

Senior Administrative Officer,  
Planning Department,  
South Dublin County Council,  
County Hall Tallaght,  
Dublin 24,  
D24 A3XC

10 December 2021

**Further Information Response for Reg. Ref. SD21A/0186**

Development at Plot 100, Profile Park,  
Nangor Road, Clondalkin, Dublin 22

Dear Sir/Madam,

We, Brock McClure, Planning & Development Consultants, 63 York Road, Dún Laoghaire, Co. Dublin, have been instructed by the applicant, **Equinix Ireland Limited, Unit 6/7 Kilcarbery Business Park, New Nangor Road, Dublin 22, D22FV12**, to lodge this Further Information Response to South Dublin County Council.

This response is made within 6 months from the date of issue of the further information request on the 30 August 2021.

We enclose the following documentation herewith for assessment as part of this application:

No	Items	Consultant	No. of Copies	Copy/Original
1.	Revised Architectural Drawings	RKD	6	Original
2.	Revised Architectural Design Statement	RKD	6	Original
3.	Elevation Studies Submitted to SDCC for Review	RKD	6	Original
4.	Revised Photomontages and Verified Views	Digital Dimensions	6	Original
5.	Revised Landscape Drawings	Murray & Associates	6	Original
6.	Revised Civil Drawings and Cover Letter	Pinnacle	6	Original
7.	Glint and Clare Assessment	Macroworks	6	Original

The following changes were made on foot of the further information request received from SDCC, these include:

- Elevational alterations including introduction of wire mesh cladding to projecting plenums;
- Removal of internal stairs and lift at roof level;
- Relocation of flues to rear of the front (north facing hot air plenums);
- Alterations to south west external staircase (south of the building);

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- Reduction in scale of roof gantry;
- Relocation of pv panels to main roof.

A response to the Further Information Request is now set out below.

We note that the further information response material was submitted to Eoin Burke and Tracy McGibbon of South Dublin County Council on the 8 September 2021 and 13 October 2021 for review and comment. No feedback was received in relation to design changes proposed, at the time of print.

## Further Information Response

### 1.0 Item No. 1

*“The applicant is advised that the Planning Authority has concerns in relation to the visual impact of the proposed development. The subject site is located at a prominent location on the Nangor Road at the entrance to the significant business parks. In addition, the site is located in a transition area with the adjacent lands to the east and south zoned for Open Space. In this context, further consideration of the relationship between the building and the Nangor Road is required. In particular, the photomontages indicate that the proposed elevation fronting to Nangor Road will present a monolithic structure with a defensive and dark appearance. The applicant is requested to reconsider the design and investigate options to reduce the visual impact and massing of the structure. Additional landscaping, setting further back of the building from the road, relocation of flues, reduction in overall height, reorientation of the building and material/ treatment changes should all be considered.”*

#### **Applicant Response to Item No. 1:**

In response to Item No. 1, we acknowledge the importance of the northern site boundary fronting Nangor Road. As such, the design team has carefully re-considered the visual and aesthetic qualities of the proposal while maintaining the functional requirements of the proposed data hall.

We note that the referenced design and building footprint provided by the Applicant’s data centre developments are unique selling points for prospective customers. The building’s composition including length, width and overall height, has been refined as far as practicable by the Applicant and their designers.

The design team has taken into consideration the Planning Authority’s concerns and revised the building accordingly.

This response seeks to clarify and expand upon the following:

- Receiving environment and suitability of the subject proposal; and
- The proposed design amendments submitted as part of this further information response.

#### **1.1 Receiving Environment**

##### **1.1.1 Existing Built Environment**

The built environment is characterised by commercial buildings at Grangecastle and Profile Park to the north, west and south of the site , by Casement Aerodrome further south and Grangecastle Golf Club to the east.

The subject site is considered a key site as it marks the entrance to Profile Park from the roundabout on Nangor Road. As such, the north western corner, overlooking the roundabout has been designed as the administrative area with extensive glazing allowing light to filter through front of house and ancillary office areas. The western elevation reads more like a commercial office building with its contrasting and diverse architectural details and finishes.

We refer to the accompanying verified views for more information.

In summary, the siting and design of the building provides structure and enclosure to Nangor Road as depicted in the verified views accompanying this response.

### **1.1.2 Existing Natural Environment**

The subject site is located in the landscape character areas known as the Newcastle Lowlands.

Each of the 5 no. landscape character areas identified in the Plan are assessed against a set of criteria to determine the capacity of the landscape to accommodate change based on landscape sensitivity and landscape value. We reiterate that the site is located within a commercial area forming part of a network of business parks in the wider context. Furthermore, the site is not located on or near an area of outstanding character or amenity. Similarly, there are no designated views on or near the subject site.

The overall height of the building, which is guided by the height restrictions associated with Casement Aerodrome, sits comfortably within the context of the surrounding commercial area. Having regard to the height and design of the proposal, existing vegetative cover, location within a commercial area and low landscape character sensitivity, we conclude the immediate area is not considered of high visual or character sensitivity.

We refer to the visual impact assessment in Section 2.0 below and conclude the development will have a largely imperceptible visual impact on the area, particularly to the south and east of the sites on the peripheral road network. On approaches to the site from the Nangor Road, it is considered the slight to moderate impact is positive and sees the development of a commercial development on a zoned and serviced site within Profile Park.

### **1.1.5 Width of Nangor Road**

The width of Nangor Road is c.24m (including grass verges, footpaths and carriageway). In this instance, the multi-storey structure with an overall height ranging from 20-21m (at the north elevation) can easily be accommodated given the generous road width of the public road.

The separation of the proposed building to the commercial offices opposite ranges from 65-70m and c.57m to the existing garage opposite on Nangor Road.

### **1.2 Proposed Design Amendments**

We note that the building underwent several design iterations as part of the pre-planning process.

A copy of the proposed alterations and options were submitted to the Planning Authority on the 8 September 2021 and 13 October 2021 for review. We note that no comments were received at the time of writing this response.

Notwithstanding the above, the Planning Authority's concerns raised as part of the request for further information have been carefully examined. Several design solutions have been employed to further reduce the perceived visual impact of the building addressing Nangor Road.

We refer the Planning Authority to the revised verified views, drawings and design statement accompanying this response for more information.



*Figure 1 – Verified View from Nangor Road looking eastwards towards the Proposed Data Hall (RHS)*

The changes proposed as part of this further information response are set out as follows:

#### **1.2.1 Height and Massing**

The provision of a multi-storey data hall building derives from a sustainability requirement of making the most efficient use of the site. The northern boundary presents an opportunity to frame and enclose the public road by positioning the building relatively close to the Nangor Road boundary. We reiterate that the height and massing of the proposal is cognisant of the surrounding built environment.

**Solution:** Thin vertical wire mesh panels have been introduced to the plenums on the northern façade. This treatment helps to break up the perceived massing and contrasts well with the horizontal nature of the building.

The internal stairs and lift at roof level has been removed and reduces the overall height of the structure when viewed from the roundabout on Nangor Road.

The flues are re-positioned and grouped to the rear of the plenum structures. The flues have been reduced by 2.074m and are now at 21.0m above ground level. The flues are also screened by the wire mesh panels wrapping to the side of the plenums.



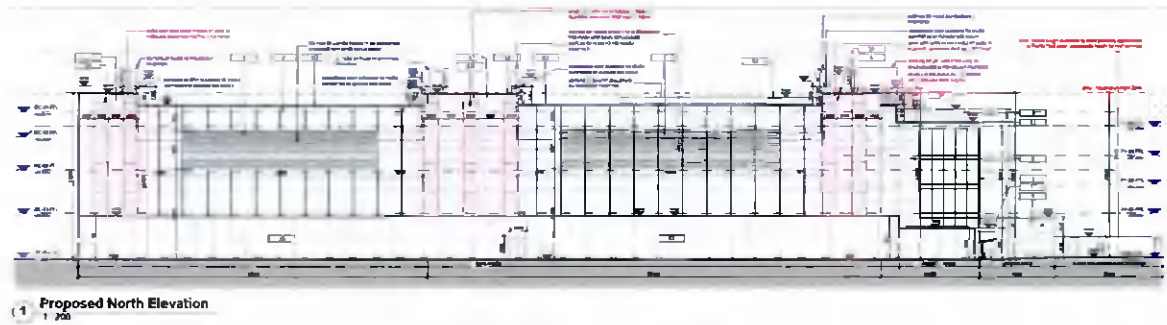


Figure 2 – Revised Northern Elevation

### 1.2.2 Length

The length of the building remains unchanged at c.111m (excluding loading dock). However, the perceived length has been reduced with the introduction of vertical mesh panels on the three plenums to the front of the building. These panels are positioned at three intervals along the elevation, visually interrupting the perceived length of the structure.

**Solution:** The northern elevation has been redesigned and is punctuated by vertical wire mesh cladded panels located at regular intervals on the plenums to break the perceived length of the elevation.

The plenums on the front elevation are slightly higher (c.1.3m) than the remainder of the northern façade. These height variations subtly break the elevation and mitigate against a monolithic block structure. The introduction of vertical panels in contrasting colours such as granite and anthracite balance with the proposed horizontal louvred panels and help to reduce the perceived length of the structure.

The transparent nature of the administrative areas allows natural light to filter through creating depth within the elevation. The space will be illuminated at night to create an active frontage more akin to commercial offices as opposed to logistics and warehouse structures.

### 1.2.3 Palette of Materials/Finishes

The design, massing and scale is appropriate for this greenfield development on zoned enterprise and employment land. The proposed façade design and materials respond to and enhance the architectural character of the area and existing commercial properties on Nangor Road.

**Solution:** A variety of façade finishes and textures are proposed along the northern elevation. The Data Centre is clad with horizontally fixed, composite flat metal panels with mineral wool core, powder coated to a dark grey colour (RAL 7016 Anthracite). Metal fins and composite panels add texture and visual interest to the wall cladding.

The overall façade will comprise a range of tonal grey colours ranging from light grey coloured composite panels to dark grey solid cladding. In contrast to the dark grey colours proposed, the fins on the side of the plenum structures comprise an assortment of light grey with accents of red (Equinix signature colour). The light grey coloured colours and glazed panels on the administrative front of house area will provide a distinctive feature within the elevation.

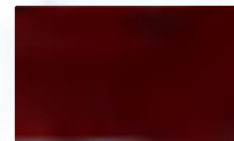
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**Picture 39:**

**Top left:** Mood image of dark grey composite cladding

**Top right:** Grey RAL colours for composite cladding and selected red for fins on plenums

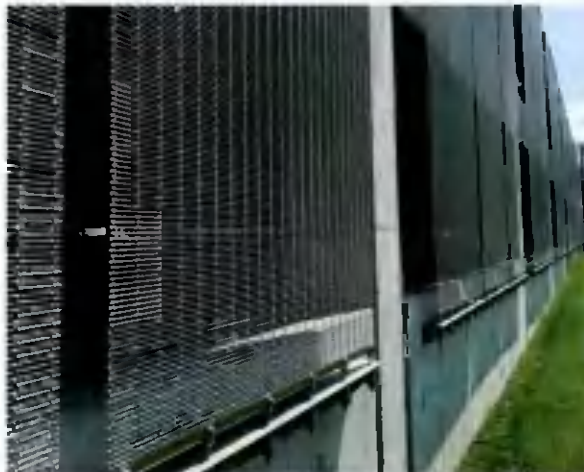
**Bottom left:** reference image of fin profile



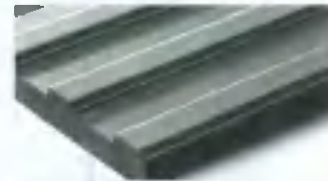
**Picture 40:**

**Left:** Reference image of wire mesh cladding

**Right:** Detail images of wire mesh



**Picture 43:** Reference image of fibre-cement cladding panels with grooved texture, colours Granite and Anthracite



**Figure 3 – Sample Materials and Finishes**

### 1.2.4 Setback

For clarity, no changes are proposed to the building footprint from originally proposed. The building is setback from the northern site boundary by c.33m, providing significant physical relief to the public road. Moreover, the proposed setback mimics the existing commercial building to the north of the site.

The overall height of the building at c.21m combined with a setback of c.33m is considered acceptable having regard to the generous width of Nangor Road at c.24m and corresponding commercial units opposite.

**Solution:** No changes are proposed to the building setback, however, the internal stairs and lift have been removed from the roof level, slightly reducing the overall height of the building.

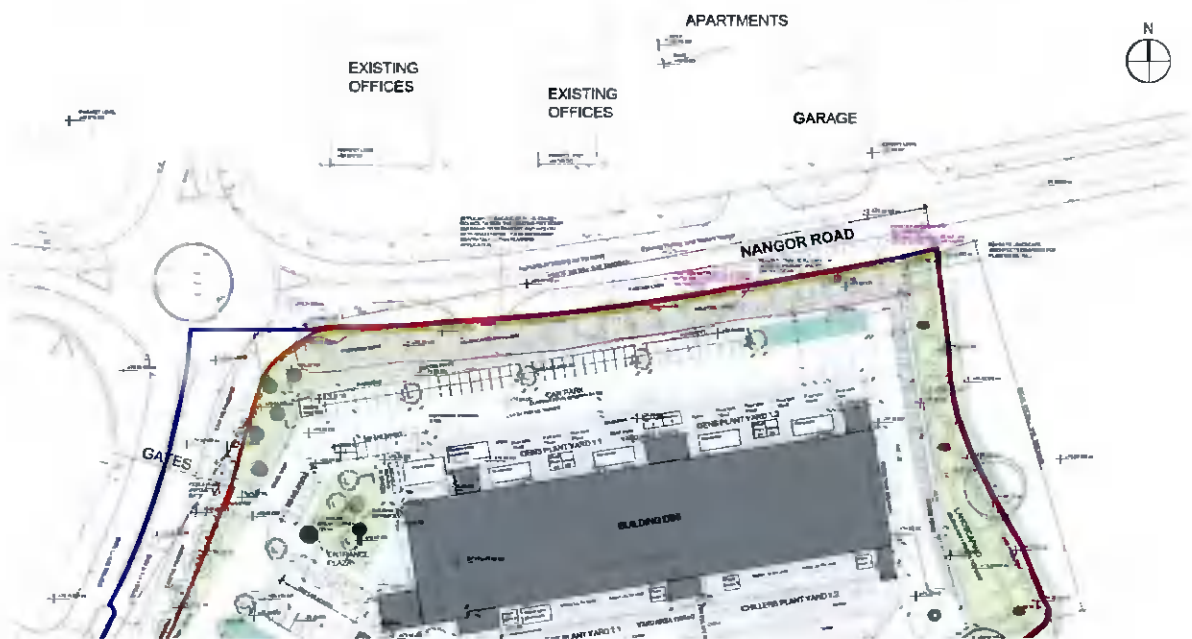


Figure 4 – Extract from the Revised Site Layout Plan with generous setbacks from Nangor Road

### 1.3 Conclusion

We note that the suggested measures outlined in the Planner's Report have been incorporated into the revised design, these include but are not limited to:

- A variation of external finishes and textures;
- Introduction of coloured and textured panelling; and
- Height adjustments resulting from the removal of stairs and lift at roof level.

To this end, we consider the revised proposal remedies the Planning Authority's concerns and should therefore be granted permission.



## 2.0 Item No. 2

*“The applicant is requested to provide revised high quality photomontages and CGIs, clearly showing the revised proposal, including treatments. Additional photomontage views from Grange Castle Golf Club/R136 to the east and the protected structures to the south/Baldonnel Road/Casement Aerodrome should be included.”*

### **Applicant Response to Item No. 2:**

In response to FI Item No. 2, we refer to the revised and additional verified views prepared by Digital Dimensions.

Additional views were taken from the R136 to the east, from casement aerodrome to the south and the roundabout on the Nangor Road to the north west of the site.

We refer to the visual impact assessment of the proposed views below for more information.

### **View 1**



As illustrated in View 1, the roof parapet is partially visible from this location but is screened by existing vegetation on Nangor Road. The visual impact is not significant at this location.

**View 2**



As illustrated in View 2, a partial view of the northern elevation is visible from this location. The existing grass verge, hedging and proposed tree planting will aid screening and provide visual interest as one travels along the Nangor Road. It is noted that tree planting in increased by 50% to aid screening and enhance the sylvan setting along the road. The formal linear tree planting also signifies the entrance to Profile Park. The proposed northern elevation helps to frame the view along the road. The visual impact is moderate and positive as this location.

**View 3**



As illustrated in View 3, the proposed development is clearly visible from the roundabout on the R134. The lightweight nature of the glazed front of house/administrative areas contrasts with the dark form of the northern elevation. The three plenums on the northern elevation are articulated by the alternative grey striped cladding that enhances the perceived verticality that balances the horizontal line of the building. The design is a fitting contribution to the built environment and entrance to Profile Park. The visual impact is significant and positive at this location.

**View 4**



As illustrated in View 4, a partial view of the northern elevation is visible from this location. The height and scale of the building with existing vegetation on either side of the R134. The visual impact is slight at this location.

**View 5**





As illustrated in View 5, the proposed development is not visible from the R134. The visual impact is imperceptible at this location.

**View 5a**



As illustrated in View 5a, the proposed development is shown in the centre right of the image with the existing Digital Realty structure in the centre left of the image. The proposed building height is complementary to the existing built environment. The palette of colours chosen in the cladding assimilates with the surrounding landscape. The visual impact is not significant at this location.



**View 6**



As illustrated in View 6, the view taken from within Profile Park, shows the southern elevation and western gable in the image background. The building nestles among the existing vegetation and is not visually dominant within this viewpoint. The visual impact is slight at this location.

**View 7**



As illustrated in View 7, the proposed development on the right hand side is complemented by the general height and form of existing commercial units on Nangor Road (shown on the left hand side of the image). The tree lined planting along the site perimeter ameliorates the built form with the natural landscape. The visual impact is moderate and positive at this location.

**View 8**



As illustrated in View 8, the data hall is screened by vegetation and trees on the golf club lands and is not visible from the R136. The visual impact is imperceptible at this location.

**View 9**



As illustrated in View 9, the data hall is screened from view by the elevated berming and boundary treatments along the R136. The visual impact is imperceptible at this location.

**View 10**



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As illustrated in View 10, the data hall is screened by dense vegetation and trees on the golf club lands and is not visible from the R136. The visual impact is imperceptible at this location.



### 3.0 Item No. 3

*“The applicant is requested to provide revised details of fencing/boundary treatments as follows: - security fencing should be behind landscaping - existing boundary fence should be replaced with a low level wall with railings above, similar to those in the surrounding area.”*

#### **Applicant Response to Item No. 3**

In response to Item No. 3, we refer to Section 3 of Drawing No. DBo80-RKD-ZZ-ZZ-DR-A-ZZZZ-3200 prepared by RKD Architects (submitted with the original proposal) and Drawing DBo80-MA-LS-XX-DR-L-PLNT-1050 prepared by Murray and Associates accompanying this response. The drawings demonstrate that the security fence is located to the south behind the landscape zone and berm. Additional tree planting along the northern berm is also proposed as part of this response.

The RFI also requests that the existing boundary fence should be replaced with a low level wall with railings above, similar to those in the surrounding area. In response to this item, this request would necessitate the removal of the existing fence and hedgerow, which we are keen to maintain and protect, similar to the riparian boundary on the southern site boundary.

We feel the removal of the established hedgerow would conflict with the key principles for development in enterprise and employment zones relating to open space and landscape as outlined in Table 11.18 and Policy ET3 Objective 5 of the County Plan which states:

*“To ensure that all business parks and industrial areas are designed to the highest architectural and landscaping standards and that natural site features, such as watercourses, trees and hedgerows are retained and enhanced as an integral part of the scheme.”*

The provision of an outer boundary (existing fence) and inner security fence (proposed) separated by the proposed landscape zone and berm enhances the level of security on site. Moreover, by retaining the existing hedge and fence to the north, the Applicant maintains minimal disruption to road users on Nangor Road.

### 4.0 Item No. 4

#### **Water:**

(a) In order to assess the feasibility of a connection to public water infrastructure further information is requested as follows: The applicant is required to engage with Irish Water through the submission of a Pre-Connection Enquiry (PCE) in order to determine the feasibility of connection to the public water infrastructure. The Confirmation of Feasibility (COF) must be submitted to the planning department as the response to this further information request. Pre-connection enquiries can be made at <https://www.water.ie/connections/get-connected>.

(b) It is unclear where the applicant proposes to make a connection to the public water mains network. The applicant is requested to submit a drawing in plan outlining the existing and proposed water supply layout for the development up to the point of connection to the public water mains. Maps of the public water mains and Wastewater drainage networks may be obtained, if available, for required locations in by emailing: [datarequests@water.ie](mailto:datarequests@water.ie).

#### **Foul:**

(a) In order to assess the feasibility of a connection to public waste water infrastructure further information is requested as follows; The applicant is required to engage with Irish Water through the submission of a Pre-Connection Enquiry (PCE) in order to determine the feasibility of connection to the public waste water infrastructure. The Confirmation of Feasibility (COF) must be submitted to the planning department as the response to this further information request. Pre-connection enquiries can be made at <https://www.water.ie/connections/get-connected/>.



(b) It is unclear where the applicant proposes to connect the foul water drainage from the development. The applicant is requested to submit a drawing showing existing and proposed foul water drainage layouts up to and including the point of connection to the public foul water sewer. The drawing shall include the location of all AJs, manholes, pipe size, material type and direction of flow. The drawing shall clearly show that the foul and surface water systems are discharging to separate pipe networks. Maps of the public watermains and Wastewater drainage networks may be obtained, if available, for required locations in by emailing: [datarequests@water.ie](mailto:datarequests@water.ie).”

#### **Applicant Response to Item No. 4:**

In response to Item No. 4, we refer to the cover letter prepared by Pinnacle Engineers.

To this end, we note that the Applicant has engaged with Irish Water in relation to connection to water and foul services. We can confirm that the subject proposal will connect to existing water and foul systems on the estate road at Profile Park.

#### **5.0 Item No. 5**

“The applicant is requested to provide: (a) a glint and glare analysis. (b) details of the MW output of the solar PVs.”

#### **Applicant Response to Item No. 5:**

In response to Item No. 5a, we refer to the accompanying glint and glare assessment prepared by Macroworks. As part of this further information response, the PV panels have been relocated on the roof of the proposed data hall. The report concludes the proposed pv panels will have a negligible impact on aircraft travelling to and from Casement Aerodrome.

In response to Item no. 5b, we confirm the mv output of the pv panels is 0.04MW.

To this end, we note that the Applicant is committed to the advancement of energy efficient solutions on site and will work closely with the Planning Authority in this regard.

#### **6.0 Item No. 6**

(a) The applicant is requested to submit a report showing greenfield run off rates and attenuation calculations for each surface water drainage catchment. The report shall include a breakdown of all surface types and run off coefficients for each surface water catchment area.

(b) The applicant is requested to submit a drawing which clearly shows : • All surface water catchment areas proposed. • The location of all proposed flow control devices and corresponding maximum discharge rates for each device. There shall be a flow control device inserted on the outfall of the proposed attenuation pond. The maximum surface water discharge rate from the site must not exceed 4.4 L/S

(c) The applicant is requested to minimise the use of underground attenuation systems on site. Where this cannot be achieved arch type systems should be installed as opposed to concrete tanks. The applicant is requested to submit a cross section detail of the proposed attenuation pond and underground attenuation systems.

(d) The applicant is requested to clarify what attenuation volumes are proposed for the development as the volumes referred to in the engineering report do not correlate with the submit surface war drainage plans.

(e) The applicant is requested to submit a drawing showing the inclusion of more Sustainable Drainage Systems (SuDS) for the development such as swales, filter drains, tree pits, rain gardens and Rainwater harvesting systems. A cross sectional detail is required of all proposed SuDS features.

*(f) The applicant is requested to demonstrate how water pollution mitigation measures have been incorporated into the design regarding fuel storage onsite. Fuel tank leakages must not allow polluted water to enter surface water drainage network. All works shall comply with the Greater Dublin Regional Code of Practice for Drainage Works in this regard. (g) The applicant is requested to confirm all petrol/oil interceptors proposed on the surface water drainage network shall be of Class 1 standard as per the requirements of the Greater Dublin Regional Code of Practice for Drainage Works."*

**Applicant Response to Item No. 6:**

In response to Item No. 6, we refer to the cover letter and drawings prepared by Pinnacle Engineers.

To this end, we note that the Applicant has endeavoured to implement sustainable and efficient surface water drainage systems in accordance with the following objectives of the County Plan:

***IE2 Objective 3:*** *To maintain and enhance existing surface water drainage systems in the County and promote and facilitate the development of Sustainable Urban Drainage Systems (SUDS), including integrated constructed wetlands, at a local, district and County level, to control surface water outfall and protect water quality.*

***IE2 Objective 5:*** *To limit surface water run-off from new developments through the use of Sustainable Urban Drainage Systems (SUDS) and avoid the use of underground attenuation and storage tanks.*

***G5 Objective 1:*** *To promote and support the development of Sustainable Urban Drainage Systems (SUDS) at a local, district and county level and to maximise the amenity and biodiversity value of these systems.*

## 7.0 Conclusion

The Applicant has endeavoured to address all items raised as part of the Further Information request and we trust that the Planning Authority will duly consider this submission in full in their assessment of the revised scheme.

The northern elevation has been suitably re-designed in response to the Planning Authority's concerns relating to visual impact outlined in Item No. 1. Several design amendments have been made to the facade including: introduction of a variety of additional finishes and colours; introduction of textured and coloured panelling; relocation of flues to the rear of the plenums and removal of internal stairs and lift at roof level resulting in the slight lowering of the overall building height.

The above measures mitigate against the perceived negative visual impacts of DB8 when viewed from the Nangor Road.

In respect of Item No. 2, we note that additional verified views have been taken from the roundabout on the Nangor Road (north west of the site), Casement Aerodrome (south of the site), R136 (east of the site). The additional verified views show the moderate yet positive visual impact of the proposal on the Nangor Road and slight/imperceptible visual impact from Casement Aerodrome and R136 respectively.

Item No. 3 requested alterations are made to the boundary treatments to the north of the site. We note that the existing hedgerow and fence will remain in situ to avoid loss of biodiversity on site, in keeping with the green infrastructure and heritage, conservation and landscape objectives outlined in the County Plan. Additional tree planting is also proposed along the northern perimeter for enhanced amenity and screening purposes.

For the purpose of clarification, the proposed security fence is located behind the proposed landscape zone and berming.

With regards to Item No. 4 and 6, a detailed response has been provided by Pinnacle Engineers in respect of the proposed water and foul connections and surface water drainage proposals for the subject development

In respect of Item No. 5, we note that the RFI is accompanied by a glint and glare assessment which concludes the proposed PV roof panels will have a negligible impact on aircraft travelling to and from Casement Aerodrome. The mv output for the pv panels is provided in this cover letter.

It is our considered planning opinion that the proposal now submitted addresses all concerns raised by the Planning Authority and we are of the view that any potential issues that may arise following consideration of this submission can be appropriately addressed by condition. As such, we foresee there being no reason to seek clarification on any of the matters given the comprehensive response prepared by the design team in this case.

All considered, we trust that the Planning Authority will look favourably on the proposed development and grant permission for the proposal as appropriate.

