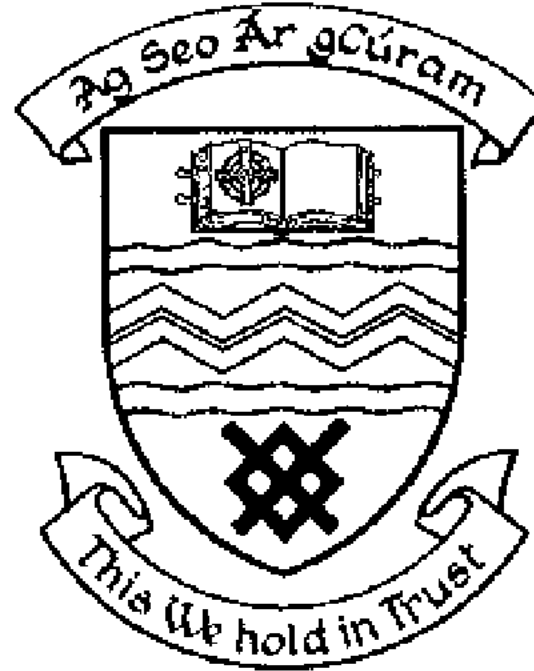


	South Dublin County Council Local Government (Planning & Development) Acts 1963 to 1993 Planning Register (Part 1)	Plan Register No. S98A/0160	
1. Location	Nangor Road, Dublin 12.		
2. Development	A new commercial vehicle centre, to include the following uses: sales, servicing, parts, assembly/repairs, offices, ancillary facilities, on site parking with access from John F. Kennedy Avenue.		
3. Date of Application	18/03/1998	Date Further Particulars (a) Requested (b) Received	
3a. Type of Application	Permission	1. 2.	1. 2.
4. Submitted by	Name: Larkin & Partners, Address: Architects, 1 Fitzwilliam Street Upper, Dublin 2.		
5. Applicant	Name: J. Harris (Assemblers), Address: Naas Road, Dublin 12.		
6. Decision	O.C.M. No. 0320 Date 16/02/1999	Effect AP GRANT PERMISSION	
7. Grant	O.C.M. No. 0658 Date 01/04/1999	Effect AP GRANT PERMISSION	
8. Appeal Lodged			
9. Appeal Decision			
10. Material Contravention			
11. Enforcement		Compensation	Purchase Notice
12. Revocation or Amendment			
13. E.I.S. Requested	E.I.S. Received	E.I.S. Appeal	
14. Registrar Date Receipt No.	

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Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

NOTIFICATION OF GRANT OF Permission

LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Final Grant Order Number 0658	Date of Final Grant 01/04/1999
Decision Order Number 0320	Date of Decision 16/02/1999
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),

Development A new commercial vehicle centre, to include the following uses: sales, servicing, parts, assembly/repairs, offices, ancillary facilities, on site parking with access from John F. Kennedy Avenue.

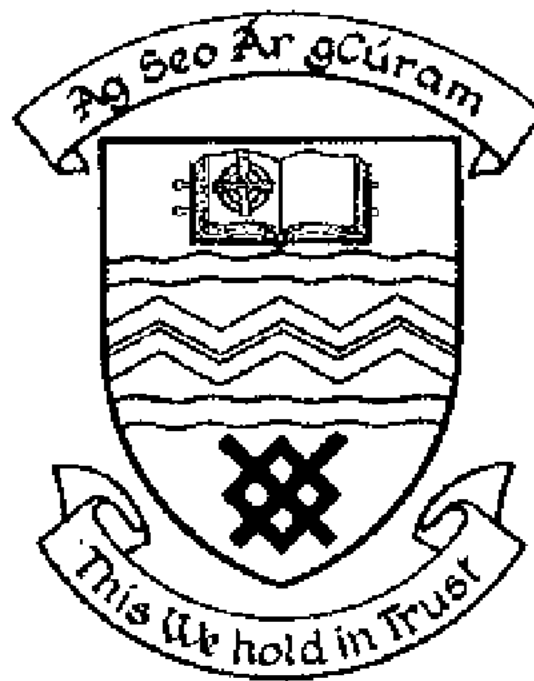
Location Nangor Road, Dublin 12.

Floor Area 5565.00 Sq Metres
Time extension(s) up to and including 17/02/1999
Additional Information Requested/Received /

A Permission has been granted for the development described above,
subject to the following (22) Conditions.

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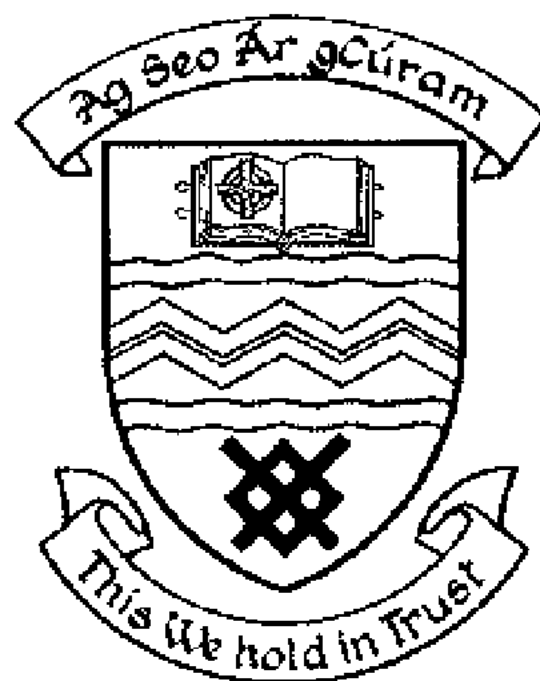
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Conditions and Reasons

- 1 The development to be carried out in its entirety in accordance with the plans, particulars and specifications lodged with the application as amended by the revised plans and details submitted by way of unsolicited additional information on 20/07/98, 25/09/98 and 14/01/99, save as may be required by the other conditions attached hereto.
REASON:
To ensure that the development shall be in accordance with the permission and that effective control be maintained.
- 2 That a financial contribution in the sum of money equivalent to the value of £23,200 (twenty three thousand two hundred pounds) as on 1st January, 1991, updated in accordance with the Wholesale Price Index - Building and Construction (Capital Goods) as published by the Central Statistics Office to the value pertaining at the time of payment shall be paid by the proposer to South Dublin County Council towards the cost of roads improvements and traffic management in the area of the proposed development and which facilitate this development; this contribution to be paid before the commencement of development on the site.
REASON:
It is considered reasonable that the developer should contribute towards the expenditure that was incurred and/or that is proposed to be incurred by the Council on road improvement works and traffic management schemes facilitating the proposed development.
- 3 Prior to the commencement of development, the developer shall lodge with the planning authority a bond for £600,000 of an insurance company, or other security to secure the provision and satisfactory completion of the diversion in connection with the development, coupled with an agreement empowering the planning authority to apply such security or part thereof to the satisfactory completion or maintenance of any part of the 9B main sewer diversion or consequential damage to adjoining Camac Culvert.
REASON:
In the interest of the proper planning and development of the area.
- 4 Prior to commencement of development, the applicant shall submit a detailed method statement from the Contractor for the protection of the Camac culvert and connections thereto and the maintenance of the existing flows discharging to and through the culvert for the written agreement of the Planning Authority.

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REASON:

In the interest of the proper planning and development of the area.

- 5 Prior to the commencement of development, the applicant shall submit a detailed method statement from the Contractor for maintaining existing foul and sewage flows and the methods of forming the final connections when bringing the new sewer into use, for the written agreement of the Planning Authority.

REASON:

In the interest of the proper planning and development of the area.

- 6 Applicant to give 5 days notice to Area Engineer Drainage, Deansrath Depot (tel. 01 4570784) so that supervision by South Dublin County Council personnel can be undertaken of all drainage works.

REASON:

In the interest of the proper planning and development of the area.

- 7 Prior to the commencement of development, the applicant shall agree with the Environmental Services Department of the Planning Authority and pay to the Council a sum towards the estimated supervision costs of the project by the Environmental Services Division of South Dublin County Council.

REASON:

In the interests of the proper planning and development of the area.

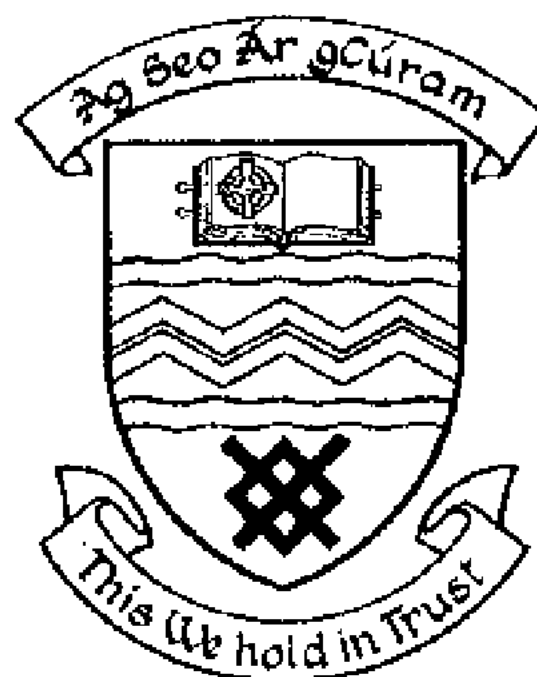
- 8 Applicant to employ a firm of reputable consulting engineers to monitor the condition of the Camac Culvert, during the diversion works, in addition to works carried out to divert the foul sewer and to certify the satisfactory completion of all works. Applicant to indemnify South Dublin County Council for a period of 10 years in respect of consequential loss due to any difficulties in maintaining existing flows to both the foul sewer and the Camac Culvert arising from proposed works.

REASON:

In the interests of the proper planning and development of the area.

- 9 Applicant to employ a firm of reputable consulting engineers to ensure that the existing hydraulic capacity of the sewer can be maintained in the re-routed sewer and a written report from the consulting engineers be submitted to South

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Dublin Count Council verifying same.

REASON:

In the interest of the proper planing and development of the area.

- 10 After the sewer diversion has been carried out the redundant sewer must be removed or filled with concrete to the satisfaction of the Building Control Section of South Dublin County Council.

REASON:

In the interest of the proper planning and development of the area.

- 11 Applicant shall submit detailed proposals for diversion of all surface water sewers discharging into the Camac River culvert which will be affected by the alterations to the site, prior to construction, for the written agreement of the Planning Authority.

REASON:

In the interests of the proper planning and development of the area.

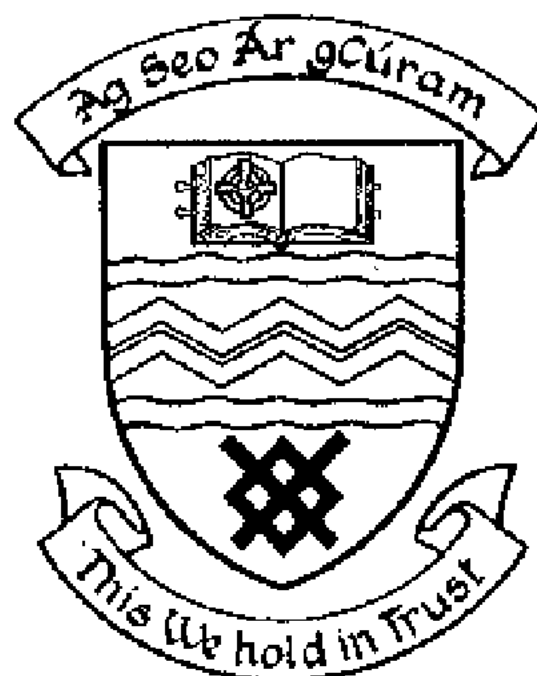
- 12 That the water supply and drainage arrangements, including the disposal of surface water, be in accordance with the requirements of the County Council in this regard the applicant shall have regard to the following:-

- a) Applicant to ensure full and complete separation of foul and surface water systems;
- b) Applicant to submit full details of proposed foul drainage, including pipe sizes, gradients, cover and invert levels, up to and including connection to public sewer prior to commencement of construction;
- c) No building shall be within 5m of public sewer or sewer with potential to be taken in charge;
- d) No discharge of trade effluent to sewer shall be permitted without the applicant first obtaining from the Environmental Services Department, a licence under Section 16 of the Water Pollution Acts 1977-1990;
- e) Applicant to grant wayleave to South Dublin County Council of 5m either side of the foul sewer;
- f) Applicant to submit full details of proposed surface water drainage arrangements, including pipe sizes, gradients, cover and invert levels, up to and including connection to public sewer prior to commencement of construction;
- g) All surface water runoff from truck parking/marshalling areas shall be routed via a petrol/oil/

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- diesel interceptor before discharging to the public sewer;
- h) Applicant to submit details of proposed watermain layout including watermain size, valve, meter and hydrant layout, and proposed point of connection to existing watermain prior to commencement of construction. Layout to be in accordance with Part B of 1997 Building Regulations;
- (i) All watermains greater than 150mm diameter shall be ductile iron;
- (j) No part of the unit shall be further than 47m from a fire hydrant;
- (k) No building shall lie within 5m of watermains less than 225mm diameter with 8m of watermains greater than 225mm diameter;
- (l) All connections to be carried out by South Dublin County Council personnel at applicant's prior expense;
- (m) 24 hour storage, a meter and a separate connection per unit shall be provided.

REASON:

In order to comply with the Sanitary Services Acts, 1878-1964.

- 13 Details, including samples of all proposed external finishes shall be submitted for the written agreement of the Planning Authority, prior to the commencement of development.

REASON:

In the interests of visual amenities.

- 14 Prior to substantial completion of the development the carparking/circulation areas indicated on the submitted plans shall be surfaced with a bituminous macadam and carparking bays shall be clearly marked out with a thermoplastic type material.

REASON:

In the interest of the proper planning and development of the area.

- 15 The carparking/circulation areas indicated shall be available at all times for carparking use and shall not be used for storage, display or other such uses.

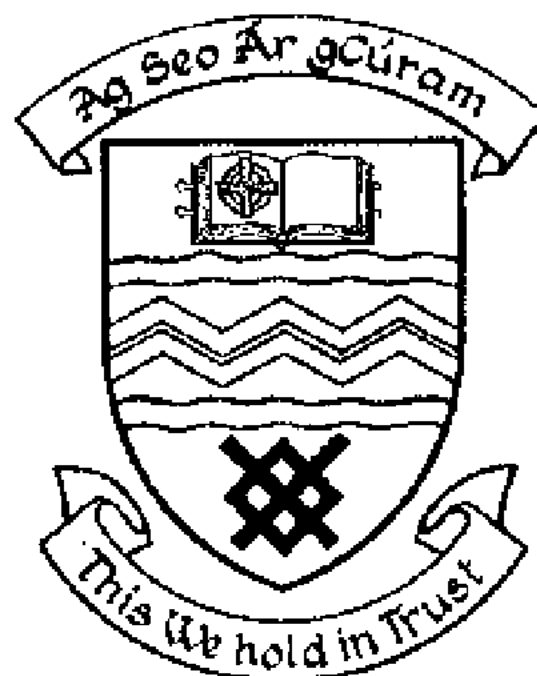
REASON:

In the interests of the proper planning and development of the area.

- 16 Prior to the commencement of development a detailed landscaping plan shall be submitted for the written agreement of the Planning Authority. All such landscaping

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and planting shall be undertaken in the first planting season following commencement of development.

REASON:

In the interests of the visual amenities of the area.

- 17 Boundary treatment to the site shall consist of a low plinth wall and railing, details to be submitted for the written agreement of the planning authority prior to the commencement of development (note - palisade fencing is not acceptable). The railings shall be painted a colour to be agreed with the Planning Authority.

REASON:

In the interests of the visual amenities of the area.

- 18 No advertising sign or structure to be erected except those which are exempted development, without prior approval of Planning Authority or An Bord Pleanála on appeal.

REASON:

In the interest of the proper planning and development of the area.

- 19 Prior to commencement of development the requirements of the Chief Fire Officer be ascertained and strictly adhered to in the development.

REASON:

In the interest of safety and the avoidance of fire hazard.

- 20 Prior to commencement of development the requirements of the Principal Environmental Health Officer be ascertained and strictly adhered to in the development.

REASON:

In the interest of health.

- 21 That a financial contribution in the sum of £44,927 (forty four thousand nine hundred and twenty seven pounds) be paid by the proposer to South Dublin County Council towards the cost of provision of public services in the area of the proposed development and which facilitate this development; this contribution to be paid before the commencement of development on the site.

REASON:

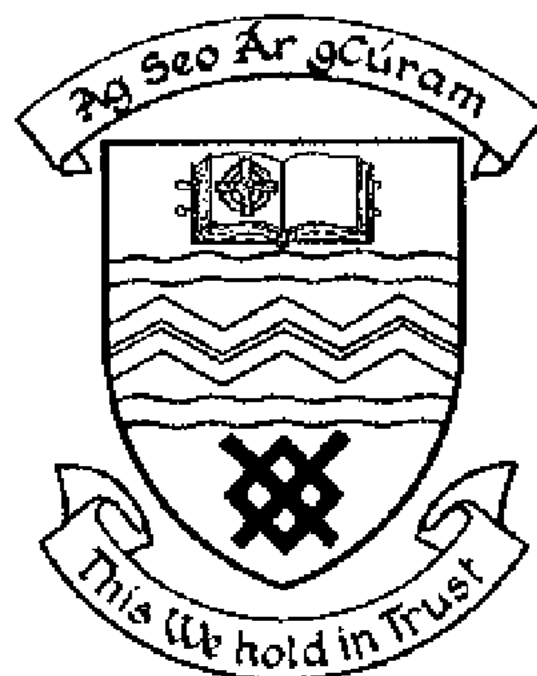
The provision of such services in the area by the Council will facilitate the proposed development. It is considered reasonable that the developer should contribute towards the cost of providing the services.

- 22 Before the development is commenced, the developer shall lodge with South Dublin County Council an approved Insurance Company Bond or cash in the sum of £20,000 (twenty thousand

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pounds) to secure the provision and satisfactory completion of services, kerbs, grass margins and footpaths fronting the site.

REASON:

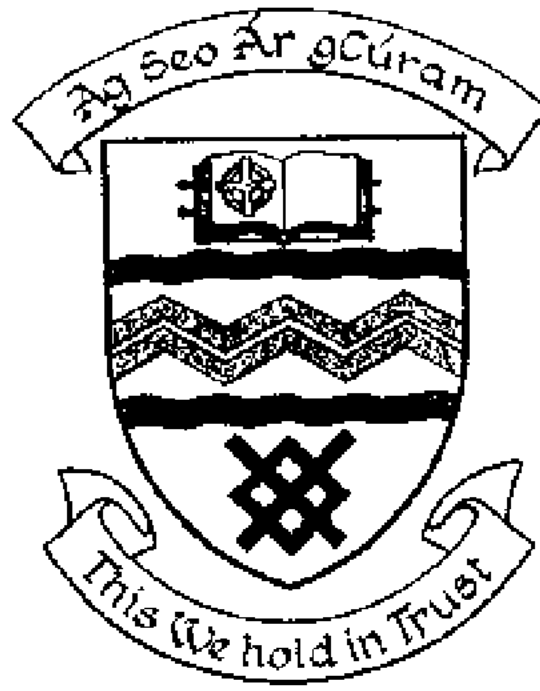
To ensure that a ready sanction may be available to induce the provision of services and prevent disamenity in the development.

- (1) All buildings must be designed and constructed in accordance with the Building Regulations 1991 amended 1994.
- (2) Building Control Regulations require a Commencement Notice. A copy of the Commencement Notice is attached.
- (3) A Fire Safety Certificate must be obtained from the Building Control Authority, where applicable.
- (4) Free Standing Walls must be designed and constructed in accordance with IS 325: Code of Practice for use of Masonry Part 1 : Structural use of unreinforced Masonry. The Owner must also ensure that the construction of all walls is supervised by a competent person.

Signed on behalf of South Dublin County Council.

Estacee Carlow 7th April 1999
for SENIOR ADMINISTRATIVE OFFICER

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**NOTIFICATION OF DECISION TO GRANT PERMISSION
LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993**

Decision Order Number 0320	Date of Decision 16/02/1999
Register Reference S98A/0160	Date: 18/03/1998

Applicant J. Harris (Assemblers),

Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.

Location Nangor Road, Dublin 12.

Floor Area Sq Metres

Time extension(s) up to and including 17/02/1999

Additional Information Requested/Received /

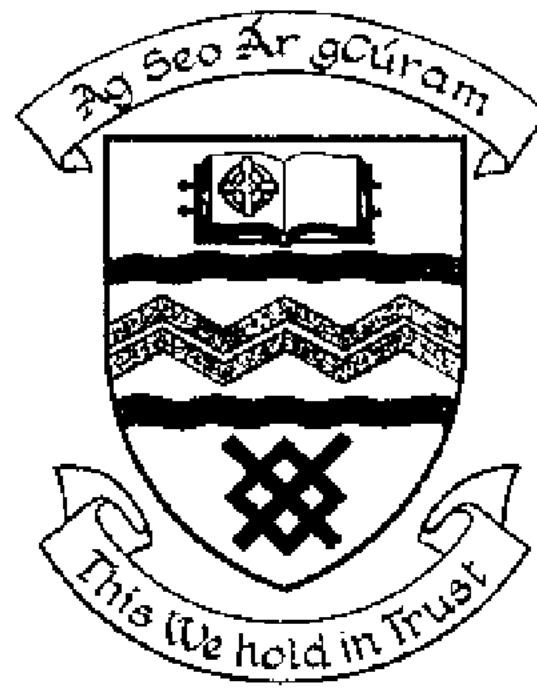
In pursuance of its functions under the above mentioned Acts, the South Dublin County Council, being the Planning Authority for the County Health District of Dublin, did by Order dated as above make a DECISION TO GRANT PERMISSION in respect of the above proposal.

Subject to the conditions (22) on the attached Numbered Pages.
Signed on behalf of the South Dublin County Council.

LA
..... 17/02/1999
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

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**NOTIFICATION OF DECISION TO GRANT PERMISSION
LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993**

Decision Order Number 0320	Date of Decision 16/02/1999
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Applicant J. Harris (Assemblers),

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John F. Kennedy Avenue.

Location Nangor Road, Dublin 12.

Floor Area Sq Metres

Time extension(s) up to and including 17/02/1999

Additional Information Requested/Received /

In pursuance of its functions under the above mentioned Acts, the South Dublin County Council, being the Planning Authority for the County Health District of Dublin, did by Order dated as above make a DECISION TO GRANT PERMISSION in respect of the above proposal.

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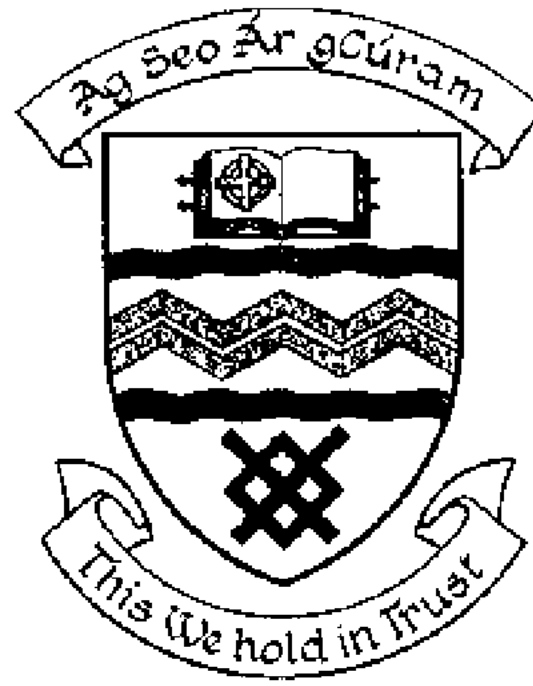
..... 17/02/1999
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
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1 Fitzwilliam Street Upper,
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Conditions and Reasons

- 1 The development to be carried out in its entirety in accordance with the plans, particulars and specifications lodged with the application as amended by the revised plans and details submitted by way of unsolicited additional information on 20/07/98, 25/09/98 and 14/01/99, save as may be required by the other conditions attached hereto.
REASON:
To ensure that the development shall be in accordance with the permission and that effective control be maintained.
- 2 That a financial contribution in the sum of money equivalent to the value of £23,200 (twenty three thousand two hundred pounds) as on 1st January, 1991, updated in accordance with the Wholesale Price Index - Building and Construction (Capital Goods) as published by the Central Statistics Office to the value pertaining at the time of payment shall be paid by the proposer to South Dublin County Council towards the cost of roads improvements and traffic management in the area of the proposed development and which facilitate this development; this contribution to be paid before the commencement of development on the site.
REASON:
It is considered reasonable that the developer should contribute towards the expenditure that was incurred and/or that is proposed to be incurred by the Council on road improvement works and traffic management schemes facilitating the proposed development.
- 3 Prior to the commencement of development, the developer shall lodge with the planning authority a bond for £600,000 of an insurance company, or other security to secure the provision and satisfactory completion of the diversion in connection with the development, coupled with an agreement empowering the planning authority to apply such security or part thereof to the satisfactory completion or maintenance of any part of the 9B main sewer diversion or consequential damage to adjoining Camac Culvert.
REASON:
In the interest of the proper planning and development of the area.

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- 4 Prior to commencement of development, the applicant shall submit a detailed method statement from the Contractor for the protection of the Camac culvert and connections thereto and the maintenance of the existing flows discharging to and through the culvert for the written agreement of the Planning Authority.

REASON:

In the interest of the proper planning and development of the area.

- 5 Prior to the commencement of development, the applicant shall submit a detailed method statement from the Contractor for maintaining existing foul and sewage flows and the methods of forming the final connections when bringing the new sewer into use, for the written agreement of the Planning Authority.

REASON:

In the interest of the proper planning and development of the area.

- 6 Applicant to give 5 days notice to Area Engineer Drainage, Deansrath Depot (tel. 01 4570784) so that supervision by South Dublin County Council personnel can be undertaken of all drainage works.

REASON:

In the interest of the proper planning and development of the area.

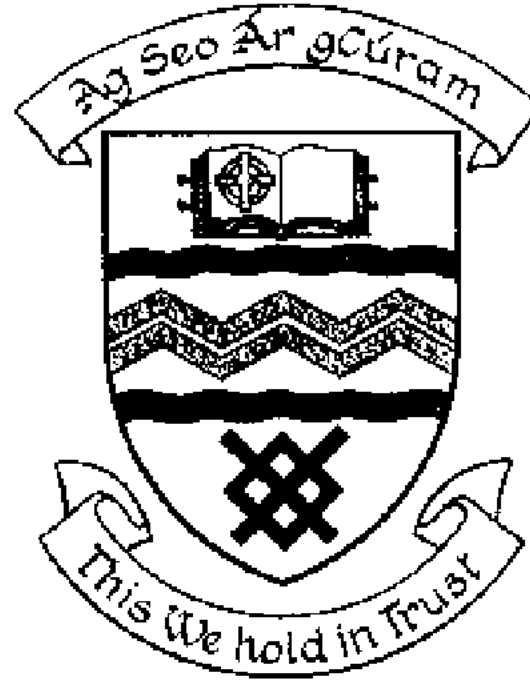
- 7 Prior to the commencement of development, the applicant shall agree with the Environmental Services Department of the Planning Authority and pay to the Council a sum towards the estimated supervision costs of the project by the Environmental Services Division of South Dublin County Council.

REASON:

In the interests of the proper planning and development of the area.

- 8 Applicant to employ a firm of reputable consulting engineers to monitor the condition of the Camac Culvert, during the diversion works, in addition to works carried out to divert the foul sewer and to certify the satisfactory completion of all works. Applicant to indemnify South Dublin County

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Council for a period of 10 years in respect of consequential loss due to any difficulties in maintaining existing flows to both the foul sewer and the Camac Culvert arising from proposed works.

REASON:

In the interests of the proper planning and development of the area.

- 9 Applicant to employ a firm of reputable consulting engineers to ensure that the existing hydraulic capacity of the sewer can be maintained in the re-routed sewer and a written report from the consulting engineers be submitted to South Dublin County Council verifying same.

REASON:

In the interest of the proper planning and development of the area.

- 10 After the sewer diversion has been carried out the redundant sewer must be removed or filled with concrete to the satisfaction of the Building Control Section of South Dublin County Council.

REASON:

In the interest of the proper planning and development of the area.

- 11 Applicant shall submit detailed proposals for diversion of all surface water sewers discharging into the Camac River culvert which will be affected by the alterations to the site, prior to construction, for the written agreement of the Planning Authority.

REASON:

In the interests of the proper planning and development of the area.

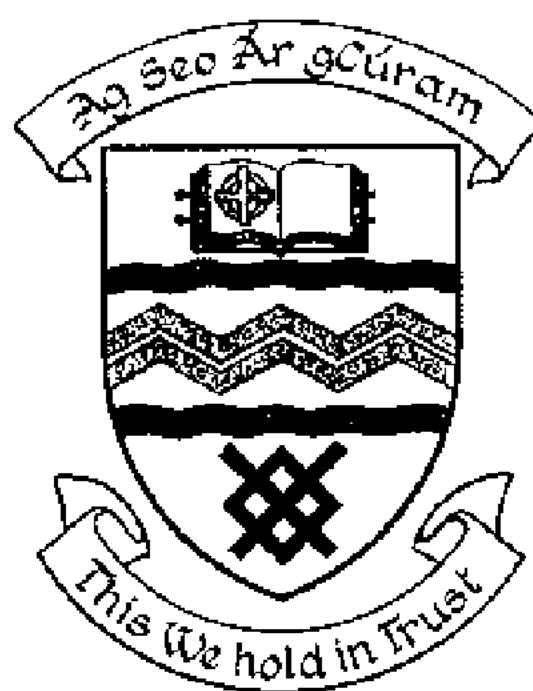
- 12 That the water supply and drainage arrangements, including the disposal of surface water, be in accordance with the requirements of the County Council in this regard the applicant shall have regard to the following:-

- a) Applicant to ensure full and complete separation of foul and surface water systems;
- b) Applicant to submit full details of proposed foul drainage, including pipe sizes, gradients, cover and

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REG. REF. S98A/0160

- invert levels, up to and including connection to public sewer prior to commencement of construction;
- c) No building shall be within 5m of public sewer or sewer with potential to be taken in charge;
 - d) No discharge of trade effluent to sewer shall be permitted without the applicant first obtaining from the Environmental Services Department, a licence under Section 16 of the Water Pollution Acts 1977-1990;
 - e) Applicant to grant wayleave to South Dublin County Council of 5m either side of the foul sewer;
 - f) Applicant to submit full details of proposed surface water drainage arrangements, including pipe sizes, gradients, cover and invert levels, up to and including connection to public sewer prior to commencement of construction;
 - g) All surface water runoff from truck parking/marshalling areas shall be routed via a petrol/oil/diesel interceptor before discharging to the public sewer;
 - h) Applicant to submit details of proposed watermain layout including watermain size, valve, meter and hydrant layout, and proposed point of connection to existing watermain prior to commencement of construction. Layout to be in accordance with Part B of 1997 Building Regulations;
 - (i) All watermains greater than 150mm diameter shall be ductile iron;
 - (j) No part of the unit shall be further than 47m from a fire hydrant;
 - (k) No building shall lie within 5m of watermains less than 225mm diameter with 8m of watermains greater than 225mm diameter;
 - (l) All connections to be carried out by South Dublin County Council personnel at applicant's prior expense;
 - (m) 24 hour storage, a meter and a separate connection per unit shall be provided.

REASON:

In order to comply with the Sanitary Services Acts, 1878-1964.

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- 13 Details, including samples of all proposed external finishes shall be submitted for the written agreement of the Planning Authority, prior to the commencement of development.

REASON:

In the interests of visual amenities.

- 14 Prior to substantial completion of the development the carparking/circulation areas indicated on the submitted plans shall be surfaced with a bituminus macadam and carparking bays shall be clearly marked out with a thermoplastic type material.

REASON:

In the interest of the proper planning and development of the area.

- 15 The carparking/circulation areas indicated shall be available at all times for carparking use and shall not be used for storage, display or other such uses.

REASON:

In the interests of the proper planning and development of the area.

- 16 Prior to the commencement of development a detailed landscaping plan shall be submitted for the written agreement of the Planning Authority. All such landscaping and planting shall be undertaken in the first planting season following commencement of development.

REASON:

In the interests of the visual amenities of the area.

- 17 Boundary treatment to the site shall consist of a low plinth wall and railing, details to be submitted for the written agreement of the planning authority prior to the commencement of development (note - palisade fencing is not acceptable). The railings shall be painted a colour to be agreed with the Planning Authority.

REASON:

In the interests of the visual amenities of the area.

- 18 No advertising sign or structure to be erected except those which are exempted development, without prior approval of Planning Authority or An Bord Pleanála on appeal.

REASON:

SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS

Bosca 4122,
Lár an Bhaile, Tamhlacht,
Baile Átha Cliath 24.

Telefon: 01-414 9000
Facs: 01-414 9104



**PLANNING
DEPARTMENT**
P.O. Box 4122,
Town Centre, Tallaght,
Dublin 24.

Telephone: 01-414 9000
Fax: 01-414 9104

REG. REF. S98A/0160

In the interest of the proper planning and development of the area.

- 19 Prior to commencement of development the requirements of the Chief Fire Officer be ascertained and strictly adhered to in the development.

REASON:

In the interest of safety and the avoidance of fire hazard.

- 20 Prior to commencement of development the requirements of the Principal Environmental Health Officer be ascertained and strictly adhered to in the development.

REASON:

In the interest of health.

- 21 That a financial contribution in the sum of £44,927 (forty four thousand nine hundred and twenty seven pounds) be paid by the proposer to South Dublin County Council towards the cost of provision of public services in the area of the proposed development and which facilitate this development; this contribution to be paid before the commencement of development on the site.

REASON:

The provision of such services in the area by the Council will facilitate the proposed development. It is considered reasonable that the developer should contribute towards the cost of providing the services.

- 22 Before the development is commenced, the developer shall lodge with South Dublin County Council an approved Insurance Company Bond or cash in the sum of £20,000 (twenty thousand pounds) to secure the provision and satisfactory completion of services, kerbs, grass margins and footpaths fronting the site.

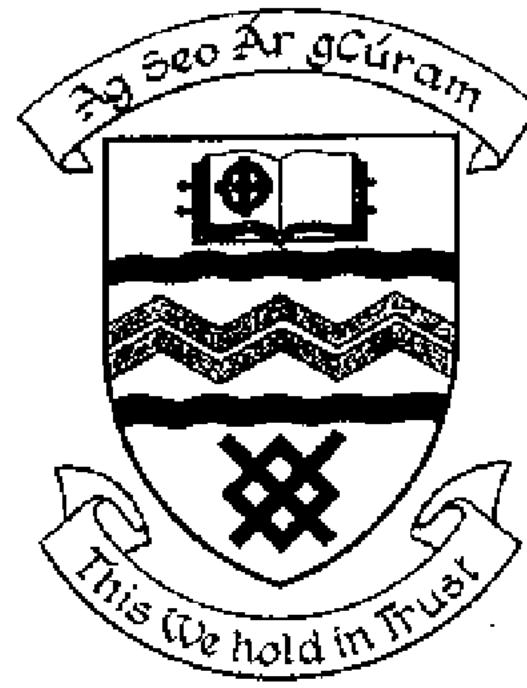
REASON:

To ensure that a ready sanction may be available to induce the provision of services and prevent disamenity in the development.

SOUTH DUBLIN COUNTY COUNCIL
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LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 0048	Date of Decision 12/01/99
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.
Location Nangor Road, Dublin 12.

Dear Sir / Madam,

In accordance with Section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/02/99

Yours faithfully

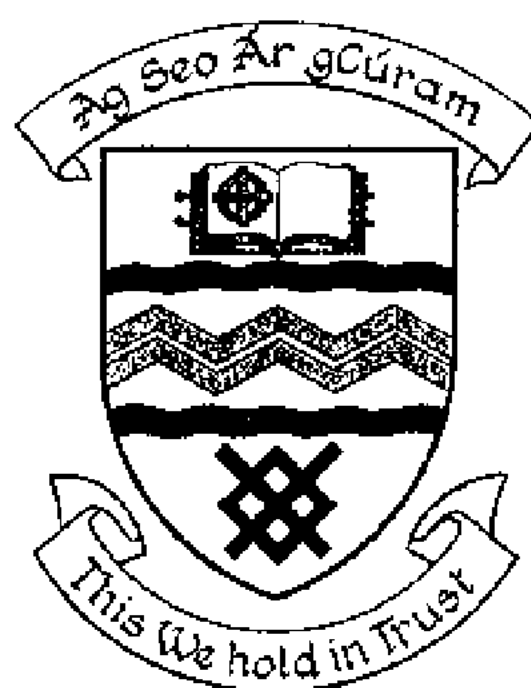
DA
..... 12/01/99
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS

Bosca 4122,
Lár an Bhaile, Tamhlacht,
Baile Átha Cliath 24.

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LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 2526	Date of Decision 15/12/98
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.
Location Nangor Road, Dublin 12.

Dear Sir / Madam,

In accordance with Section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/01/99

Yours faithfully

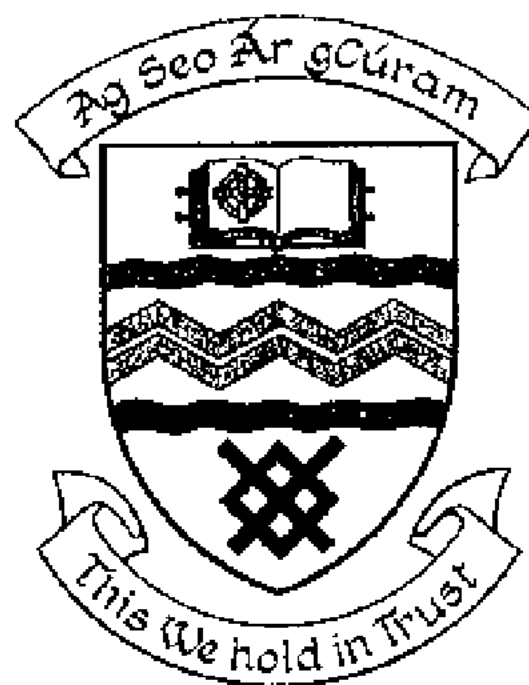
LAH
..... 15/12/98
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

**SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS**

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LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 2259	Date of Decision 12/11/98
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.
Location Nangor Road, Dublin 12.

Dear Sir / Madam,

In accordance with Section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/12/98

Yours faithfully

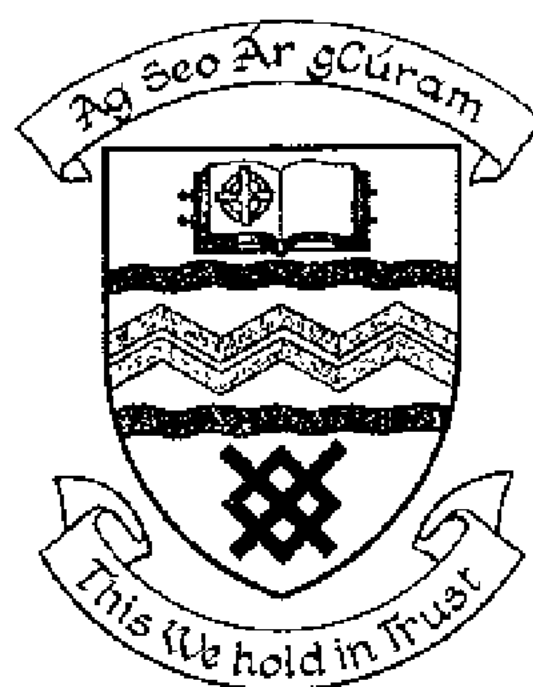
LH
..... 12/11/98
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS

Bosca 4122,
Lár an Bhaile, Tamhlacht,
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**PLANNING
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LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 2083	Date of Decision 15/10/98
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.
Location Nangor Road, Dublin 12.

Dear Sir / Madam,

In accordance with section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/11/98

Yours faithfully

RA
..... 15/10/98
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS



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LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 1825	Date of Decision 11/09/98
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.

Location Nangor Road, Dublin 12.

Dear Sir / Madam,

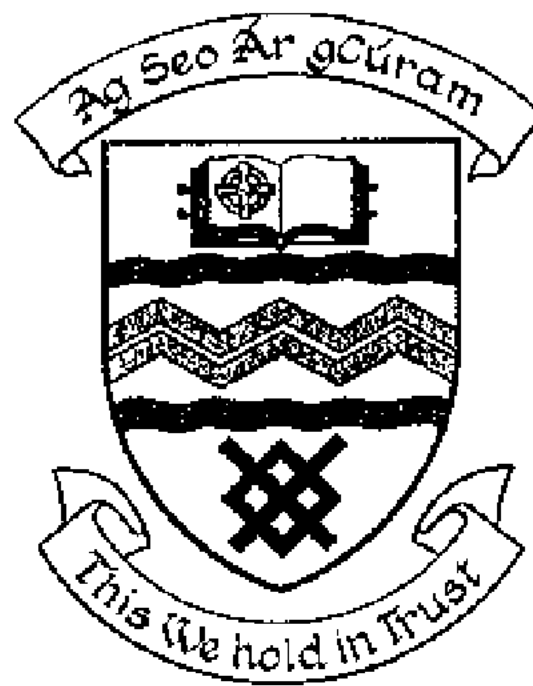
In accordance with Section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/10/98

Yours faithfully

..... 11/09/98
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS



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LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 1387	Date of Decision 14/07/98
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development: A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.

Location Nangor Road, Dublin 12.

Dear Sir / Madam,

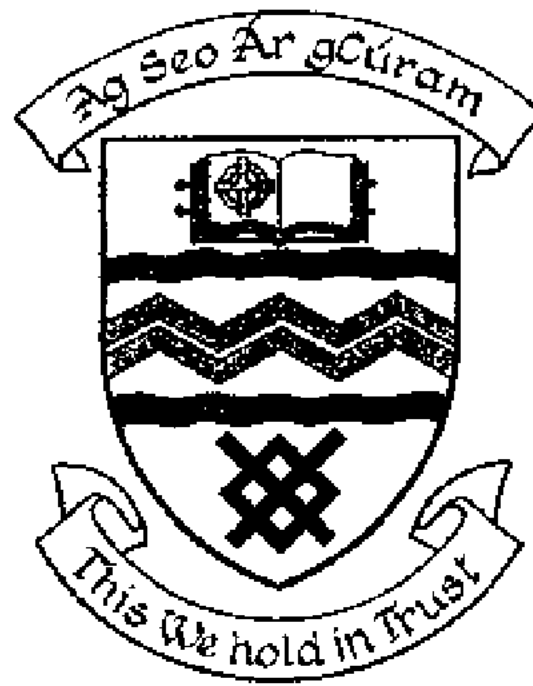
In accordance with Section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/09/98

Yours faithfully

..... 14/07/98
for SENIOR ADMINISTRATIVE OFFICER

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS



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~~REG REF. S98A/0160~~

LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 1122	Date of Decision 11/06/98
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.

Location Nangor Road, Dublin 12.

Dear Sir / Madam,

In accordance with Section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/07/98

Yours faithfully

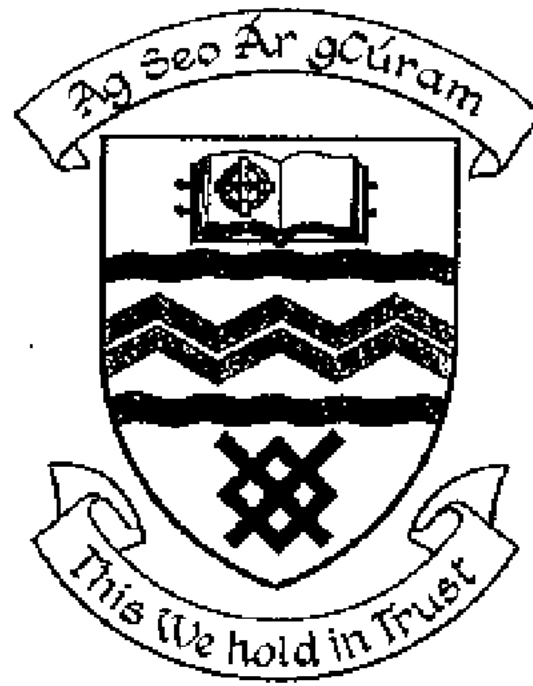
.....
for SENIOR ADMINISTRATIVE OFFICER 11/06/98

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.

SOUTH DUBLIN COUNTY COUNCIL
COMHAIRLE CHONTAE ÁTHA CLIATH THEAS

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LOCAL GOVERNMENT (PLANNING AND DEVELOPMENT) ACTS, 1963 TO 1993

Decision Order Number 0773	Date of Decision 22/04/98
Register Reference S98A/0160	Date 18th March 1998

Applicant J. Harris (Assemblers),
App. Type Permission
Development A new commercial vehicle centre, to include the following
uses: sales, servicing, parts, assembly/repairs, offices,
ancillary facilities, on site parking with access from
John F. Kennedy Avenue.

Location Nangor Road, Dublin 12.

Dear Sir / Madam,

In accordance with Section 26 (4a) of the Local Government (Planning and Development) Act 1963 as amended by Section 39 (f) of the Local Government (Planning and Development) Act 1976 the period for considering this application has been extended, up to and including 17/06/98

Yours faithfully

.....
for SENIOR ADMINISTRATIVE OFFICER 22/04/98

Larkin & Partners,
Architects,
1 Fitzwilliam Street Upper,
Dublin 2.