, showing the architectural expression of the landmark building.

We refer the Planning Authority to the Design Statement prepared by Henry J Lyons, Architects for further elaboration on the design intent behind the proposals for this landmark building.

## 8.7 Flexible Use Buildings

The proposed development is entirely residential in use. However, floor to ceiling heights of the ground floor units are in excess of the minimum for residential units required under the Design Standards for New Apartments 2020. This provides flexibility for future conversion of these units to commercial use, if the demand should arise. It is noted that any such conversions would be the subject of a future planning application(s).

## 8.8 Development Standards for Residential Units

### 8.8.1 Unit Size

Section 2.3.47 of the Planning Scheme specifies that required house unit sizes in Adamstown shall be consistent with the standards set out in the 2007 Ministerial Guidelines, *“Quality Housing for Sustainable Communities”* as amended upwards by Table 2.10 of the SDZ Planning Scheme. The amendment to the Planning Scheme which was adopted in 2017 altered this paragraph to include a required to comply with Design Standards for New Apartments (2015) or any superseding standards. The updated Design Standards for New Apartments published in 2020 can be understood to be ‘superseding standards’.

The Housing Quality Assessment prepared by Henry J Lyons Architects confirms that all proposed dwellings meet and exceed standards set out in the 2020 Ministerial Guidelines and therefore fully comply with the Planning Scheme.

Compliance with the Apartment Guidelines in respect of unit size is further detailed in Section 9 of this Report.

### 8.8.2 Private Amenity Space

As highlighted above, an amendment to the Planning Scheme was adopted in 2017 to bring the Planning Scheme into alignment with the Ministerial Guidelines, namely the 2015 Apartment Guidelines or any superseding document. As outlined above, the Design Standards for New Apartments (2020) have superseded the 2015 document and therefore compliance with these standards for private amenity space equates to compliance with the Planning Scheme.

The Schedule of Accommodation prepared by Henry J Lyons confirms that all proposed dwellings meet or exceed these standards.

Compliance with the Apartment Guidelines in respect of private amenity space is shown in Section 9 of this Report.

### 8.8.3 Car Parking

Section 6 of the enclosed Transport Statement prepared by Atkins Consulting engineers sets out the car parking provision for the proposed development. This confirms that the car parking provision complies with both the Planning Scheme and the Design Standards for New Apartments.

88no. car parking spaces are proposed beneath the podium, 60 no. car parking spaces are provided at street level and a further 72no. provided within the Block F multi-storey car park which has already been permitted, is under construction and which is controlled by the Applicant. This provides a car parking ratio of approximately 0.5 car parking spaces per apartment. 2no. visitor car parking spaces are available at street level.

It is note that the Adamstown SDZ recommends a residential minimum requirement of 1 space per unit. In application Reg Ref SDZ20A/0008 (Phase 1 of Adamstown District Centre), it was established that this minimum requirement should apply across the entire Adamstown SDZ, when considering a lower residential parking provision in Adamstown District Centre. An analysis was undertaken as part of that application which confirmed that a provision of 1 parking space per 2 residential units for the Adamstown Station tile maintains an overall parking ratio in the order of 1.25 car parking spaces per unit for the entire Adamstown SDZ.

It is therefore submitted that the proposed car parking provision of approximately 0.5 space per unit in this planning application is compliant with the Planning Scheme as a whole, as well as the Apartment Guidelines 2020.

We refer the Planning Authority to the Transport Statement prepared by Atkins Consulting Engineers for further information on the car parking strategy for the Adamstown Station Tile.

#### **8.8.4 Cycle Parking**

A minimum standard of 1 cycle parking space per apartment applies, as per Table 2.13 of the Planning Scheme. As such, for apartment residents, 416no. secure cycle parking spaces are proposed. An additional 110no. visitor bicycle parking spaces are provided as uncovered Sheffield Stands in the public space. A total of 526no. bicycle parking spaces are proposed within Blocks A, C and D.

Further details of the proposals to accommodate bike parking can be found in the plans and particulars prepared by Henry J Lyons Architects accompanying this application.

#### **8.9 Landscaping: Public Open Spaces, Major Parks and Play Areas**

We refer the Planning Authority in the first instance to the landscape details within the enclosed Landscape Urban Design Statement, prepared Camlins Landscape Architects.

The Adamstown Planning Scheme sets out guidelines for the provision of Young Children's Areas for Play (YCAPs), Local Equipped Areas for Play (LEAPs) and Neighbourhood Equipped Areas for Play (NEAPs) per each development area of Adamstown. The LEAPs are provided in the major parks, with the NEAPS normally provided in the Development Areas, save for a number of exceptions, includes where the Development Area directly abuts a major park.

The guidelines suggest that those areas that are located close to the major amenity areas of Adamstown will have their requirements for LEAPS and NEAPS provided for in those Amenity Areas.

The proposed development will ultimately be served by Central Boulevard Park. Central Boulevard, a major park, will provide a wide variety of equipped areas for play for the residents of Adamstown Station and other adjoining Development Areas, as indicated in Table 2.15 of the Planning Scheme, including, inter alia "2 x district level LEAP & NEAP play facilities at opposite edges, courts, landscaping and footpath" Table 2.17 confirms that the threshold for requiring a LEAP is "1 per Development Area, excluding areas close to major and urban parks – see Table 2.15".

In addition, a significant civic space is to be provided very close to the application site, at the heart of Adamstown District Centre. This was permitted under Reg. Ref. SDZ20A/0008. This civic space has been designed so as to provide an attractive place for people of all ages and to have the capacity to accommodate periodic events. This public square provides well in excess of the minimum public open space requirement for the entire Adamstown Station Development Area (16,000sqm is provided, while only 3,000sqm is required).

Notwithstanding this, the proposed development also provides additional public open space of c. 3,138 sq.m in landscaped areas on site. As can be seen on the enclosed landscape drawings prepared by Camlins Landscape Architects, provision has been made for informal play opportunities within the proposed public open space.

#### **8.10 Transport and Connectivity**

##### **8.10.1 Cycling and Walking Routes**

Section 2.4 (vi) of the Planning Scheme relates to the primacy that walking and cycling should be afforded over vehicular transport in the design of development in Adamstown. The proposed development is integrated into the overall Adamstown Station Development Area masterplan. This



masterplan prioritises cyclist and pedestrian permeability through Avenues, Local Streets and Civic Linking Spaces that are designed intentionally to slow down vehicular traffic.

We refer the Planning Authority to the enclosed Design Statement, prepared by Henry J Lyons Architects, as well as the Transport Statement prepared by Atkins for further details.

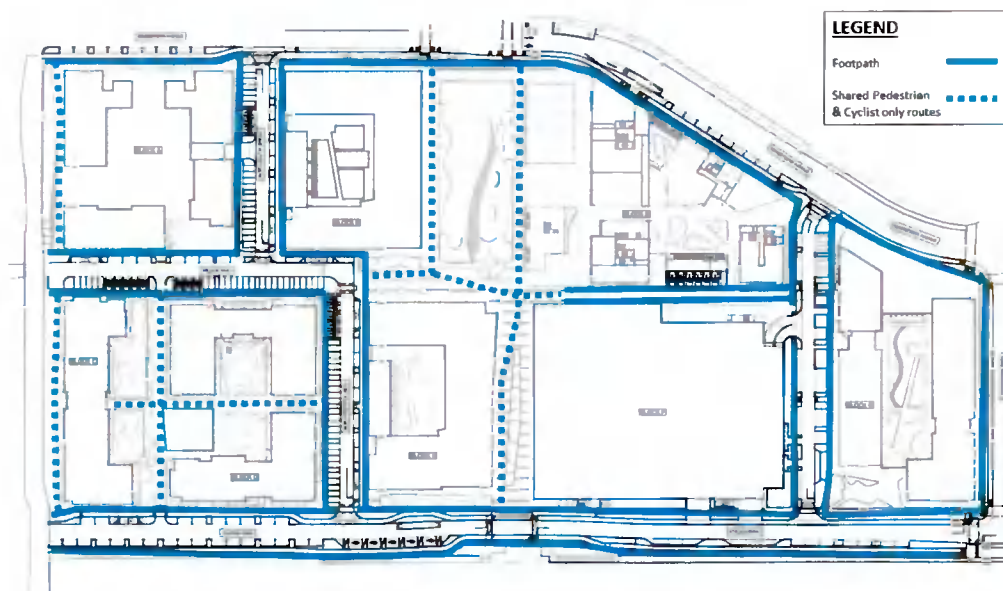


Figure 11: Extract from the Outline Residential Mobility Management Plan, prepared by Atkins Consulting Engineers showing Pedestrian Movement within the proposed development.

### 8.10.2 Public Transport

The site is within walking distance of the No. 25B Dublin Bus route, which currently serves Adamstown, with the nearest stop being at Adamstown Station. The Adamstown Station rail station is also within walking distance of the site. Figure 2.17 of the Planning Scheme confirms that the site is within a 0–5minute walk of the existing Adamstown Rail Station.

The site is also within walking distance of the planned Adamstown Quality Bus Corridor (QBC) along Adamstown Boulevard, to the north. Figure 2.18 of the Planning Scheme confirms that the site is within a 0–5minute walk of the QBC.

As Adamstown grows, the demand and corresponding supply of public transport by bus will increase in the area. Adamstown Boulevard will incorporate the main quality bus corridor route through Adamstown.

### 8.11 Childcare Facilities

Table 2.16 of the Planning Scheme outlines the necessary minimum requirements for childcare facilities in Adamstown. This provides for a focussed delivery of childcare facilities at Adamstown in specific locations, rather than a haphazard sprinkling of these facilities all around Adamstown. The Planning Scheme does prescribe the need for a childcare facility in the Station Development Area.

There is an existing Giraffe childcare facility in Adamstown Castle. In addition, 200 childcare spaces have been granted permission under Shackleton 2 (SDZ19A/0011) and St. Helen's 2 (SDZ18A/0002). An operator has purchased the Shackleton Mill Creche, and outfit is underway with operation intended to commence in 2023. The operation of the St. Helen's 2 facility is dependent upon securing a commercial tenant.

We can confirm that the Applicant has already commenced an engagement with the Planning Authority in respect of the planned childcare facility in the Adamstown Station Development Area. The final phase of the Station Development Area has been identified for that facility. There is thus no proposed childcare facility to be provided as part of this application.

### 8.11.1 Overall Design of Development

In addition to those other aspects that are detailed above, the following guiding principles – set down in section 2.3.2 of the Planning Scheme – have been followed in the design of the proposed development.

### 8.11.2 Road and Street Layout and Design

The development provides connectivity and permeability for pedestrians, with direct routes and connections that are subject to passive surveillance throughout, while limiting the movement of cars in vehicular cul-de-sacs. The streets have been designed in accordance with the Design Manual for Urban Roads and Streets, 2013 (DMURS) and the Adamstown Street Design Guidelines, 2009, as required in the Planning Scheme. Atkins, Transport Planning Consultants have guided the Design Team so as to ensure these principles were factored into the design from the outset.

The layout of the road network across the Adamstown Station Development Area masterplan has been informed by the indicative layout provided by Figure 2.10 of the Planning Scheme. The subject site is bounded by Adamstown Avenue, Station Road and to the west by the railway overpass and its approach road, known locally as ‘the farmer’s bridge’ and a back street between the proposed Blocks A & D and permitted Blocks B and E.

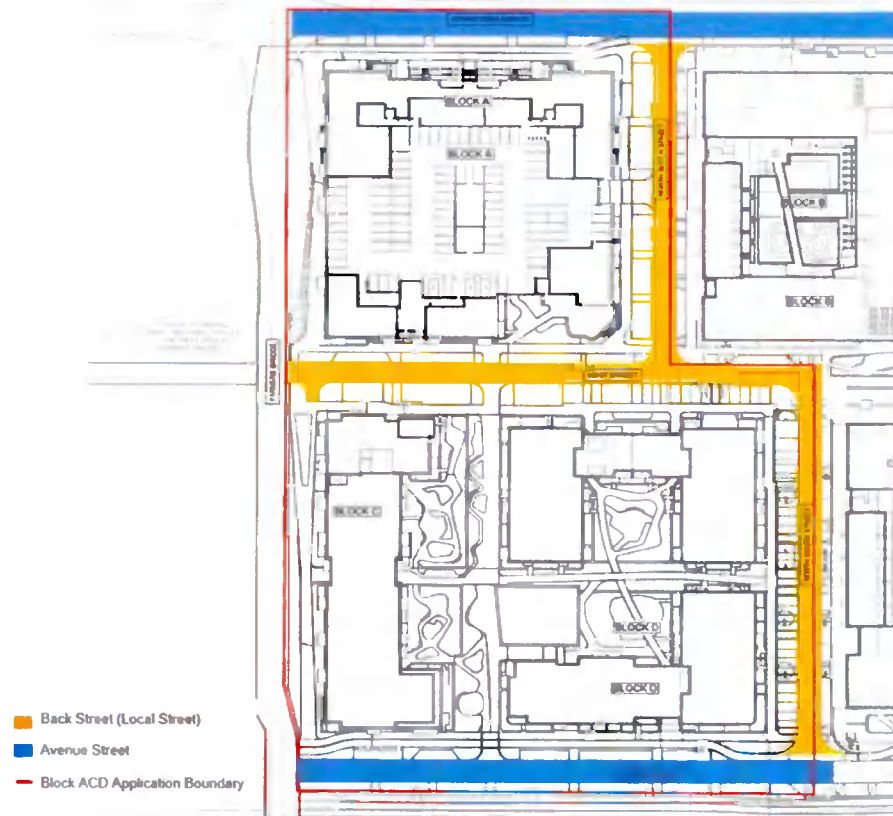


Figure 12: Extract from the Roads Engineering Report, prepared by Atkins Consulting Engineers showing Block A, C & D Street Typology and Layout

For more information on the design of streets, please see enclosed drawing and Roads Engineering Report prepared by Atkins Consulting Engineers.

### 8.12 Part V- Social Housing

The Planning Scheme requires that any development comply with Part V of the Planning & Development Act, 2000 (as amended) and the South Dublin County Housing Strategy 2014 – 2018. As detailed in Section 11 of this Report, the applicant has provided details of how the proposed development will meet the requirements of Part V of the Act.

### 8.13 Compliance with Phasing Requirements

With c. 5,153 units already permitted, the Adamstown Planning Scheme is currently in Phase 7 (5,001-5,800units). However, it is noted that the occupation of such units are presently still in Phase 4 (2,601-3,400 units) at 3,155 units up to 31<sup>th</sup> December 2021. The Planning Authority have confirmed that the infrastructure requirements for Phase 3 have all been met. As such, all of the phasing requirements for Phases 1 – 3 are provided. As such, it is possible to occupy up to 3,400 dwellings before the Phase 4 requirements will apply. There are therefore some 245 occupations before these Phase 4 phasing requirements apply.

Section 4 of the Planning Scheme determines the required infrastructure to be delivered prior to the occupation of the 2,601<sup>st</sup> dwelling in Adamstown. This infrastructure is partially completed, as outlined in the tables below.

We refer the Planning Authority to the below tables showing the progress of infrastructure throughout Adamstown phases 2 – 6.

Infrastructure, Services, Facilities and Amenities	Status
<b>Phase 2 (1,001-1,800 dwellings)</b>	
Adamstown Access Road adjoining the railway line (as Haul Road)	Complete
Upgrade Existing Lucan-Esker and Lucan Low-Level pump stations.	Complete
Construction of Primary Schools 1 + 2 with a minimum of 8 no. classrooms permanently constructed, or a secondary school with minimum 12 no. classrooms of permanent construction.	Complete

Infrastructure, Services, Facilities and Amenities	Status
<p>Community Centre (Two Options):</p> <p>1 – One community centre with a minimum floor area of 1,200 square metres shall be provided in phase 2. This facility shall incorporate a community sports hall (minimum 33m x 18m), 4 no. multi-purpose meeting rooms, a kitchenette and toilet facilities.</p> <p>OR</p> <p>2 – A school sports hall (minimum 600m<sup>2</sup>) shall be provided in phase 2 and this shall provide for community use, and in addition a 600m<sup>2</sup> community centre (4 no. multi-purpose meetings rooms, a kitchenette and toilet facilities).</p>	Complete
Provisions of an all-weather pitch with a minimum floor area of 2000m <sup>2</sup> .	Complete
Loop Road 1 – Alternative access arrangement for existing eastern section of Tandy's Lane are to be determined by the Planning Authority in consultation with the residents of that section of Tandy's Lane.	Complete
Provision of surface water drainage on a pro-rata basis.	Complete
Completion of the Newcastle Road (R120) study as part of an overall Lucan Movement Framework Plan.	Complete.

Table 5: Progress of Phase 2 Infrastructure in Adamstown

Infrastructure, Services, Facilities and Amenities	Status
<b>Phase 3 (1,801-2,600 dwellings)</b>	
Both Sides of Adamstown Link Road (linking Adamstown to the Outer Ring Road to	Complete

Infrastructure, Services, Facilities and Amenities	Status
provide a two-way dedicated QBC busway in addition to a two-way carriageway road.	
Adamstown Station Access Road adjoining the Railway line	Com pète
Part of the northern section of Loop Road 3 (as a haul road).	Com pète
The northern section of Loop Road 2 to replace the existing western section of Tandy's Lane.	Com pète
Tobermaclugg pumping station and associated overflow management measures and rising mains	Com pète
Upgrade to Tobermaclugg pumping station between the SDZ lands and N4 to include upgrading the Tubber Lane surface water drain. Regrading section of the channel and enhancing the capacity of the N4 culvert.	Com pète
A dedicated QBC busway on Adamstown Avenue (from outer Ring Road to Adamstown Park)	Com pète
Commencement of Tandy's Lane Park OR Airlie Park	Com pète Permission has been granted for both Tandys Lane Park and Airlie Park. Site clearance works, drainage and attenuation works for Tandys Lane Park commenced
Floor Space Provision: Provision of minimum floor space of Tandy's Lane Village or Tobermaclugg Local Centre (1,620 m <sup>2</sup> or 1800 m <sup>2</sup> ) OR Minimum 1800 m <sup>2</sup> of floorspace provision in the District Centre.	Com pète Planning Permission obtained (SDZ18A/0015) and works commenced – Tobermaclugg Village – 3,753.7sq m GFA
The eastern section of the Loop Road around Adamstown District Centre.	Com pète
A District Centre Busway Loop Road adjoining Adamstown Station.	This is required only if the required retail provision in this Phase occurs at the District Centre.
65 minimum full-time childcare spaces.	Com pète

Infrastructure, Services, Facilities and Amenities	Status
	To date 133no. full time childcare spaces provided. 65no. allocated to this Phase 3 requirement. A further 200no spaces have been granted permission, and 100no. are due for completion in Q4 2019 in Tobermaclugg Village.
Surface water drainage on a pro-rata basis.	Proposals for the surface water drainage for the subject site are included in the enclosed Engineering Report, prepared by Waterman Moylan Consulting Engineers.
Any necessary improvement works to the Newcastle Road between Adamstown and the N4, arising from the Newcastle Road Study.	Complete

**Table 6:** Progress of Phase 3 Development Infrastructure in Adamstown

Infrastructure, Services, Facilities and Amenities	Status
<b>Phase 4 (2,601-3,400 dwellings)</b>	
Construction of a Secondary School with a minimum of 12no. classrooms of permanent construction if not constructed as part of Phase 2, or construction of Primary School No.1 and no.2 with a minimum of 8no. classrooms, of permanent construction.	Complete
Completion of Park commenced (Airlie or Tandy's Lane) in Phase 3 including pitches/courts, play facilities, landscaping, and footpaths.	Complete Tandy's Lane Park is complete and open to the public – SDCC have advised that Airlie Park is earmarked to be open in August 2022.
The Link Road to the rear of the existing Lucan District Centre with pedestrian/cycle or vehicular access to the centre.	This pedestrian link as detailed in the granted Somerton II planning permission ref SDZ19A/0004, Condition 3 is under construction and is due to be completed in Q3 2022 up to the boundary wall. The recent grant of permission for the extension to Lucan Shopping Centre includes a condition to finish the link.

Infrastructure, Services, Facilities and Amenities	Status
The northernmost, central and southernmost sections of the central spine of Loop Road 2 (Adamstown Boulevard) and provision of a dedicated QBC Busway.	This is road (J22 to J17) is granted under Planning refs SDZ18A/0002 and SDZ 18A/0009. Its construction is substantially complete and will be opened in summer 2022 in tandem with the completion of residential sites, Airlie Park site and footpaths/verges immediately adjacent to it to allow those sites be completed efficiently and safely.
Part of the northern section of Loop Road 3.	<b>Com pète</b> This road (J22 to J29) was granted planning permission under SDZ16A/0003. It has been completed and is open to the public.
Two cross links between Loop Roads 1 and 2, one of which comprises the north eastern section of the Loop Road around Adamstown District Centre.	The northern Cross link (J39 to J21) is granted under planning reference SDZ18A/0009. It is substantially complete and will be opened in 2022 in tandem with the adjacent residential sites. The Southern Cross Link (J12 to J18) was granted under SDZ06A/0005. It is complete and open to the public.
The provision of surface water drainage on a pro-rata basis.	<b>Com pète</b>

Table 7: Progress of Phase 4 Development Infrastructure in Adamstown

Infrastructure, Services, Facilities and Amenities	Status
<b>Phase 5 (3,401 – 4,200 dwellings)</b>	
Health Centre	Detailed discussions to finalise terms with an operator for No.1 Adamstown Boulevard are currently being concluded with Quintain.
Commencement of Airlie or Tandy's Lane Park (whichever has not been provided in Phase 3-4)	<b>Com pète</b> Tandy's Lane Park is completed as per the Phase 4 requirement. Planning permission for Airlie Park is granted. The construction is being undertaken by SDCC through the LIHAF scheme. Airlie Park is currently under construction and is due for completion in Q3 2022.

Infrastructure, Services, Facilities and Am enities	Status
Provision of minimum floor space of Tandy's Lane Village OR Tobermaclugg Local Centre (1,620 sq.m or 1,800 sq.m). <u>OR</u> Minimum 3600 sq.m floorspace provision in the District Centre.	Planning application has been granted under SDZ20A/0008 (Phase 1 of the District Centre) and is under construction and due to be completed Q1 2023.
The north western section of the Loop Road around Adamstown District Centre	This road (J18 to J35) was granted under permission SDZ 06/0005 and is constructed. A contractor has been appointed to complete finishing works in conjunction with Aderrig Phase 1 residential tile. It is intended that these finishing works will commence Q4 2022 and be completed in Q1 2023.
A District Centre busway loop road adjoining Adamstown Station	<b>Com pète</b> This is open to the public with public lighting and signals operational and Dublin Bus operates a regular bus service which services the Adamstown Station*. Works are currently ongoing here to upgrade certain active travel elements of Station Road are ongoing and will be completed in 2022.
Provision of a minimum of 65 full-time childcare spaces	<b>Com pète</b> 200 childcare spaces have been granted permission under Shackleton 2 SDZ19A/0011 and St Helens 2 SDZ18/0002. An operator has purchased the Shackleton Mill Creche and fitout is underway with operation intended to commence in 2023. It must be noted that provision of further facilities is dependent on a suitable third party operator coming forward.
Site made available for primary school no. 3 (min 16 classrooms).	<b>Com pète</b> Tandy's Lane Village school site planning (SDZ21A/0001) is granted, and the site has been transferred to the DoES.
Further assessment of the sewerage works and to upgrade the 9B sewer if required as this phase or a later one specified by the Development Agency arising from the assessment.	<b>Com pète</b> These upgrade works were completed in conjunction with the construction of the Thomas Omar Way Road works by SDCC. This is now a matter for Irish Water as part of their overall remit relating to the receiving foul water network. To date Irish Water have not



Infrastructure, Services, Facilities and Amenities	Status
	raised any concerns regarding capacity for Adamstown.
The provision of surface water drainage on a pro-rata basis	Completed
New junction on the R403 Celbridge Road	Planning permission is granted. This requirement is being met by SDCC through the LIHAF scheme. Celbridge Link Road is currently under construction and completion is envisaged in Q3 2022.
Celbridge Road Link and Remaining Section of the Northern Part of Loop Road #3 as haul roads where 400 units or more are provided in the four development areas (as shown in fig 4.7a, page 99 of the Planning Scheme)	Planning permission is granted. This haul road requirement has been met by SDCC through the LIHAF scheme. Celbridge Link Road is currently under construction and completion is envisaged in Q3 2022.

Table 8: Progress of Phase 5 Development Infrastructure in Adamstown

Infrastructure, Services, Facilities and Amenities	Status
<b>Phase 6 (4,201 – 5,000 dwellings)</b>	
A further cross link road between Loop Roads #1 and #2	Planning permission has been granted and this road is currently under construction.
Opening of primary school No. 3 (minimum 16 classrooms)	As per Phase 5 above.
Provision of Civic Centre/Library Building (not illustrated).	Design has been included in the Adamstown Station tile masterplan. This item is to be delivered under URDF in accordance with the submitted Preliminary Business Case.
Completion of Park commencement (Airlie or Tandy's Lane) in Phase 5 including pitches/courts, play facilities, landscaping and footpaths.	Planning permission for Airlie Park is granted. The construction is being undertaken by SDCC through the LIHAF scheme. Airlie Park is currently under construction and is due for completion in Q3 2022.

Infrastructure, Services, Facilities and Amenities	Status
The provision of surface water drainage on a pro-rata basis	Completed
Celbridge Road Link and Remaining Section of Northern Part of Loop Road #3 as haul roads (if not already provided) where 400 units or more are provided in the four development areas as shown on Figure 4.7a	Planning permission is granted. This haul road requirement has been met by SDCC through the LIHAF scheme. Celbridge Link Road is currently under construction for completion in Q3 2022.

**Table 9:** Progress of Phase 6 Development Infrastructure in Adamstown

The applicant is fully aware of their obligations to deliver those items that are their responsibility under the phasing programme, and in many instances, they are on course to deliver infrastructure much earlier than required, as has consistently been the case since the Planning Scheme was first approved in 2003.

The Applicant is committed to carefully ensuring that the necessary infrastructure, services, facilities, and amenities outlined in the Adamstown Planning Scheme are delivered in-tandem with residential development, both in the spirit, and to the requirement, of the Planning Scheme.

## 9 COMPLIANCE WITH APARTMENT GUIDELINES (2020)

Below we outline how the proposed apartment units accord with the document “*Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities*” published by the Minister for the Environment, Community and Local Government in 2020.

For the convenience of the Planning Authority, the application pack includes a Design Statement, Schedule of Accommodation and Housing Quality Assessment, prepared by Henry J Lyons Architects. These documents should be read in conjunction with this section of the report.

### 9.1.1 Unit Mix

In overall terms the unit mix for Apartments proposed in this application is as follows

- Studio units – 9no. dwellings (2%)
- 1 bedroom – 204no. dwellings (47%)
- 2 bedroom (3person) – 16no. dwellings (4%)
- 2 bedroom (4person) – 197no. dwellings (45%)
- 3 bedroom – 10no. dwelling (2%)

Having regard to the variety of unit types and the sizes of these units it is considered that an appropriate mix of unit types is proposed. This unit mix is also consistent with *Specific Planning Policy Requirement 1*. For further details on the various unit types, the attention of the Planning Authority is drawn to the Housing Quality Assessment prepared by Henry J Lyons Architects that accompanies this application.

### 9.1.2 Apartment Floor Area

Under *Specific Planning Policy Requirement 3* of the Apartment Guidelines (2020) the following minimum floor areas apply:

- Studio Apartment (1 person): 37 m<sup>2</sup>
- 1-bedroom apartment (2 persons): 45 m<sup>2</sup>
- 2-bedroom apartment (4 persons): 73 m<sup>2</sup>
- 3-bedroom apartment (5 persons): 90 m<sup>2</sup>

All apartments that are the subject of this planning application are in excess of minimum apartment sizes, as confirmed in the enclosed Housing Quality Assessment prepared by Henry J Lyons Architects. The smallest proposed units are 39m<sup>2</sup> (a studio), and the largest units are 96.6m<sup>2</sup> (a three bedroom unit).

### 9.1.3 Target Average Floor Areas

The Guidelines require that the majority of all apartments in a proposed scheme of 10 or more apartments must exceed the minimum floor area standard for any combination of the relevant 1, 2 or 3-bedroom unit types, by a minimum of 10% (any studio apartments must be included in the total but are not calculable as units that exceed the minimum by at least 10%).

We refer the Planning Authority to the Housing Quality Assessment prepared by Henry J Lyons Architects which demonstrates compliance with this requirement and is therefore consistent with *Specific Planning Policy Requirement 3* of the Apartment Guidelines.

### 9.1.4 Aspect & Orientation

The Apartment Guidelines states:

*"It is a policy requirement that apartment schemes deliver at least 33% of the units as dual aspect in more central and accessible and some intermediate locations, i.e. on sites near to city or town centres, close to high quality public transport or in SDZ areas."*

Where single aspect apartments are provided, the provision of south facing units should be maximised, with west or east facing single aspect units also being acceptable.

Given the site's immediate proximity to the future Adamstown District Centre, as well as Adamstown Station, which is served by regular rail and bus services, it is considered that this the 50% minimum is a reasonable target in this instance. 50% of the proposed apartments are dual aspect in this particular case, which exceeds this minimum requirement. We refer the Planning Authority to the enclosed Housing Quality Assessment, prepared by Henry J Lyons Architects for further details.

We also refer to the Sunlight & Daylight Analysis, prepared by OCSC Consulting Engineering enclosed as part of this planning application. This assessed the internal daylight and amenity sunlight for the proposed development. The proposed development was found to achieve excellent levels of both internal daylight and sunlight to amenity areas.

#### 9.1.5 Floor to Ceiling Height

Minimum floor to ceiling height must accord with the Building Regulations requirement of 2.4m, except in relation to ground floor apartments where it should be greater. *Specific Planning Policy Requirement 5* states that ground level apartment floor to ceiling heights shall be a minimum of 2.7m.

The proposed apartment blocks each have floor to ceiling heights that meet or exceed these standards. We refer the Planning Authority to the enclosed Housing Quality Assessment prepared by Henry J Lyons Architects for further details.

#### 9.1.6 Storage

The Guidelines require the following minimum areas for storage areas to be provided:

##### Minimum storage space requirements

Studio	3 sq m
One bedroom	3 sq m
Two bedrooms (3 person)	5 sq m
Two bedrooms (4 person)	6 sq m
Three or more bedrooms	9 sq m

The proposed apartments all meet the requirement for storage, with storage located within the individual units. We refer the Planning Authority to the enclosed Housing Quality Assessment prepared by Henry J Lyons Architects for further details.

#### 9.1.7 Private Am enity Space

The Guidelines require that private amenity space shall be provided in the form of gardens or patios/terraces for ground floor apartments and balconies at upper levels. Private amenity space should be located to optimise solar orientation and designed to minimise overshadowing and overlooking.

The minimum required areas are set out below. The minimum areas must be achieved in a space with a minimum depth of at least 1.5 m.

**Minimum floor areas for private amenity space**

Studio	4 sq m
One bedroom	5 sq m
Two bedrooms (3 person)	6 sq m
Two bedrooms (4 person)	7 sq m
Three bedrooms	9 sq m

The proposed scheme provides individual private balconies or terraces to all apartments. All units have private amenity space that meets or exceeds the minimum standards above. We refer the Planning Authority to the enclosed Housing Quality Assessment prepared by Henry J Lyons Architects, and the apartment floor plan drawings enclosed with this application, which clearly label the location and size of all private amenity space.

**9.1.8 Communal Amenity Space**

In respect of communal amenity space, the minimum required areas are set out below:

**Minimum floor areas for communal amenity space**

Studio	4 sq m
One bedrooms	5 sq m
Two bedrooms (3 person)	6 sq m
Two bedrooms (4 person)	7 sq m
Three bedrooms	9 sq m

We refer the Planning Authority to the enclosed Site Layout Plan and Apartment Block Floor Plans prepared by Henry J Lyons Architects. The proposed development is accompanied by communal amenity space (3,281sqm) exceeding the required minimum (2,621sqm). We also refer the Planning Authority to the Housing Quality Assessment prepared by Henry J Lyons Architects which demonstrates this fact.

**9.1.9 Apartments Per Core**

*Specific Planning Policy Requirement 6* states that a maximum of 12 apartments per floor per core may be provided in apartment schemes. We refer the Planning Authority to the Housing Quality Assessment prepared by Henry J Lyons Architects which confirms compliance with this requirement.

**9.1.10 Building Lifecycle Report**

In line with the requirements of the Apartment Guidelines, a Building Lifecycle Report, prepared by O'Connor Sutton Cronin Consulting is enclosed as part of this application.

**9.1.11 Specific Planning Policy Requirements 2, 7, 8 and 9**

It is noted that Specific Planning Policy Requirements 2, 7, and 8 relate to building refurbishment schemes or to build to rent developments respectively. The proposed development is none of these, therefore the provisions of these Specific Planning Policy Requirements do not apply in this instance.

## **10 ENVIRONMENTAL ASSESSMENTS**

### **10.1 Public Lighting Proposals**

Proposals for public lighting are provided in the enclosed Public Lighting Report and Site Lighting Layout Plan, prepared by O'Connor Sutton Cronin Consulting Engineers. These have been carefully co-ordinated with the landscaping proposals.

### **10.2 Engineering Services Report**

We refer the Planning Authority to the enclosed Engineering Services Report, prepared by CS Consulting. This report provides details of the proposed foul drainage infrastructure, stormwater drainage infrastructure and potable water infrastructure.

A Confirmation of Feasibility from Irish Water, dated 11 September 2019, is appended to this report which confirms that the proposed connection to the Irish Water networks can be facilitated.

### **10.3 Flood Risk Assessment**

The enclosed Flood Risk Assessment prepared by CS Consulting addresses the question of flood risk. In overall terms a low flood risk to and from the proposed development arises under the various sources of flood risk identified in the Flood Risk Guidelines.

### **10.4 Ecological Appraisal Report**

We refer the Planning Authority to the Ecological Appraisal Report, prepared by Brady Shipman which considers the ecological aspects of the development. This report concludes that the proposed development will not result in any long-term residual impact on any ecological receptors, either within or in the vicinity of the site.

### **10.5 Screening for Appropriate Assessment**

An Appropriate Assessment (AA) Screening Report has been prepared by Brady Shipman Martin and is enclosed as part of this application. It is considered that this report provided sufficient relevant information to allow South Dublin County Council, as the Competent Authority, to carry out an AA Screening.

The report concludes that in view of best scientific knowledge, the proposed development, individually or in combination with another plan or project, will not have a significant effect on any European sites.

### **10.6 Sunlight and Daylight Analysis**

A Sunlight & Daylight Analysis has been prepared by O'Connor Sutton Cronin Consulting Engineering and is enclosed as part of this planning application. This assessed the internal daylight and amenity sunlight for the proposed development.

This assessment demonstrates that excellent levels of daylight access to the apartments is achieved. It also shows that the majority of open spaces will benefit from excellent levels of sunlight.

### **10.7 Pedestrian Wind Comfort Study**

We refer the Planning Authority to the enclosed Pedestrian Wind Comfort Study, prepared by O'Connor Sutton Cronin Consulting Engineers. This study demonstrates that with the incorporation

of suitable mitigation, predicted wind speeds are within acceptable levels to enable a good level of pedestrian comfort within and in the vicinity of the proposed development.

#### **10.8 Noise and Vibration Assessment**

An Acoustic Report prepared by iAcoustics Integrated Acoustic Solutions is enclosed. This report provides an assessment of potential noise sources and identifies mitigation where necessary. It demonstrates that appropriate ambient noise levels are entirely achievable within the development through use of suitable façade materials. It also demonstrates compliance with BS8233:2014 and WHO guideline values. We refer the Planning Authority to the iAcoustics Integrated Acoustic Solutions report for further detail.

#### **10.9 Resource Waste Management Plan for a Proposed Residential Development**

We refer the Planning Authority to the enclosed Resource Waste Management Plan prepared by Awn Consulting.

This document provides information on the legal and policy framework for C&D waste management in Ireland, estimates of the type and quantity of waste to be generated by the proposed development and makes recommendations for management of different waste streams.

#### **10.10 Construction Environmental Management Plan**

We refer the Planning Authority to the enclosed Construction Environmental Management Plan prepared by AWN Consulting.

This document sets the overall management strategy for excavation and construction works for the proposed development. It aims to ensure that the management of excavation and construction activity is carried out in a planned, structured, and considerate manner which minimises impacts on the local community and environment.

#### **10.11 Operational Waste Management Plan**

We refer the Planning Authority to the enclosed Operational Waste Management Plan, prepared by AWN Consulting.

This document presents a waste strategy that complies with all legal requirements, waste policies and best practice guidelines and demonstrates that the required storage areas have been incorporated into the design of the development.

#### **10.12 Energy & Sustainability Report**

An Energy & Sustainability Report prepared by O'Connor Sutton Cronin Consulting Engineers is enclosed with this application. This outlines how the proposed development will comply with the relevant energy standards with which the proposed development will be required to comply.

#### **10.13 Obstacle Limitation Surfaces Report**

Cyrus Limited has been engaged by Quintain Developments Ireland Limited to carry out an assessment of the impact of a series of apartment buildings on the Obstacle Limitation Surfaces (OLS) of Weston Airport. It is found that the proposed development should not adversely affect the safety or significantly affect the regularity of operations of aeroplanes.

We refer the Planning Authority to the full details of the investigation and findings in the enclosed Obstacle Limitation Surfaces Assessment prepared by Cyrus Limited.

## 11 PART V PROPOSAL

In line with the requirements of S.I. No. 387 of 2015, the Applicant sets out below their proposal to comply with Part V of the Planning & Development Act 2000, as amended as follows:

Representatives of Adamstown Station and Boulevard Limited have been in contact with the Housing Department of South Dublin County Council to initiate discussions with respect to Part V and how our client's obligations under Section 96 of the Planning and Development Act 2000, as amended, would be met with respect to the subject site.

We hereby wish to confirm that the Applicant would be prepared to enter into an agreement under a Condition of planning permission that fully meets the requirements of Part V in a manner that is acceptable to the Council. A Part V proposal is enclosed with this application for the Planning Authority's consideration, as well as a copy of a letter from SDCC Housing Department confirming that initial discussions have taken place.

The transfer of these dwellings would naturally be subject to securing planning permission and agreement with the Housing Department on attributable costs. Our client will be available to review the above or other alternative proposals with the Housing Department as required.

## 12 CONCLUSION

It is our considered professional planning opinion that the proposed development subject to this planning application should be supported by the Planning Authority on the grounds that:

- The proposed development is consistent with the SDZ Planning Scheme.
- The proposed development is complementary to its immediate environs within Adamstown.
- The proposed development will facilitate a further 436no. much needed residential units, as supported by Rebuilding Ireland, the government's Action Plan for Housing and Homelessness.



## 13 ENCLOSURES

The following items are included with this application: -

1. Completed Application Form
2. Newspaper Notice published in the Irish Daily Star on 13 April 2022 (1 no. copy of the original newspaper notice)
3. Site Notice erected at 4no. locations on 13 April 2022(1 no. copy enclosed)
4. Letter of Consent from Adamstown Infrastructure Designated Activity Company
5. Letter of Consent from Adamstown Station 1 Limited
6. Letter of Consent from Adamstown Station & Boulevard Limited.
7. Receipt for Planning Fee paid by Electronic Fund Transfers to South Dublin County Council
8. Part V Validation Letter issued by South Dublin County Council
9. Part V Proposal Letter

6no. copies of the following documents and drawings:

10. Planning Application Report, prepared by Stephen Little & Associates.
11. Planning Drawings, prepared by Henry J Lyons Architects (Please refer to enclosed drawing schedule)
12. Architectural Design Statement, prepared by Henry J Lyons Architects
13. Housing Quality Assessment prepared by Henry J Lyons Architects.
14. Engineering Drawings, prepared by CS Consulting Engineers (Please refer to enclosed drawing schedule)
15. Engineering Service Report, prepared by CS Consulting Engineers
16. Flood Risk Assessment, prepared by CS Consulting Engineers
17. Roads Engineering Drawings, prepared by Atkins Consulting Engineers (please refer to enclosed drawing schedule)
18. Roads Design Report, prepared by Atkins Consulting Engineers
19. Transport Statement, prepared by Atkins Consulting Engineers
20. Design Stage Traffic Management Plan, prepared by Atkins Consulting Engineers
21. Stage 1 Road Safety Audit, prepared by Atkins Consulting Engineers
22. Outline Residential Mobility Management Plan, prepared by Atkins Consulting Engineers
23. Landscape Urban Design Statement, prepared by Camlins Landscape Architects
24. Landscape Drawings, prepared by Camlins Landscape Architects (please refer to enclosed drawing schedule)
25. Electrical Site Lighting Layout Block A, C, D Drawing No: DR / E / 0001 prepared by OCSC Consulting Engineers
26. Public Lighting Report, prepared by OCSC Consulting Engineers
27. Building Life Cycle Report, prepared by OCSC Consulting Engineers
28. Pedestrian Wind Comfort Study, prepared by OCSC Consulting Engineers
29. Daylight Sunlight Report, prepared by OCSC Consulting Engineers
30. Energy & Sustainability Report, prepared by OCSC Consulting Engineers
31. Resource Waste Management Plan, prepared by Awn Consultants

32. Operational Waste Management Plan, prepared by AWN Consultants
33. Construction Environmental Management Plan, prepared by AWN Consultants
34. Ecological Appraisal, prepared by Brady Shipman Martin
35. Appropriate Assessment Screening Report, prepared by Brady Shipman Martin
36. Acoustic Report, prepared by iAcoustics Integrated Acoustic Solutions
37. CGI's and Photomontages, prepared by Red Vertex
38. Universal Access Statement, prepared by O'Herlihy Access Consultancy
39. Obstacle Limitation Surfaces Assessment, prepared by Cyrrus

We trust that everything is in order and we look forward to receiving a favourable decision from the Planning Authority in due course.

We confirm that we act for the Applicant in this case and would ask that all future correspondence in relation to this planning application be directed to this office.

**STEPHEN LITTLE & ASSOCIATES**  
13 April 2022